

# HIGHWAY 111 WEST

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**SPECIFIC PLAN**



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CITY OF RANCHO MIRAGE

# 111 WEST SPECIFIC PLAN

## CITY OF RANCHO MIRAGE

*Updated January 13, 2014*

This updated version includes ordinances and amendments as they became effective following the approval of the original Highway 111 West Specific Document by City Council on December 22, 1989.

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# Forward

The guidelines and regulations contained in this document provide the framework for design and consider project-wide issues as well as site-specific issues. The guidelines express a desired character for future development and address site planning, architecture, signage, landscape and hardscape concepts.

The objective is unified environment with a master planned design. At a site-specific level, individual projects shall be required to comply with relevant design guidelines applicable to each use. While each project should relate to the community context, this document is not intended to limit innovative design, but rather provide clear direction and design criteria. Each project shall be compatible with common community elements; however, differences in product type, use, tenant preferences and other factors necessitate the need for separate identity of each individual project.

Summarized, the goals of the Specific Plan are as follows:

- To provide the City of Rancho Mirage with the necessary assurances that the projects attain the desired level of quality.
- To serve as design criteria for use by planners, architects, landscape architects, engineers, builders, and future property owners;
- To provide guidance to City Staff, Planning Commission and the City Council when reviewing future development projects within the project area; and
- To provide a viable framework and clear direction without limiting the creativity of project applicants, architect and landscape architects.

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# Table of Contents

<b>1. Introduction</b>	<b>1</b>	<b>6. Guideline Framework</b>	<b>63</b>	<b>12. Parking and Parking “Plazas”</b>	<b>96</b>
SETTING	1	PURPOSE AND INTENT	63	RESIDENTIAL GARAGES AND CARPORTS	96
PURPOSE & INTENT	1	<b>7. Site Planning Guidelines</b>	<b>63</b>	<b>13. Architectural Guidelines</b>	<b>99</b>
AUTHORITY & SCOPE	1	ORIENTATION OF STRUCTURES	63	INTRODUCTION	99
CEQA COMPLIANCE	1	SITE COVERAGE	63	POSITIVE AND NEGATIVE DESIGN FEATURES	99
BACKGROUND	1	GRADING	63	SITE PLANNING CONSIDERATIONS	100
PLANNING OBJECTIVES	4	DRAINAGE	64	PLAN COMPONENTS	100
OPPORTUNITIES AND CONSTRAINTS	4	LOADING STORAGE, REFUSE AND SERVICE AREAS	64	COMMON DETAILS	102
REGULATIONS AND GUIDELINE FRAMEWORK	6	UTILITIES AND EXTERIOR EQUIPMENT	66	<b>14. Implementation</b>	<b>117</b>
PROCESS OF DEVELOPMENT PLAN AND DESIGN REVIEW	6	<b>8. Landscape Guidelines</b>	<b>66</b>	ASSISTANCE AND INCENTIVE PROGRAM CRITERIA	117
<b>2. Plan Structure and Design Concepts</b>	<b>9</b>	GENERAL PROVISIONS	66	ASSISTANCE AND INCENTIVE PROGRAM ACTIONS	117
LAND USE PLAN	9	SPECIFICATIONS	68	LOT CONSOLIDATION	117
SUMMARY OF USES	9	LANDSCAPING OF COMMERCIAL PARKING AREAS	68	LANDSCAPE EASEMENT DEVELOPMENT	117
DISTRICT DESIGN CONCEPTS	9	LANDSCAPE PLANT PALETTE RECOMMENDATIONS	70		
DISTRICT 1	12	<b>9. Hardscape Elements</b>	<b>79</b>		
<b>3. Corridor Circulation</b>	<b>26</b>	CURBING AND CURB CUTS	79		
CIRCULATION PLAN	26	PAVING	79		
HIERARCHY OF STREETS	28	WALLS AND FENCES	79		
PEDESTRIAN WALKWAYS	28	STREET FURNITURE	82		
<b>4. Landscape/Hardscape Concept</b>	<b>30</b>	<b>10. Sign Guidelines</b>	<b>86</b>		
EDGE TREATMENTS	32	GENERAL PROVISIONS	86		
SPECIAL INTERSECTIONS	40	DESIGN, CONSTRUCTION AND MAINTENANCE	86		
DESIGN CONCEPTS	40	GROUND SIGNS	88		
RESIDENTIAL ENTRY TREATMENT	42	TENANT IDENTIFICATION SIGNS	90		
PUBLIC PLAZAS AND NATURAL FEATURES	44	TEMPORARY SIGNS	93		
<b>5. Land Development Regulations</b>	<b>47</b>	PROHIBITED SIGNS	93		
PLANNING DISTRICTS	48	<b>11. Lighting Guidelines</b>	<b>94</b>		
PERMITTED USES	50	GENERAL PROVISIONS	94		
DETERMINATION OF USE	50	HIGHWAY 111 LIGHTING	94		
DISTRICT REGULATIONS	55	ON-SITE PARKING LOT AND PEDESTRIAN LIGHTING	94		
SPECIAL CONDITIONS	57	ARCHITECTURAL LIGHTING	94		
GENERAL PROVISIONS	57	LANDSCAPE LIGHTING	94		



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# Exhibits and Illustrations

EXHIBIT 1. AERIAL PHOTO.....	3
EXHIBIT 2. OPPORTUNITIES AND CONSTRAINTS .....	5
EXHIBIT 3. CONCEPTUAL LAND USE PLAN .....	10
EXHIBIT 4. ILLUSTRATIVE PLAN.....	11
EXHIBIT 5. DISTRICT 1—RESTAURANT ROW .....	13
EXHIBIT 6. DISTRICT 2— RESTAURANT/DESIGN CENTER .....	15
EXHIBIT 7. DISTRICT 3— ATTACHED RESIDENTIAL .....	17
EXHIBIT 8. DISTRICT 4—MOBILE HOME PARK .....	19
EXHIBIT 9. DISTRICT 5—OFFICE CENTER.....	21
EXHIBIT 10. DISTRICT 6—COMMERCIAL EAST .....	23
EXHIBIT 11 DISTRICT 7— COMMERCIAL WEST .....	25
EXHIBIT 12. CIRCULATION PLAN.....	27
EXHIBIT 13. TYPICAL STREET SECTIONS .....	29
EXHIBIT 14. CONCEPTUAL LANDSCAPE PLAN .....	31
EXHIBIT 15. SECTIONS A1-A1’ AND A2-A2’ .....	33
EXHIBIT 16. SECTIONS A3-A3’ AND A4-A4’ .....	35
EXHIBIT 17. SECTIONS B-B’ AND C-C’ .....	37
EXHIBIT 18. ROADWAY LANDSCAPE SECTIONS D-D’ AND E-E’ .....	39
EXHIBIT 19. SPECIAL INTERSECTION CONCEPT.....	41
ILLUSTRATION 1. RESIDENTIAL PLAN ENTRY ILLUSTRATION .....	43
ILLUSTRATION 2. PUBLIC PLAZAS AND NATURAL FEATURES.....	45
EXHIBIT 20. LAND USE DISTRICTS.....	49
ILLUSTRATION 3. LOADING AREA/UTILITY SCREENING.....	65
EXHIBIT 21. LANDSCAPE PLAN.....	67
ILLUSTRATION 4. PARKING PLAZA LANDSCAPE .....	69
ILLUSTRATION 5. FENCES AND WALLS.....	81
ILLUSTRATION 6. STREET FURNITURE 1.....	83
ILLUSTRATION 7. STREET FURNITURE 2.....	85
EXHIBIT 22. CONCEPTUAL SIGN LOCATION PLAN .....	87
ILLUSTRATION 8. GROUND SIGN TYPES.....	89
ILLUSTRATION 9. DIRECTIONAL SIGNS.....	91
ILLUSTRATION 10. IDENTIFICATION SIGNS .....	92

ILLUSTRATION 11. LIGHTING DESIGN PRINCIPLES .....	95
ILLUSTRATION 12. PARKING PLAZA CONCEPT AND LAYOUT .....	97
ILLUSTRATION 13. PARKING SHELTER DESIGN PARAMETERS .....	98
ILLUSTRATION 14. PLAN COMPONENTS .....	101
ILLUSTRATION 15. COMMON DETAILS .....	103
ILLUSTRATION 16. ROOFS AND WALLS .....	105
ILLUSTRATION 17. COLUMNS AND ARCHES .....	107
ILLUSTRATION 18. WINDOWS .....	109
ILLUSTRATION 19. EXTERIOR DOORS .....	111
ILLUSTRATION 20. PAVING .....	113
ILLUSTRATION 21. WALLS, PERGOLAS, AND SHADE STRUCTURES.....	115



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# Section 1

## THE SPECIFIC PLAN APPROACH

### 1. Introduction

#### SETTING

The Highway 111 Corridor, approximately four and one-half miles in length, is a major circulation and commercial axis for the City of Rancho Mirage. Highway 111 connects directly with the adjacent desert communities of Cathedral City to the north and Palm Desert to the south and is a primary artery through the Coachella Valley. The Corridor poses a major opportunity for projecting a positive City image as it is historically well-traveled and serves as the gateway into the City. It also poses a challenge to the City to assure its continued economic vitality.

This Plan focuses on a one-mile increment of the Corridor known as 111 West. The planning area is shown in the aerial photo in Exhibit 1 (updated as of 2013).

#### PURPOSE & INTENT

The purpose of this Specific Plan is to stimulate responsible development and to guide revitalization of the Highway 111 Corridor Area in the City of Rancho Mirage through the implementation of customized Development Regulations and Design Guidelines. It is intended to implement policies of the City's General Plan, Community Economic Development Strategy and other applicable planning documents, with particular emphasis given to recommendations and implementation measures encouraging the development of certain land use and circulation components and, the establishment of development criteria commensurate with quality and economic success.

The Specific Plan provides a "bridge" between the general goals, objectives and policies of the General Plan and the regulatory procedures of the Zoning Code. The Specific Plan is intended to facilitate and make easier the development process while at the same time assuring quality development.

#### AUTHORITY & SCOPE

The California Government Code authorizes cities to adopt Specific Plans either by resolution as policy or by ordinance as regulation. Both Planning Commission and City Council hearings are required. In either form, Specific Plans must be adopted by the City Council to be in effect.

This is a regulatory Specific Plan. It serves as zoning for the properties involved. Development plans and tentative parcel maps or tract maps in this area must be consistent with this Specific Plan. If a development agreement is sought, it must also be found to be consistent with the General Plan and this Specific Plan. The scope of subjects covered is the same as the scope of the General Plan, to the extent they apply to this portion of the community.

The Highway 111 Corridor Specific Plan is established through the authority granted to the City of Rancho Mirage by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457.

This Specific Plan was adopted by the Rancho Mirage City Council on May 31, 1990. The adopting ordinance is Ordinance Number 473. Specific Plan Exhibit number 20, Land Use Districts, was adopted as part of the General Plan Land Use Element. It was adopted by resolution by the City Council on July 26, 1990.

#### CEQA COMPLIANCE

The Specific Plan was prepared in accordance with the California Environmental Quality Act (CEQA). The required initial study was prepared for the Specific Plan, which determined that this project may have significant environmental impacts; and therefore, an Environmental Impact Report (EIR) was warranted. The EIR addresses the potential environmental impacts associated with the Specific Plan/General Plan Amendment and is intended to serve as a Master EIR document for the specific plan project area. The EIR will apply to future development plans, tentative tract maps, parcel maps and any other development processed in conformance with the 111 West Specific Plan. Future development projects that are consistent with this Specific Plan will require either no further environmental documentation or, in special cases as may be determined by the Community Development Director, focused environmental analysis and action pursuant to Section 15162 of the CEQA Guidelines.

#### BACKGROUND

In the fall of 1988, field surveys, data mapping, analyses and assessments were undertaken by the consulting firm Terra Nova Planning and Research, Inc. and the Redevelopment Agency of the City of Rancho Mirage to determine the physical condition of

improvements and infrastructure along the Highway 111 corridor and to identify the various land uses occurring there, as a basis for planning a redevelopment strategy. Data was logged on a parcel by parcel basis.

Digital mapping of the study area followed. Structures were ranked by condition (very good, acceptable to poor, and unacceptable to dangerous) and the varying degrees of aesthetic and structural maintenance they required. Additional criteria were added by the Agency to further refine the ranking process, including conditions of design and character of physical construction, building and lot utilization, density of population and percentage of lot coverage. Also considered, where appropriate, were issues of adequate building ventilation, light, sanitation, open spaces and impacts on the scenic highway. Building age, obsolescence, deterioration, dilapidation, mixed character, or shifting area uses were also considered.

Staff concerns also included impacts to traffic flows along State Highway 111, created by existing patterns of land use. Traffic congestion, speed and confusion of traffic flow intimidate many drivers who would otherwise patronize exiting commercial development. It is believed that commercial space, while at a premium, is seriously under-utilized in the Highway 111 corridor and that these lands could yield a substantially greater share of the local retail and restaurant market. Numerous traffic studies prepared for the City have elaborated on the impacts currently generated by land use and highway access patterns.

Other characteristics of parcels occurring within the study area were also assessed to determine the degree to which they suffered from economic dislocation, deterioration, or disuse because of one or more of the following factors:

- Previous subdivision and sale of lots of irregular form, shape or inadequate size for proper usefulness and development.
- Previous subdividing of parcels with inadequate regard for contours, local topography or physical characteristics of the ground and surrounding conditions.
- Existence of inadequate public improvements, public facilities, open spaces, or utilities which cannot be remedied by private or governmental action without redevelopment.
- Prevalence of depreciation or stagnated value, impaired investment, or social or economic maladjustment.



Conclusions of the survey, which applied the above described criteria to ranking the approximately 353 parcels in the study area, yielded the following results: 62 parcels (17.5%) were ranked No. 1 (very good conditions; no maintenance necessary); 47 parcels (13.3%) were ranked No. 2 (acceptable to poor conditions; minor to major aesthetic and structured maintenance necessary); and 70 parcels (19.8%) were ranked No. 3 (unacceptable to dangerous conditions; major aesthetic and structured maintenance necessary). Of the total parcels surveyed, a total of 174 (49.3%) were vacant.

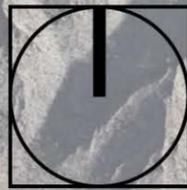
These findings provided the basis of a redevelopment program and this Specific Plan.



CITY OF RANCHO MIRAGE

**EXHIBIT 01**  
Aerial Photo

■ ■ ■ ■ ■ HIGHWAY 111 WEST SPECIFIC PLAN BOUNDARY



1" = 500'





### PLANNING OBJECTIVES

- The Highway 111 West Specific Plan took as objectives the following:
- To provide focused opportunities for economically productive business and commercial enterprises as well as residential development;
- To establish land uses, circulation design treatments, site development standards and design guidelines which contribute to the preservation and enhancement of adjacent residential environments where they abut the Corridor;
- To provide for the scale and nature of uses which can make the most efficient use of the existing development pattern as well as parcel size and shape;
- To achieve gradual improvement of older uses through expansion of use, redevelopment, consolidation of parcels, rehabilitation, landscaping, design improvements or any combination of these devices desired by the property owner(s) and supportable by the City;
- To contribute to the improvement of both visual and functional attributes of the Corridor to help stimulate business activity;
- To expand the traffic handling capacity of arterial highways, local streets, alleys and driveways that serve the area to accommodate new development and redevelopment or rehabilitation of existing areas; and
- To establish a sense of place, as well as continuity and consistency of development standards within the Corridor.

### OPPORTUNITIES AND CONSTRAINTS

With the inventory of existing physical conditions along the corridor as a framework, the opportunities and constraints of the planning area were mapped.

#### Conditions of Improvements

The categories of existing conditions mapped were; very good, acceptable to poor, and unacceptable to dangerous. These categories were assigned to parcels within the planning area based on field surveys by Terra Nova Planning and Research, Inc. in conjunction with the Rancho Mirage Redevelopment Agency.

The most immediate opportunity for redevelopment exists in the form of vacant land in the planning area. Development projects in these areas can be easily planned and processed because the vacant status reduces, in a relative sense, the political problems of land acquisition. Acceptable to poor, and poor to dangerous land uses provide less immediate redevelopment opportunities but do serve as opportunities for longer term use intensification

#### Irregular Lots/Conflicting Access

Small and irregular lots are identified on Exhibit 2. They represent opportunities for lot consolidation for the purpose of promoting coordinated, high quality redevelopment projects (discussed in Section 5, Chapter 15 of the Specific Plan).

One result of lot consolidation is the elimination of excess conflicting access points on Highway 111. In addition, the establishment of parking collector roads leading to parking plazas further reduces the need for direct 111 access points for individual commercial establishments. The reduction of direct access points will result in improved through-traffic circulation.

#### Billboards and Overhead Utilities

The elimination of these undesirable street scape elements is an immediate action that can be taken to improve the corridors appearance. Undergrounding of utilities shall be done in accordance with the regulations found in Section III, Chapter 5 of the Specific Plan.



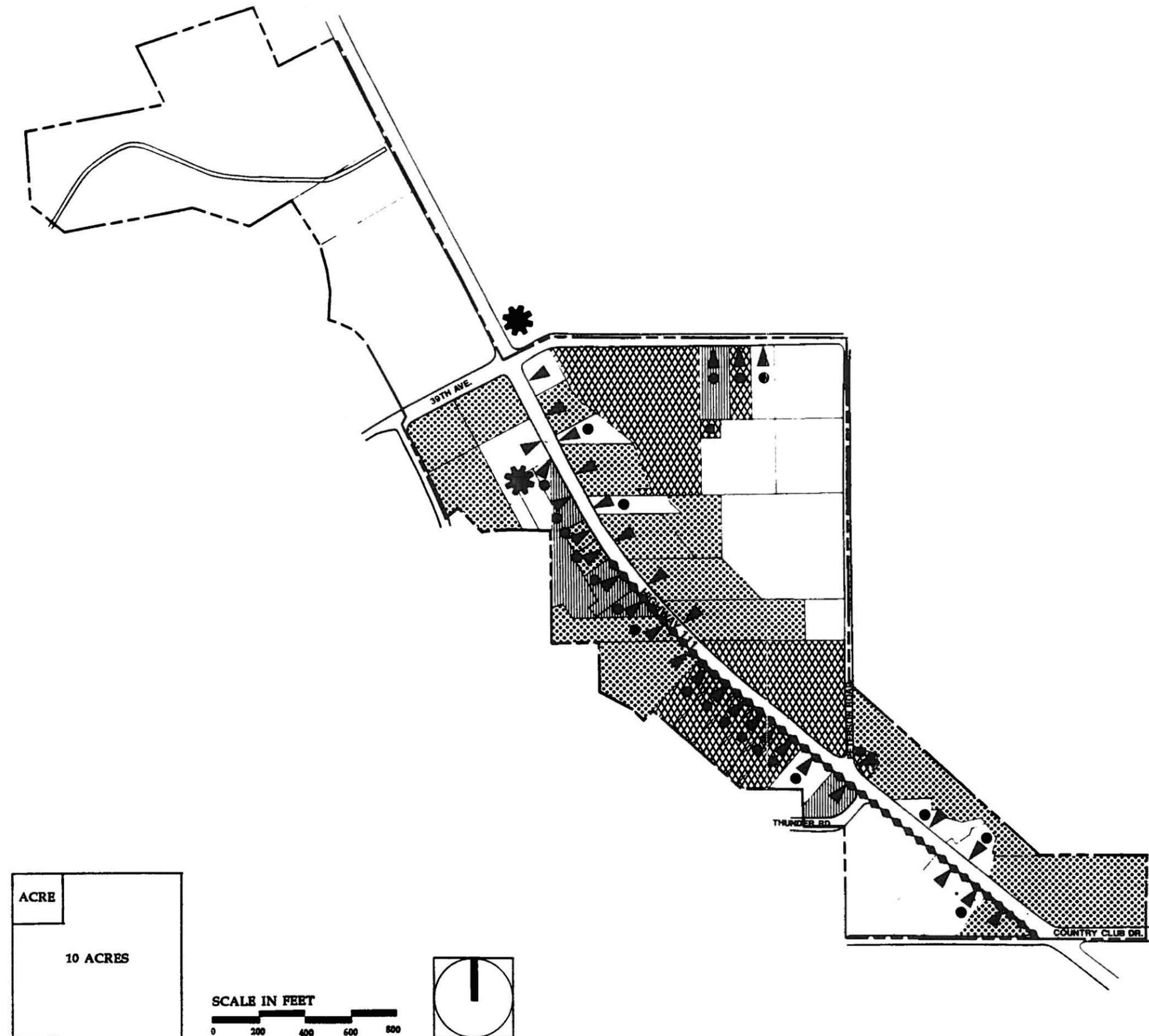
CITY OF RANCHO MIRAGE

## EXHIBIT 02

Opportunities and Constraints

-  OVERHEAD UTILITY LINES
-  BILLBOARDS
-  CONFLICTING ACCESS POINTS
-  SMALL/IRREGULAR LOTS
-  VERY GOOD CONDITION\*
-  ACCEPTABLE TO POOR CONDITION\*
-  UNACCEPTABLE TO DANGEROUS CONDITION\*
-  VACANT

\* Note:  
Source of Data:  
Terra Nova/RM Redevelopment Agency





## REGULATIONS AND GUIDELINE FRAMEWORK

This Specific Plan contains all the necessary information on the planning area and guidance toward implementation. It is designed to facilitate use by those who wish to build projects in conformance with the plan, staff personnel who must review project proposals and decision makers who are called upon to act on these projects.

### Contents and Sequence

1. The Introduction presents the project area to the document user by identifying its location and presenting statements of authority, CEQA compliance, purpose, goals and objectives, and the process for development plan review.
2. The Design Concepts section sets forth the plan concepts for land use, circulation, and landscaping. This section establishes the criteria upon which the development regulations are based and the “planning intent” against which proposed development projects will be judged.
3. The Development Regulations section states the specific development standards that must be observed by all projects. It covers permitted uses, building regulations and area wide standards.
4. The Design Guidelines section describes in narrative and illustrates in diagrams the design aspects of development which need be addressed by each development project to assure the quality intended by the plan.
5. The Programs and Incentives section describes the criteria for evaluating project responsiveness to the Specific Plan and subsequent incentive program actions that will encourage greater responsiveness. This section also includes a discussion of shared parking area possibilities.

## PROCESS OF DEVELOPMENT PLAN AND DESIGN REVIEW

### Objectives

The Highway 111 Corridor Specific Plan shall be implemented through the use of Development Plan Review. A development plan shall be required for all rehabilitation, redevelopment, expansion or intensification of existing use or structures and new development within the Specific Plan area. A development plan will not be required for rehabilitation of a structure where there is no square footage increase or use intensification. The applicant may opt for development plan review where it is not required herein. This requirement is instituted for the following reasons:

1. To ensure consistency with the intent of the Specific Plan.
2. To encourage innovative community design and development.
3. To assure substantial long range compliance with the Redevelopment Plan and General Plan.
4. To promote the highest contemporary standards of development design.
5. To adapt specific or special development conditions that occur from time to time while continuing to implement the Specific Plan.
6. To facilitate complete documentation of land use entitlements authorized and conditions pertinent thereto.

### Procedures

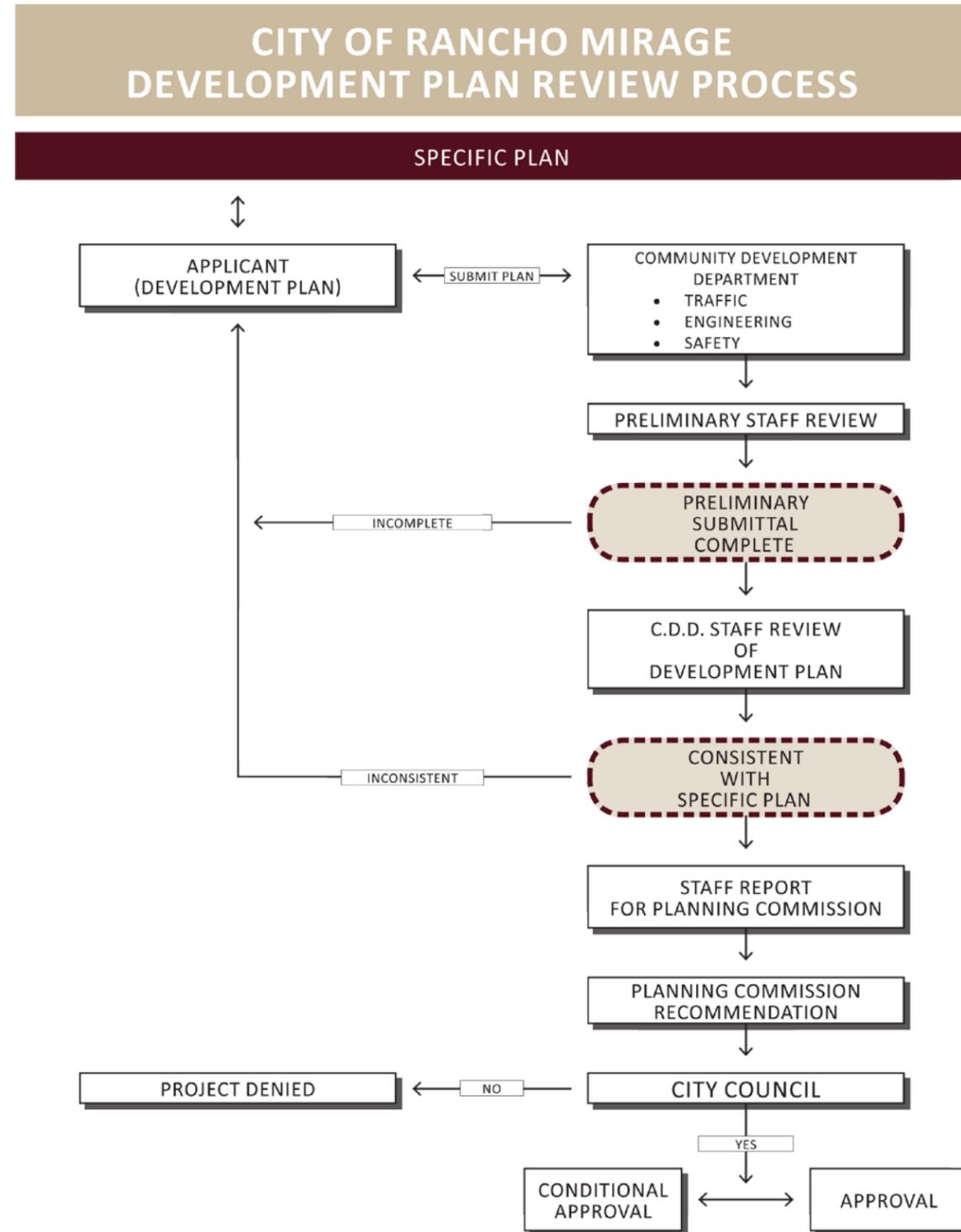
Development plans, which contain plans, drawings, illustrations, designs, reports and other detailed information as required herein, shall be submitted to the city staff for review and comment. Applicants are encouraged to submit preliminary plans for review and comment by the Community Development Department prior to the final preparation of a development plan. Comment from other City Departments and service agencies shall be sought by the staff prior to preparing a recommendation of the finalized development plan.

Applicants should insure that they familiarize themselves with the Design Guidelines in this Specific Plan. This will assist the developer in achieving consistency with the Specific Plan and generally facilitate project processing.

Upon determination that the development plan complies with the provisions of the Specific Plan, the staff shall prepare a staff report with recommendations which shall be submitted along with the development plan to the Planning Commission at the earliest possible regular meeting. The Planning Commission shall recommend to City Council the approval, denial or conditional approval of the development plan.



TABLE 1. DEVELOPMENT PLAN REVIEW CHART



**Requirements for Development Plan**

A development plan shall consist of plans, drawings, illustrations, designs, reports and other detailed information as required to determine compliance with the provision of the Specific Plan and responsiveness to design guidelines. The following list of plans and information is required:

1. All applicable tentative tract maps or tentative parcel maps.
2. A legal and physical description of the site, including boundaries, easements, existing topography, natural features, existing buildings, structures and utilities.
3. Location, grades, widths and types of improvements proposed for all streets.
4. A development plan showing location of all structures, landscape and hardscape areas, parking areas, walks, internal circulation, access, adjacent streets, sign type and placement and fence-wall type and placement.
5. Description of the extent to which Specific Plan Design Guidelines have been used in the plan.
6. A tabulation of square footage, area devoted to signage, parking, landscape coverage, building heights and number of residential units.
7. Such other items and applications as may be requested by the City staff.

**Specific Plan Consistency Check**

The Specific Plan is intended to allow for design flexibility, yet it does provide certain direction that is quite critical.

A development plan’s consistency with the Specific Plan is evaluated by checking it against the three sections of the Specific Plan that regulate development: Design Concepts, Development Regulations, and Design Guidelines. This process is described below.

The initial screen for plan consistency is consistency with the land use and design concepts. The Specific Plan Section II, Chapter 2 (Plan Structure and Design Concepts) divides the 111 West area into seven distinct planning districts, dictated by existing land use and physical constraints. The program for a proposed project



must be consistent with the stated land use and design concepts that have been established for the project's Planning District.

The second check for plan consistency will be the extent of project compliance with the Development Regulations of Specific Plan Section III, Chapter 5. This chapter provides a permitted use matrix which will determine if a proposed project is permitted, conditionally permitted, or prohibited. There is also a building regulation matrix that sets forth detailed building envelope regulations per Planning District that the project shall adhere to. Finally, there are area wide standards that apply to all Planning Districts.

The third check for plan consistency is the extent to which the Design Guidelines (Section IV, Chapters 6 through 13 of the Specific Plan) are adhered to.

#### **Project Inconsistency**

If a proposed project does not fully respond to the Specific Plan priorities stated above, the project applicant is encouraged to consult with city planning staff to discuss some of the approved incentives that may be employed to achieve specific plan consistency. These incentives are found in Specific Plan Section V, Chapter 14. This latter point is not a basis for rejecting a proposal, given compliance with regulations, but it may lead to conditions which need to be placed on projects or design changes that need to be accomplished.

If an applicant cannot obtain a judgment of consistency from Staff in accordance with existing code provisions, the applicant's options shall be a Request for Variance or an Appeal to the Planning Commission. These actions shall be in accordance with Chapter 17.50 (Hearings and Appeals) of the Zoning Code.

#### **Mandatory Findings for Approval of a Development Plan**

The City Council shall make the following written findings before approving or conditionally approving any development plan.

1. The proposed project is compatible with the Specific Plan.
2. The plan will not have an adverse impact on the public health, safety, interest, convenience or general welfare.
3. The development plan is compatible with the purpose and intent of the Highway 111 Corridor Design Guidelines. A determination of compatibility should address all of the following in writing:

- technical development standards
- all statements of purpose and intent
- goal and objective statements

#### **Applicability**

Approval of a development plan shall be required prior to or concurrent with a Tentative Tract or Parcel map for all proposed projects within the Study Area. Where no Tentative Tract or Parcel map is required, approval of a development plan shall occur prior to issuance of building permits. Authority for approval of development plan shall rest with the City Council.

#### **Environmental Determination**

The Development Plan Review process is discretionary, not ministerial, and is therefore subject to the requirements of the California Environmental Quality Act (CEQA).

The Environmental Impact Report (EIR) prepared for this Plan, in accordance with the California Environmental Quality Act (CEQA), will apply to future development plans, tentative tract maps, parcel maps and any other development processed in conformance with the 111 West Specific Plan. Future development projects that are consistent with this Specific Plan will require either no further environmental documentation or, in special cases, focused environmental analysis and action pursuant to Section 15162 of the CEQA Guidelines.

#### **Time Limitation**

Development plan approval in accordance with this policy, shall be valid for a period of one year plus reasonable extensions pursuant to the Municipal Code. If construction of a project does not commence within that period and proceed with due diligence thereafter, the approval of the development plan shall terminate and an additional submittal, review and approval will be required.

#### **Plan Monitoring**

While the Plan does impose certain regulations on individual projects, it is mainly a guide and stimulus to development in the Corridor. Just as many pressures have resulted in recent changes in the Corridor, numerous anticipated and unanticipated changes will occur in the future. It is important, from time to time, to evaluate what is taking place, how it compares to the Plan and whether or not the Plan needs to be changed.

#### **Amendments to the Specific Plan**

Minor revisions to the Specific Plan shall be approved by the City of Rancho Mirage Community Development Director.

A major amendment to the Specific Plan will require review and approval by the City of Rancho Mirage Community Development Director, the Planning Commission, and the City Council. Such major amendments are governed by the California Government Code, Section 65500, which requires an application and fee submitted to the Community Development Department stating in detail the reasons for the proposed amendment.

# Section 2

## DESIGN CONCEPTS

### 2. Plan Structure and Design Concepts

The Highway 111 Specific Plan is designed to facilitate rational development and redevelopment of the Highway 111 Corridor Area through the City of Rancho Mirage from Braemar Drive to Country Club Drive within the Whitewater Redevelopment Area. This plan does not imply, require or prohibit the use of eminent domain proceedings relating to any development activity within the Specific Plan Area.

The major function of this portion of the Plan is to establish the rationale for the development regulations. **It constitutes the policy section of the Plan.** By herein explaining the various plan concepts, the rationale for Section Three, Development Regulations, will be more understandable.

This chapter will serve as the first point of reference for determining whether or not, and to what degree, a proposed project is consistent with the Specific Plan. Subsequent chapters spell out other components of the plan which work together to fulfill the stated goals and objectives.

#### LAND USE PLAN

The Conceptual Land Use Plan consists of an arrangement of uses within defined “planning districts”. The boundaries of the individual districts and their associated land use and design concepts are derived from physical constraints and existing land use conditions as determined by the inventories conducted by Terra Nova Planning and Research, Inc. in association with the City Redevelopment Agency.

**The Conceptual Land Use Plan** is shown in Exhibit 3. The general arrangements of commercial, office and residential land uses are shown, including parking plaza areas, open space and Planning District boundaries. The exhibit is annotated with description statements that call out the major land use concepts for each planning district.

In the **Summary of Uses** section below, Table 2 lists the planning districts and summarizes their primary and secondary intended uses.

In Exhibit 4, the **Illustrative Plan**, the major design concepts for the planning area are shown graphically and called out with description statements. In the District Design Concepts section that follows, each Planning district is discussed individually and is accompanied by detailed illustrative exhibits.

#### SUMMARY OF USES

Depicted in the table below are the Land Use Districts, each with a primary basic use, and secondary uses when and where such uses are also acceptable.

**TABLE 2. LAND USES BY DISTRICT**

District	Adjusted Gross Acres	Net Buildable Area	Primary Use	Secondary Use
1	8.7	2.2	Dinner House Restaurant	Design Oriented Retail Commercial
2	10.6	2.6	Restaurants	Design Oriented Retail/Office
3	20.1	5.0	Attached Residential	Tennis Resort/ Residential Hotel
4	8.3	2.1	Mobile Home Park	Retail Commercial
5	4.2	1.1	Professional Office	None
6	9.6	2.4	Retail Commercial	Professional Office
7	23.7	5.9	Retail Commercial	Professional Office
<b>TOTAL</b>	<b>85.2</b>	<b>21.3</b>		

\*Adjusted gross acreages are total gross district area minus the street right-of-ways. The net buildable area figures were derived from the adjusted gross figures, calculated by assuming an average lot coverage of 25 percent. This is consistent with past coverage experience in Rancho Mirage given parking, landscape, and setback requirements.

#### DISTRICT DESIGN CONCEPTS

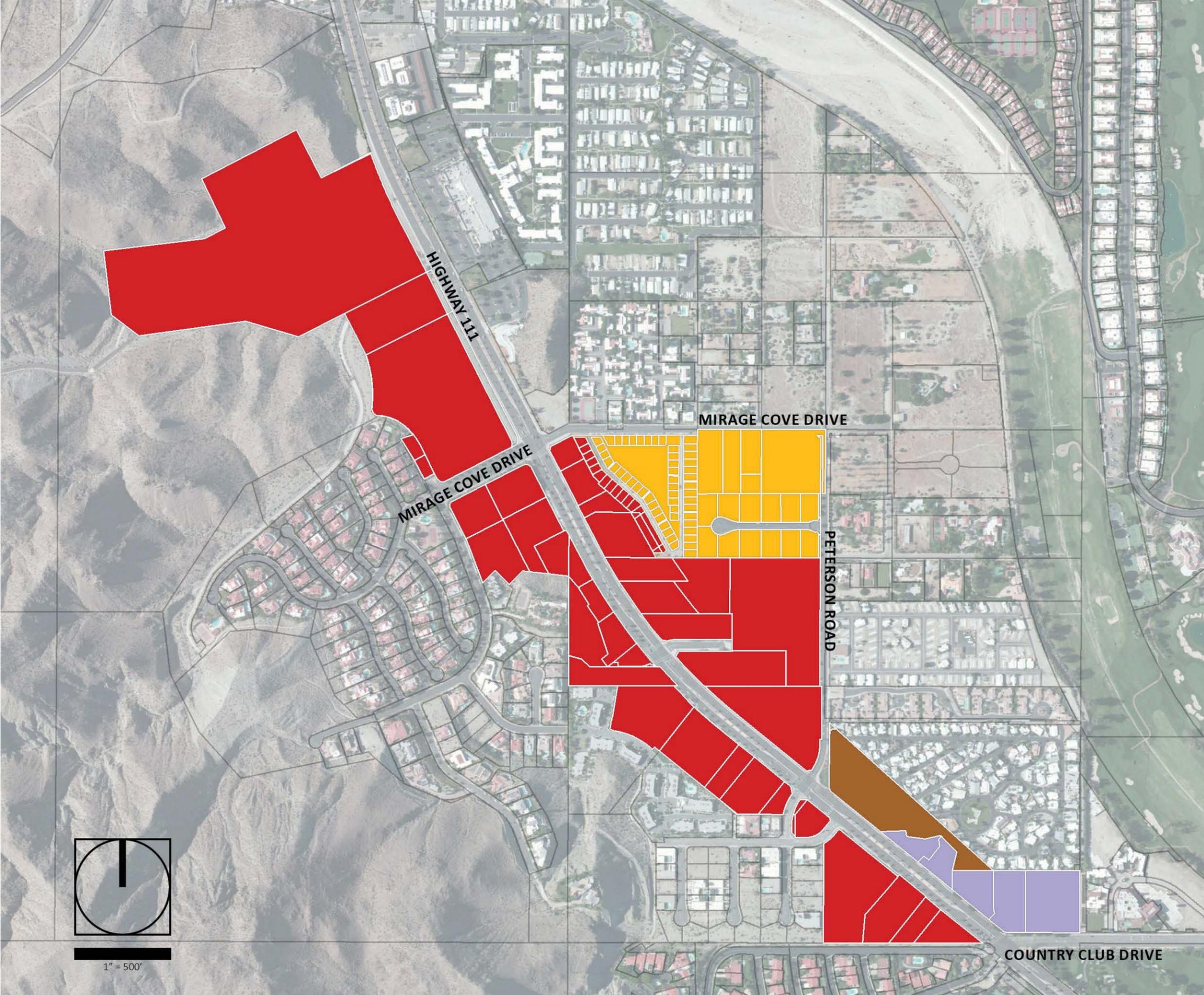
The Design and Land Use Concepts presented below identified the location, existing conditions, and land use concepts that have been established for the seven different Planning Districts of the 111 West Planning Area. These District concepts set forth the recommended land use types and Urban Design themes that should characterize the initial Phases of Development Projects. The accompanying illustrative exhibits are included to visually communicate the desired, landscaping, circulation and lot configurations.



CITY OF RANCHO MIRAGE

**EXHIBIT 03**  
Conceptual Land Use Plan

- COMMERCIAL
- RESIDENTIAL
- MOBILE HOME PARK
- OFFICE





**DISTRICT 1**

*RESTAURANT ROW • 8.7 GROSS ACRES  
DINNER HOUSE RESTAURANT USE*

**Location**

Located at the center of the Specific Plan area on the north side of 111. The district stretches from 39th Avenue on the west, to the mobile home park on the south. The five sit-down restaurants establish the well-defined character of this district (see Exhibit 5).

The three additional sites, identified in the Illustrative Plan and in Exhibit 5, are envisioned to provide opportunities for additional high-quality dining establishments.

**Existing Conditions**

Existing conditions which define this district and provide potential for design enhancements are:

- The existing restaurants fronting Highway 111.
- The vacant parcel at the southeast corner of Highway 111 and 39<sup>th</sup> Street provides a potential restaurant site provided that consolidated parking can be achieved.
- The vacant parcel adjacent to the northern restaurant serves as a possible expansion site or a new dining establishment.
- The vacant strip-parcel at the center of the district may serve as a potential alignment for a parking collector road.
- The open space to the rear of the existing parking present opportunities for additional parking and another restaurant site or compatible use.
- Current parking facilities can be simply redesigned to create additional parking space and more efficient parking collector roads.

**Land Use Concept**

The land use concept proposed for the Restaurant Row District is one of the “high end” dining establishments in a highly accessible and compact area. The centralized location of this well established, specialized district makes it an ideal location for drawing pedestrian traffic from nearby commercial and

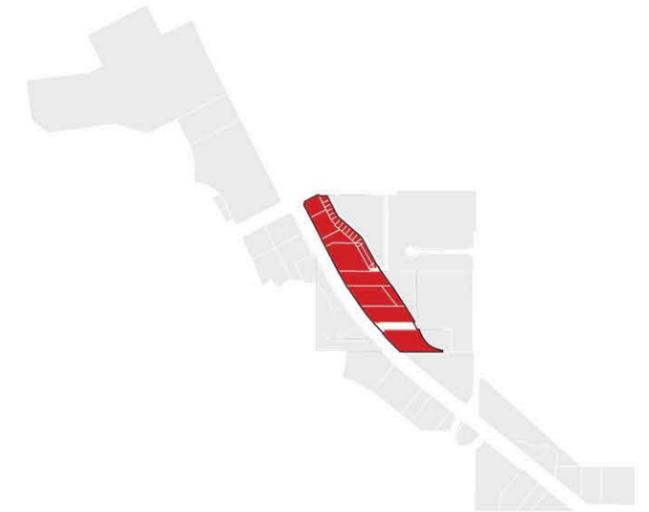
residential districts (Planning Districts 2 and 3, respectively). In addition, intensification and enhancement of these existing uses will increase the customer draw from the surrounding communities. Peak restaurant traffic hours should not conflict with peak commuting traffic. The parking collector road and redesign of the parking areas will produce more efficient traffic flows and create additional parking capacity.



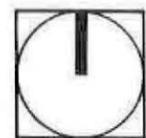
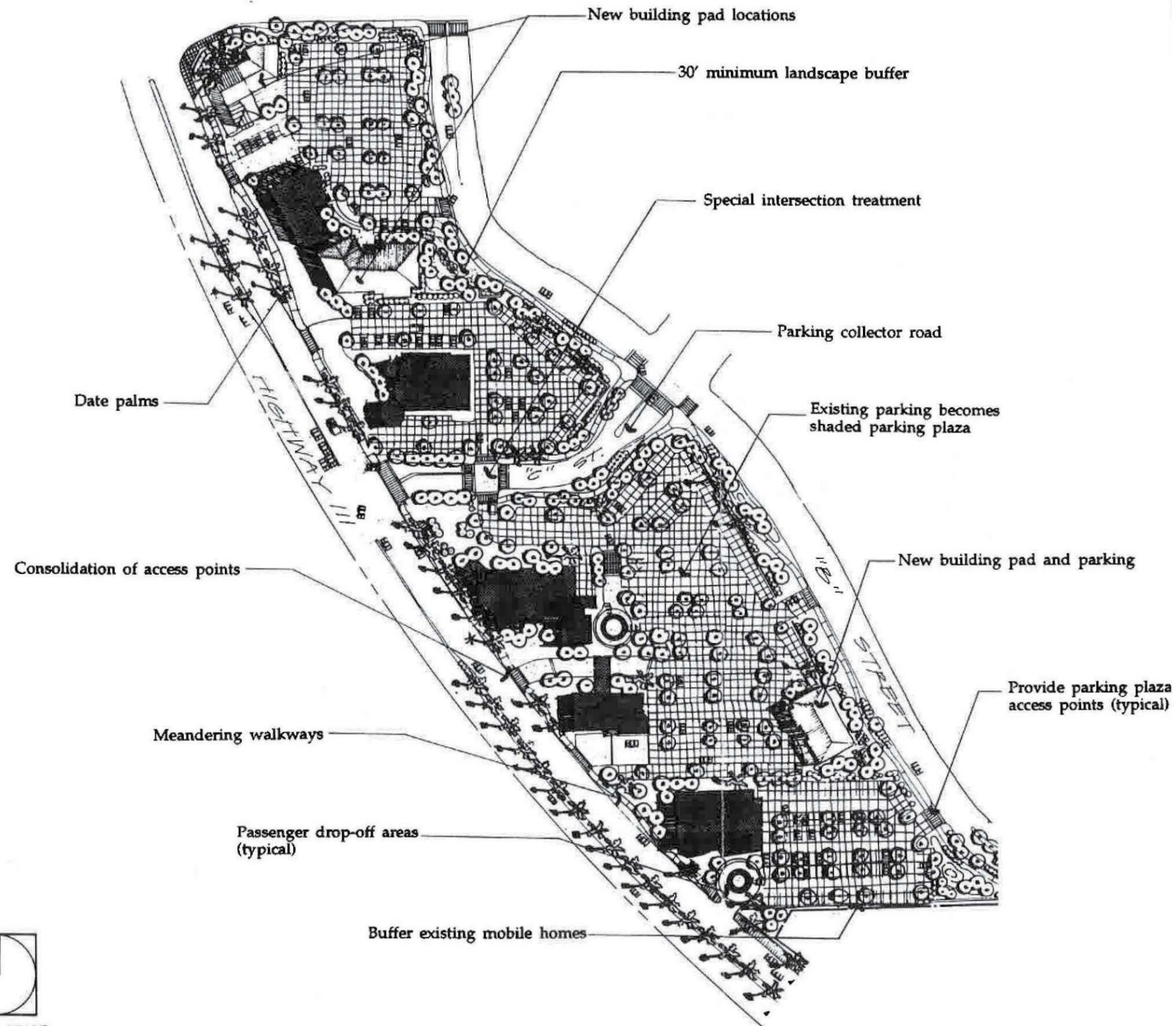
CITY OF RANCHO MIRAGE

### EXHIBIT 05

District 1 Concept  
"Restaurant Row"



■ COMMERCIAL



NOT TO SCALE

**DISTRICT 2**

RESTAURANT/DESIGN CENTER • 10.6 GROSS ACRES  
RESTAURANTS/DESIGN OFFICES

**Location**

This district is located on the south side of Highway 111 and extends from Mirage Cove Drive south to that point across from the western edge of the mobile home park (see Exhibit 6).

**Existing Conditions**

Existing conditions which define this district and provide potential for future improvements are:

- A prominent foothill/rock outcropping that roughly bisects the site. This physical feature provides a dramatic natural backdrop for existing and future land uses. It provides potential for architectural, landscape and lighting treatments that will create a strong identity for this planning district, and the 111 west planning area as a whole.
- Existing commercial office and retail uses in the west end of the area are of high quality.
- Two existing dinner house restaurants provide a land use link to the Restaurant Row district directly across the street.
- A vacant parcel and parcels supporting substandard uses are located at the base of the hill. These parcels provide opportunity for redevelopment in the future.

**Land Use Concept**

The land use concept envisioned for The Restaurant Design Center is a mix of design oriented office and retail uses combined with restaurant uses. This land use concept is derived from the existing uses.

The inclusion of design oriented commercial uses with restaurant uses differentiates this district from the Restaurant Row district across the street and will result in a more active streetscape during the day.

In order to achieve this concept, new development and intensification must occur in specific areas. The vacant parcel in the west end of the district should support additional design oriented uses such as interior design or architecture offices,

galleries, or high quality home furnishing stores. A restaurant fronting along Highway 111 and integrated into the office or retail development would be appropriate. The buildings located at the base of the foothill outcropping should be upgraded by redevelopment.



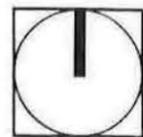
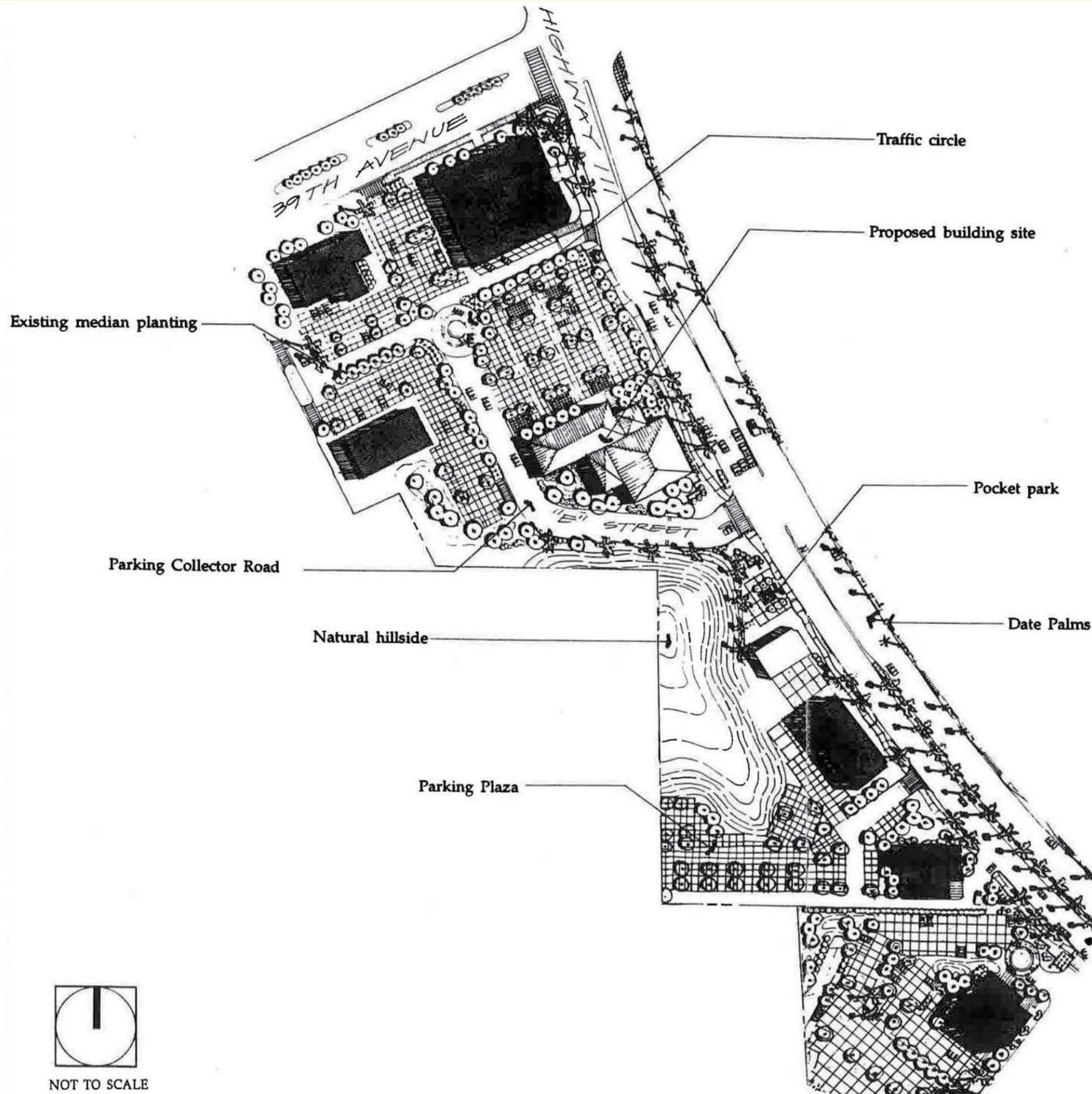
CITY OF RANCHO MIRAGE

### EXHIBIT 06

District 2 Concept  
"Restaurant/Design Center"



■ COMMERCIAL



NOT TO SCALE



### **DISTRICT 3**

*ATTACHED RESIDENTIAL • 20.1 GROSS ACRES*

*ATTACHED RESIDENTIAL*

#### **Location**

The location of this district is in the center of the planning area at the southwest corner of the Peterson Parkway/39th Street intersection. The area has a potential for medium density residential development as its main feature; low density would also be appropriate should the market demand it (see Exhibit7).

#### **Existing Conditions**

Existing conditions which define this district and potential for design enhancement are:

- large parcels of vacant land which can accommodate residential development
- existing apartment units
- abandoned date groves

#### **Land Use/Design Concepts**

The land use concept envisioned for the Residential District is development of attached homes at a density that does not overdevelop the site. The construction of a planned unit development with a medium density (5.0 dwellings/acre, gross) also provides an opportunity to enhance the quantity and type of residential development in the district. This area seeks to provide additional housing units in the City, as well as to further intensify the types of residential development currently in or around the planning district.

Pending market conditions, a secondary use of a tennis resort or residential hotel may be deemed appropriate if developed at an intensity that is equivalent to the allowable residential density. An additional environmental study for such use will be required and other documentation and/or standards may be imposed by Community Development Department Staff.

In any event no residential development shall be approved that exceeds 5.0 dwelling units per gross acre as set forth in the City Zoning Code.

In order to achieve this concept, the buffer roadway located at the back of the Restaurant Row District will serve at the primary access into the residential development. The monumentation and

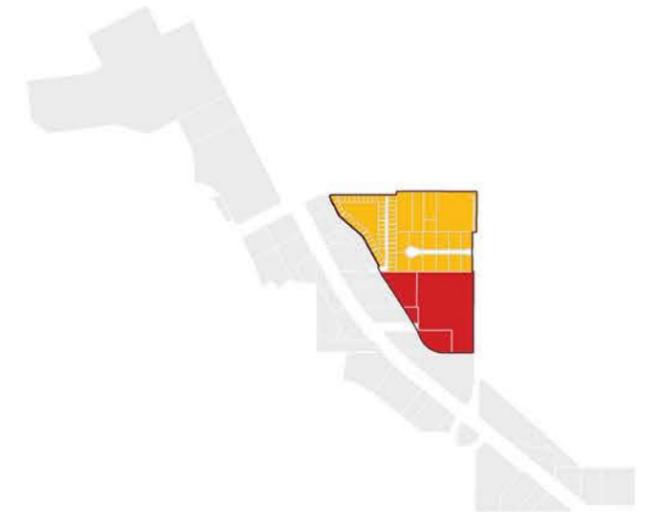
the design features at the entrance to the residential will differentiate and delineate the residential use from the adjoining restaurant and commercial uses.



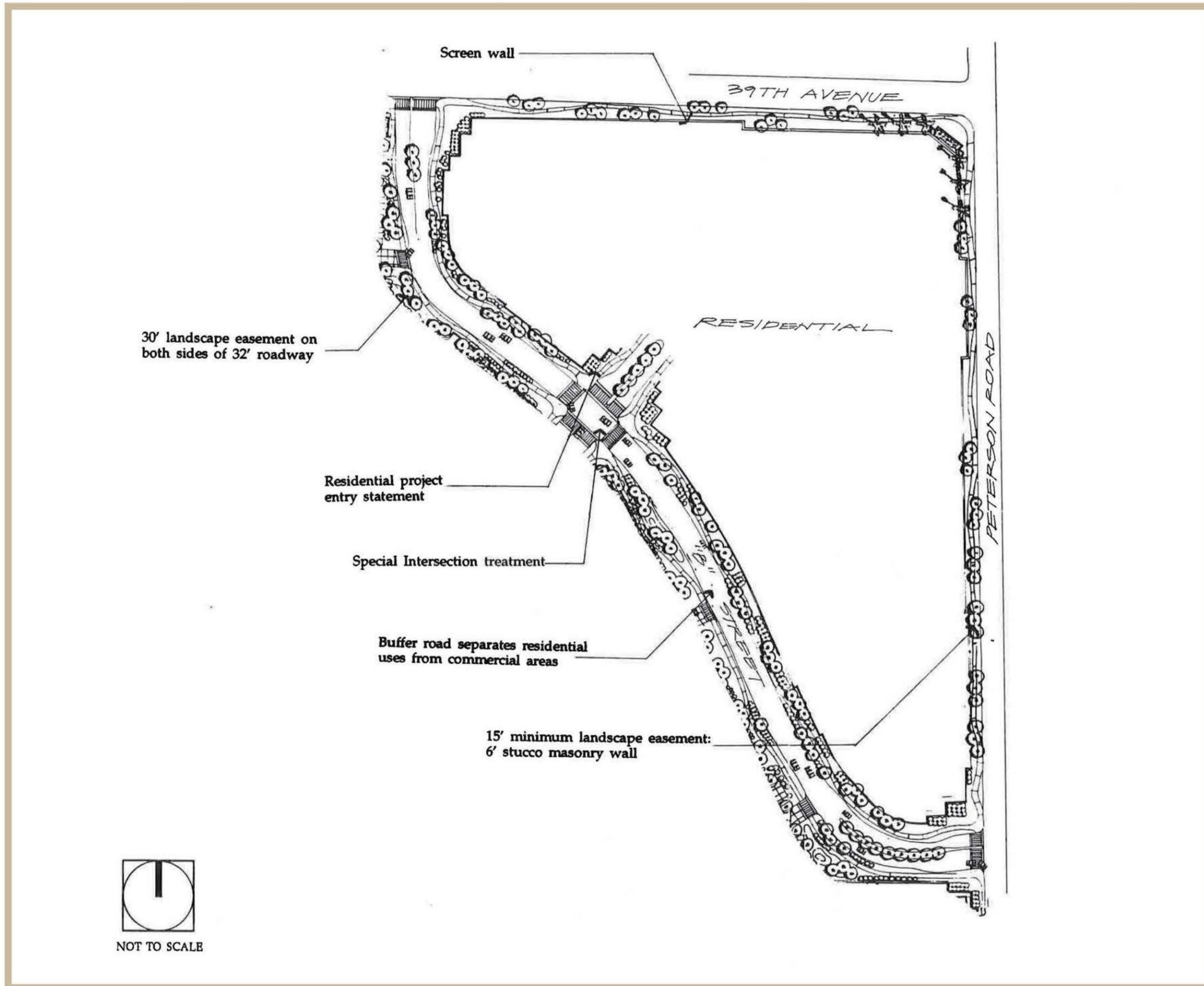
CITY OF RANCHO MIRAGE

### EXHIBIT 07

District 3 Concept  
"Attached Residential"



■ COMMERCIAL  
■ RESIDENTIAL





#### **DISTRICT 4**

*MOBILE HOME PARK • 8.3 GROSS ACRES*

*MOBILE HOME PARK*

#### **Location**

District 4 is located on the north side of Highway 111 and extends along the eastern two-thirds of the Planning area (see Exhibit 8).

#### **Existing Conditions**

Existing conditions which define this district and provide potential for design enhancement are:

- Two existing mobile home parks that border Highway 111. They are to continue operations in their current locations.
- The required realignment of Highway 111 will provide the opportunity to improve the wall and landscaping that screens the mobile homes from 111.
- The nursery located at 111 and Peterson Parkway provides visual interest and street activity in the otherwise “dead” section of the 111 streetscape.
- Existing open space between the east mobile home park and 111 provides a location for a street-side pocket park.

#### **Land Use Concept**

The designed concept envisioned for the Mobile Home Park District focuses on screening the residential uses from Highway 111, and creating a pocket park with a water feature as a link to the Commercial East District directly across the street.

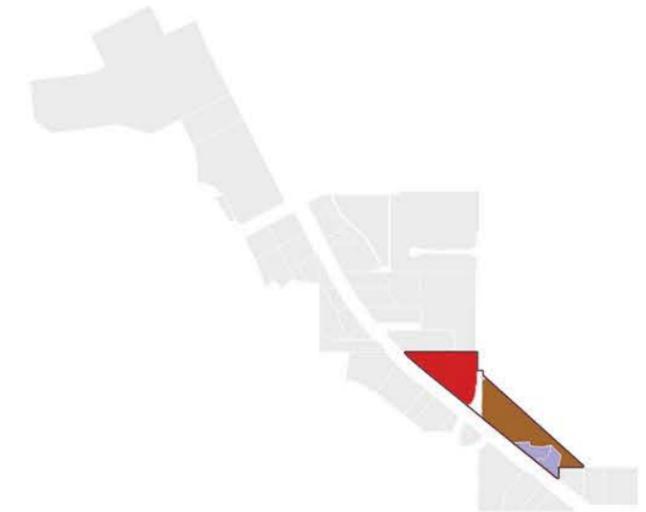
To achieve these land use and design goals, the Specific Plan Design guidelines must be utilized during the realignment of Highway 111 and the construction of the pocket park.



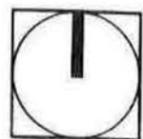
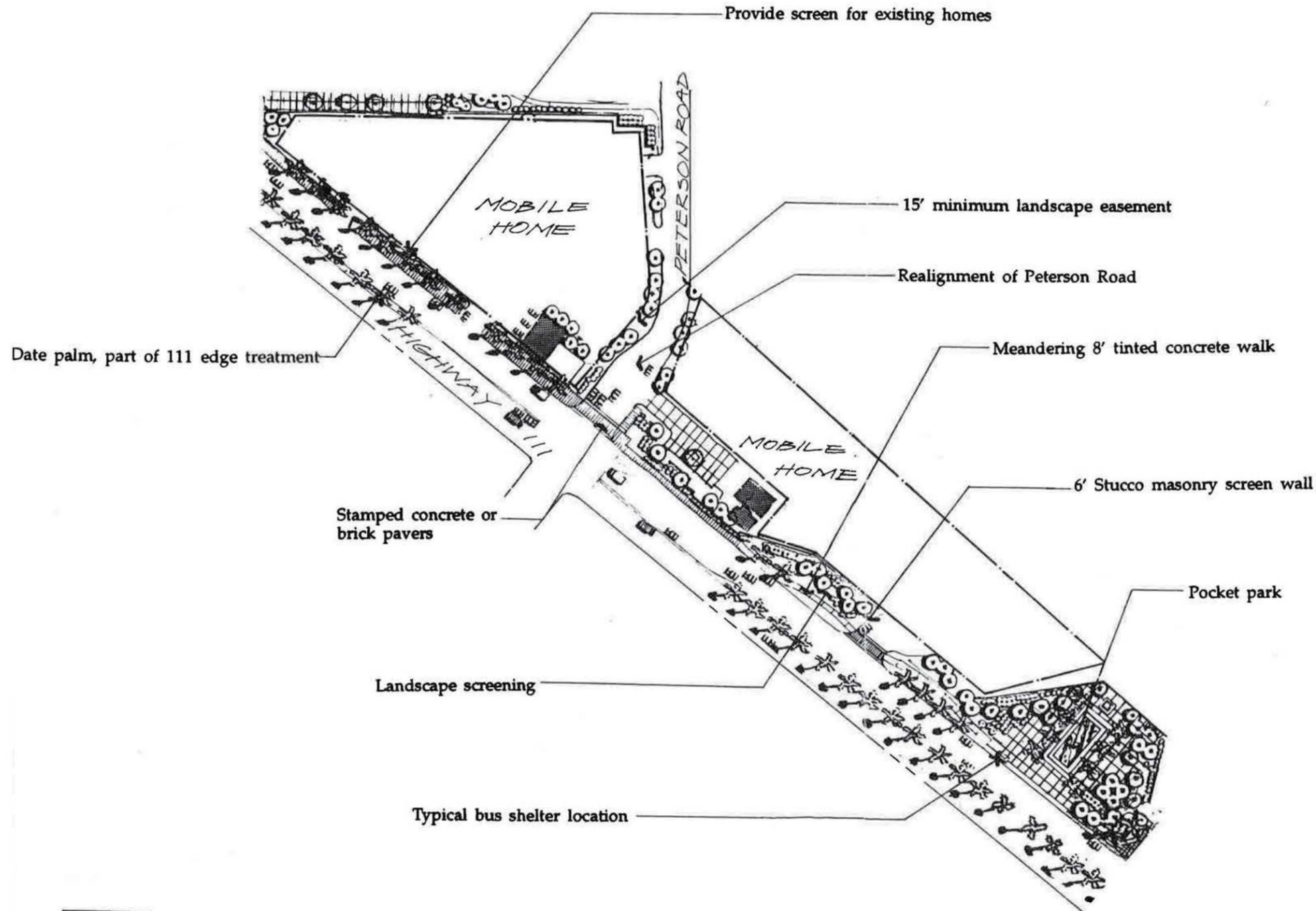
CITY OF RANCHO MIRAGE

### EXHIBIT 08

District 4 Concept  
"Attached Residential"



- COMMERCIAL
- MOBILE HOME PARK
- OFFICE



NOT TO SCALE



**DISTRICT 5**

*OFFICE CENTER • 4.2 GROSS ACRES*

*PROFESSIONAL OFFICES*

**Location**

The location of this district is at the east end of the planning area at the north side of Highway 111 at Country Club Drive (see Exhibit 9).

**Existing Conditions**

Existing conditions which define this district and provide potential for design enhancement are:

- Existing commercial office use supports a financial institution of high quality design.
- Vacant land adjacent to existing office use.
- Easily identifiable location (Highway 111 at Country Club Drive).

**Land Use Concept**

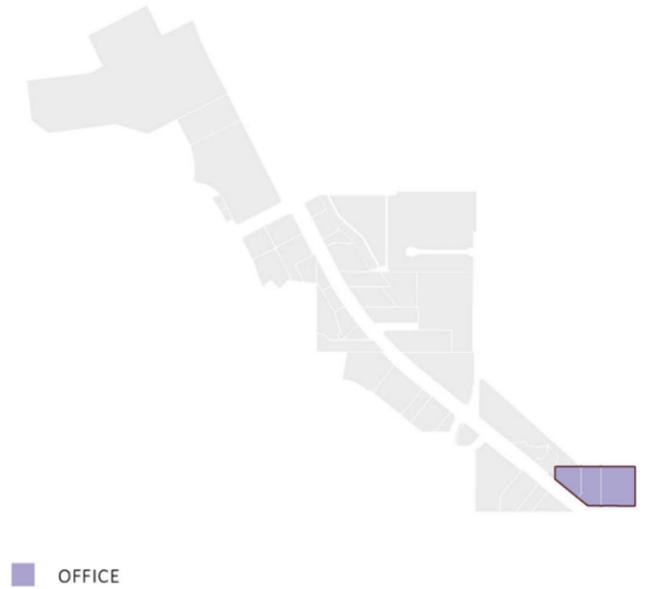
The land use concept envisioned for the office district is one of high quality, well maintained office uses and expansion of existing use. Primary consideration will be given to architectural compatibility with adjacent office building and the provision of adequate landscaping.

In order to assure that the existing high quality office character is maintained, future development must adhere to the Development Regulations and Design Guidelines of the Specific Plan.

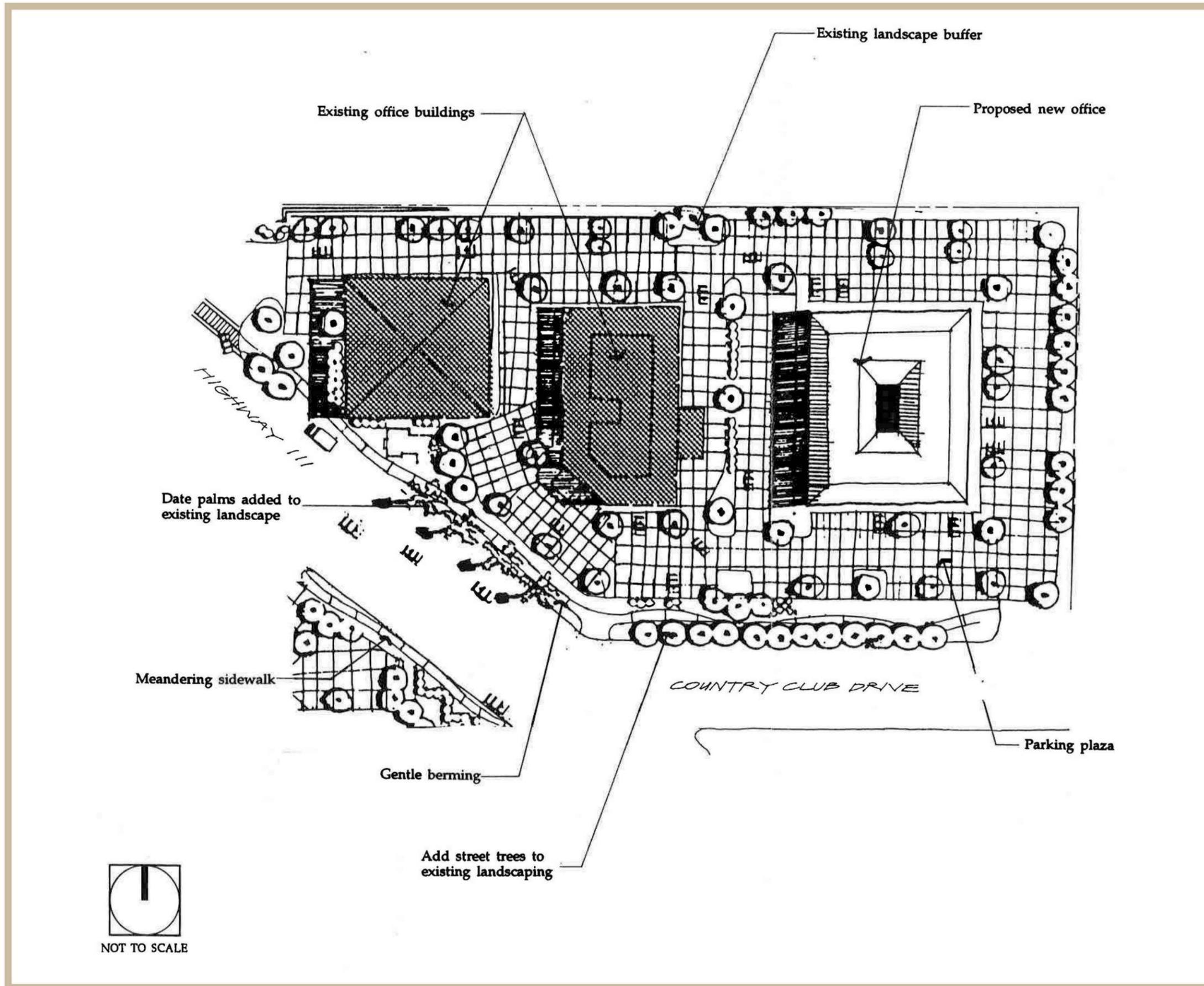


CITY OF RANCHO MIRAGE

**EXHIBIT 09**  
District 5 Concept  
"Attached Residential"



OFFICE



**DISTRICT 6**

COMMERCIAL EAST • 9.6 GROSS ACRES  
COMMERCIAL RETAIL

**Location**

Planning District 6 is located along the south side of Highway 111 at the east end of the planning area (see Exhibit 10).

**Existing Conditions**

Existing conditions which define this district and provide opportunities and constraints for land use and design improvements are:

- Vacant Land. A large portion of the commercial east entry district is made up of a large vacant parcel. This parcel is large enough to support a reasonable size commercial development.
- Existing uses include a restaurant and a service station.
- Commercial uses at the west end of the district provide opportunities for intensified uses and redevelopment.
- Awkward alignments of Peterson Parkway and Thunder Road provide the opportunity for realignment and circulation improvements.

**Land Use Concept**

The land use concept envisioned for the Commercial East district is that of an active commercial retail area. The intent is to establish an open and heavily landscaped retail plaza that will provide visual excitement and an obvious sense of arrival to a unique area in Rancho Mirage. The vacant land at the east end of the district is suited for a coordinated commercial project that may support a major retailer, movie theaters, and associated support retail uses. Existing open space located directly across Highway 111 is a type of “front lawn” for the mobile home park, it is intended to support a large pocket park with water feature. This park will enhance the entry statement. New development across from the pocket park should be well set back from the Highway 111 right-of-way in order accentuate the open space effect of the park (as shown in Roadway Landscape Section A1-A1, Exhibit 15).

As a contrast to the openness of the commercial entry area, the scale of the streetscape becomes somewhat enclosed as one moves westward through the planning area.

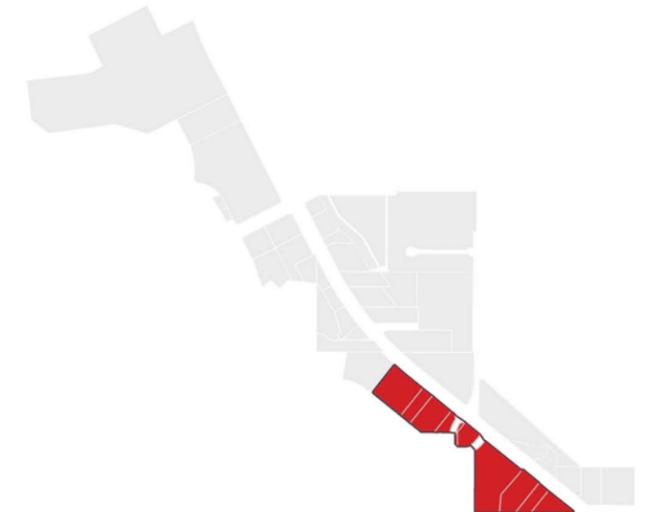
To achieve this concept, new commercial buildings should be placed to the rear of the east end vacant parcels. Existing commercial uses in the districts west end should be intensified through redevelopment. New uses here should be located as close to Highway 111 as possible and be grouped close together. Specific design guidelines for landscape, architecture lighting, and signage are provided to enhance quality and assist in defining district identity.

Full access to existing uses will be maintained, enhanced and incorporated within parking plaza design for new uses. Efforts should be made to consolidate curb cuts during expansion or remodeling of existing uses or during conversion of non-conforming uses to conforming uses.

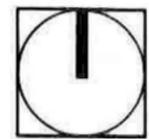
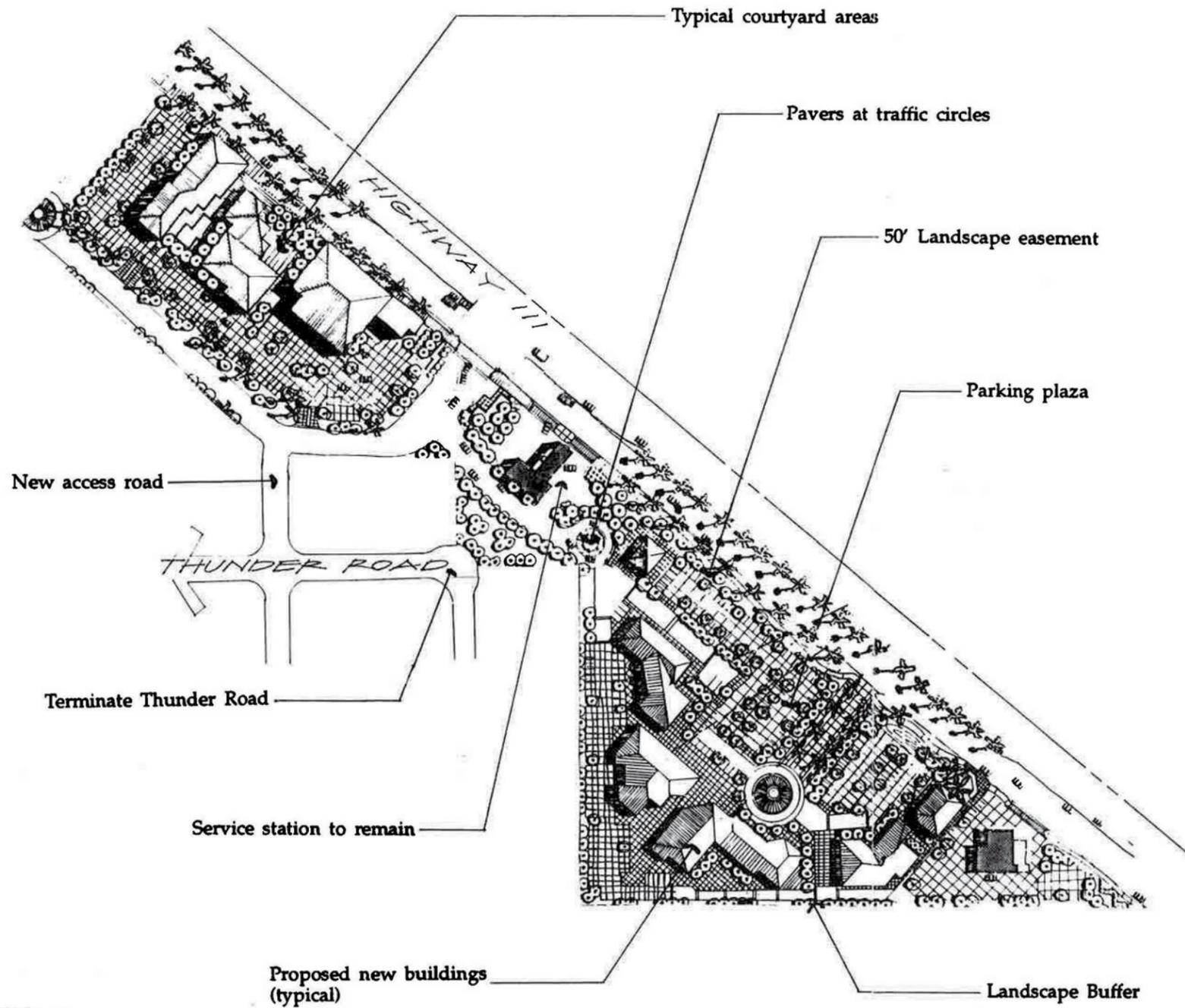


CITY OF RANCHO MIRAGE

**EXHIBIT 10**  
District 6 Concept  
"Attached Residential"



■ COMMERCIAL



NOT TO SCALE



**DISTRICT 7***COMMERCIAL WEST • 23.7 GROSS ACRES**COMMERCIAL RETAIL***Location**

This district is located on the south side of Highway 111 and extends from Mirage Cove Drive westward to Braemar Drive (see Exhibit 11).

**Existing Conditions**

Existing conditions which define this district and provide potential for design enhancement are:

- The Mountain foothills that define the western and southern boundaries of the commercial west district.
- The entire district is currently vacant and unimproved, providing a “clean slate” for new development.

**Land Use Concept**

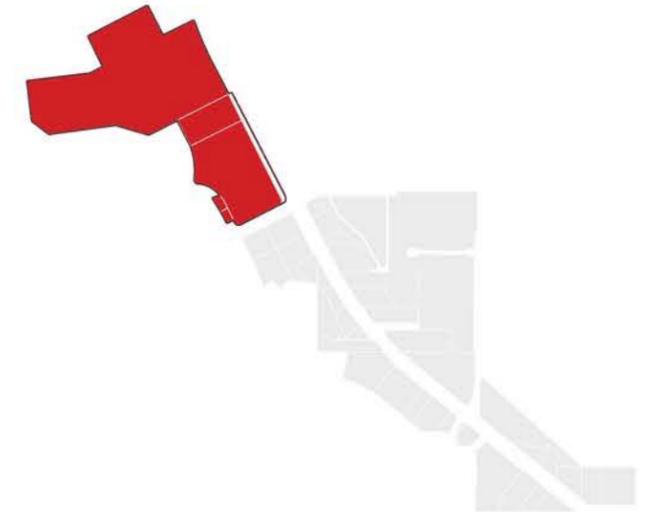
The land use concept envisioned for the Commercial West District is that of a commercial center that will provide for retail and office uses. Either of these uses may dominate the district, depending upon the market at any given time. In order to achieve this concept to the degree of development quality that is desired in the 111 west area, individual project must conform to the Development Regulations and Design Guidelines that are set forth in this Specific Plan.

In addition, a support circulation system has been designed to take traffic off Highway 111 and into the district. A landscaped Buffer Road drive with pathways will provide access to rear parking plazas in the eastern half of the district. A minor arterial road will provide access into the western half of the site directly from Highway 111. At the same time this landscaped feature will provide a buffer between the residential uses located immediately to the south and the parking plazas.

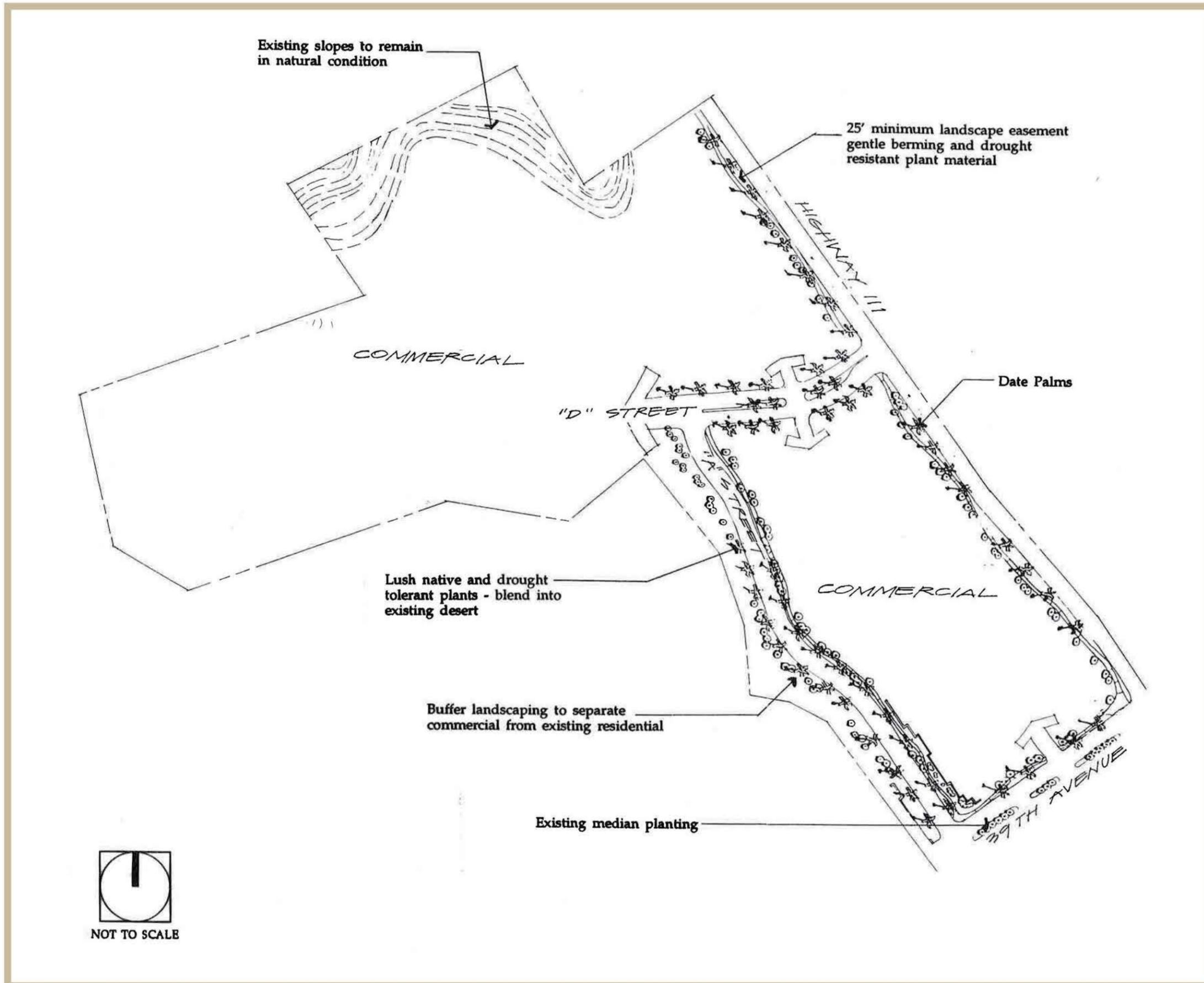


CITY OF RANCHO MIRAGE

**EXHIBIT 11**  
District 7 Concept  
"Attached Residential"



■ COMMERCIAL





### 3. Corridor Circulation

In order to improve circulation within and through the 111 West planning area and to reinforce land use structure and visual identity, a consistent design quality of public rights-of-way shall be maintained. The quality of the circulation and parking environments will be enhanced through the consistent application of guidelines for major entries, intersections, and streetscapes and the establishment of a clear hierarchy of street function.

This chapter explains the hierarchy of streets for the 111 West corridor.

The vehicular circulation structure is composed of major arterials, minor arterials, buffer roads, parking collector roads, two-lane local roads, as well as Highway 111 access control (median breaks, signalized intersections, and right in-out locations) and parking.

Additional design treatments within the road right of ways such as landscaping, street furnishings, signing and lighting are discussed in the Guideline sections of this Plan dealing with these topics.

#### CIRCULATION PLAN

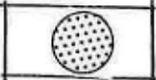
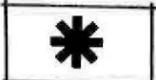
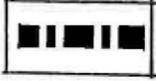
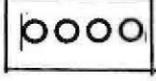
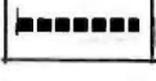
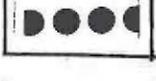
The Circulation Plan for the 111 West planning area is represented as a hierarchy of streets and a pedestrian walkway system. It is shown in Exhibit 12. The hierarchy is designed to separate different traffic functions in order to improve circulation. For example, the parking Collector Roads route commercial traffic off of Highway 111 to parking plazas. The concept provides improved access to commercial uses and will result in improved through-traffic conditions.

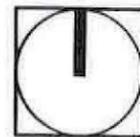
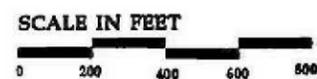
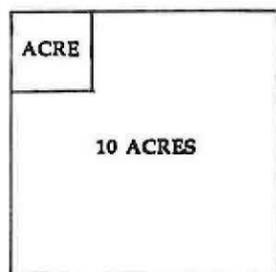
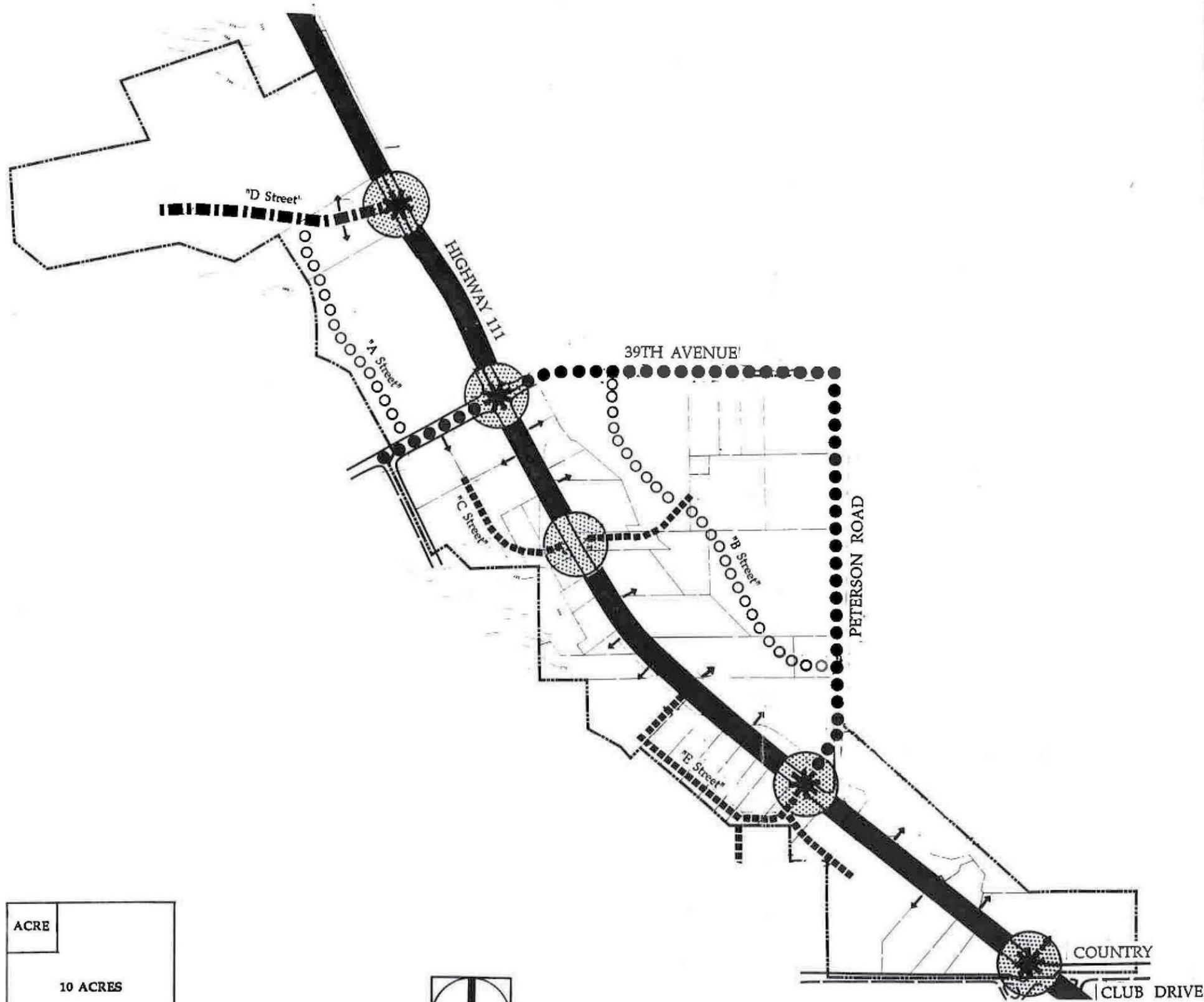
The hierarchy is reinforced by different parkway configurations. Parkway landscape treatments help bring visual and aesthetic unity and identity to each street to the planning area as a whole. Also located within the parkways are the paths and walkways. They provide the opportunity for pedestrian and bicycle circulation within the planning area.



CITY OF RANCHO MIRAGE

**EXHIBIT 12**  
Circulation Plan

-  **MEDIAN BREAKS**
-  **SIGNALIZED INTERSECTIONS**
-  **HIGHWAY 111 (110' R.O.W.)**
-  **MINOR ARTERIAL (110' R.O.W.)**
-  **BUFFER ROAD (92' R.O.W.)**
-  **PARKING COLLECTOR (60' R.O.W.)**
-  **LOCAL ROAD (50' R.O.W. MINIMUM)**
-  **RIGHT IN - RIGHT OUT**





## HIERARCHY OF STREETS

Exhibit 13 depicts the cross sections of the five road types in the Specific Plan area. Their locations are shown on Exhibit 12, the Circulation plan. Travel lanes, landscape medians, and landscape setbacks are shown.

### 1. Highway 111, Major Arterial (110' R.O.W.)

Highway 111, the corridor arterial, will directly or indirectly access all portions of the project area. It will also be utilized as a linear parkway and activity corridor linking the planning districts.

Highway 111 will be designed as a limited access arterial divided by a landscaped median not less than 14 feet wide. It will generally be a 110' R.O.W. road accessible only from right-in/right-out driveways, intersections and selected median breaks.

Highway 111 will provide three lanes of traffic in each direction and, optimally, a 25-foot landscaped parkway on each side. The 25-foot parkways shall be comprised of 9 feet of land within the right-of-ways and an additional 16-foot landscape easements on either side of the road. The landscape easement will provide space for 8-foot meandering sidewalk/bike paths and underground utilities.

Building Setbacks. Building (excluding bus shelters) will be setback a minimum of 16-feet outside the road right-of-way except where stated otherwise in this document.

### 2. "D Street", Minor Arterial (110' R.O.W.)

The minor arterial, which connects directly to Highway 111, is designed to be a dramatic access road that takes advantage of its proximity to mountain foothills. It will provide access to sites in the Commercial West planning district. This road provides a 16-foot raised landscaped median, 4 travel lanes and 21-foot parkways within the right-of-way on each side of the road that support 8-foot meandering sidewalk/bike paths.

Buildings will be setback a minimum of 4 feet outside the road right-of-way in order to provide a 25-foot parkway on each side of the road.

#### 2a. "D Street" west of "A Street" (60' R.O.W.)

The right-of-way configuration for D Street changes west of its point of connection to A Street (see Exhibit 12, Circulation Plan). The technical section is shown as Section 2a in Exhibit 13, Typical Street Sections. This portion of D Street maintains the original

landscape premises but incorporated them in a smaller right-of-way. The most notable differences are the lack of a landscaped median and reduced parkways on either side of the road.

This portion of D Street shall have 4-foot landscape easements outside the r.o.w., will conform to the designated technical cross-section (Section 2a, Exhibit 13), and shall adapt the landscape requirements of the Buffer Road Treatment (Chapter 4, Landscape Section C-C).

### 3. "A Street" and "B Street" – Buffer Roads (92' R.O.W.)

The Buffer roads provide 32-feet of pavement, with rolled curbs and 30-foot parkways within the right-of-way on each side of the road. The parkways provide space for 8-foot meandering sidewalks/bike paths. These roads are designed with two main purposes: to serve as buffers between residential neighborhoods and adjacent commercial areas, and to provide rear access to commercial uses fronting Highway 111.

#### 4. 39<sup>th</sup> Avenue east of Highway 111 (50' R.O.W.)

The two-lane local roadway, 39th Avenue, provides a means of back access to Highway 111 from the Residential district. It will provide 36-feet of pavement, and 7 foot parkways on each side of the road. It will provide an additional 15-foot landscape easement outside the r.o.w. on the project side of the road within which is an 8-foot meandering walkway.

#### 4a. 39th Avenue west of Highway 111 (122' R.O.W.)

39<sup>th</sup> Avenue, west of Highway 111, is designed to provide a special approach and entry to existing and future commercial retail uses in Districts 2 and 7, and a residential development beyond the Specific Plan area. The 122-foot right-of-way supports a 22-foot center median, 32 feet of pavement in each direction, and 18-foot parkways on each side of the road (see Exhibit 13, Typical Street Sections).

#### 4b. Peterson Parkway (50' R.O.W)

The Peterson Parkway (see Exhibit 13a) will have a special right-of-way and cross-section design which will accommodate an integrated bicycle and pedestrian trail system with enhanced landscape areas along with a two-lane roadway. This roadway right-of-way and improvement design accentuates the feeling of undulating walkways, landscaping and low-speed curves.

### 5. "C Street" and "E Street" – Parking Collector Roads (60' R.O.W)

The parking collector roads play an important role in the improvement of Highway 111 circulation. The roads connect with Highway 111 at two major intersections within the planning area and direct traffic to shared parking areas in the different planning districts. The roads provide 40 feet of pavement with 10 feet on each side for screening berms and walls.

## PEDESTRIAN WALKWAYS

The circulation needs of pedestrians will be met in the form of 8-foot wide meandering sidewalks. They will be provided on both sides of major arterial, minor arterial, and buffer roads, and on the project side of 39<sup>th</sup> Avenue. A 6-foot path will be provided only on the non-project side of 39<sup>th</sup> Avenue. No paths or walkways will be provided on the parking collector roads.

Where right-of-way conditions limit the landscape easement/front setback area of Highway 111 frontage, the meandering path shall expand to a formally designed 10-foot wide sidewalk.

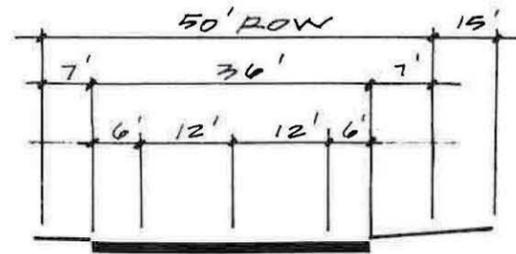
Such systems will provide safe, and aesthetically pleasing means of pedestrian movement and will be an integral part of the overall architecture and site design concept.



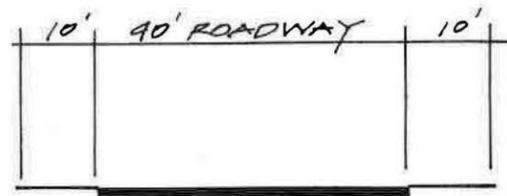
CITY OF RANCHO MIRAGE

### EXHIBIT 13

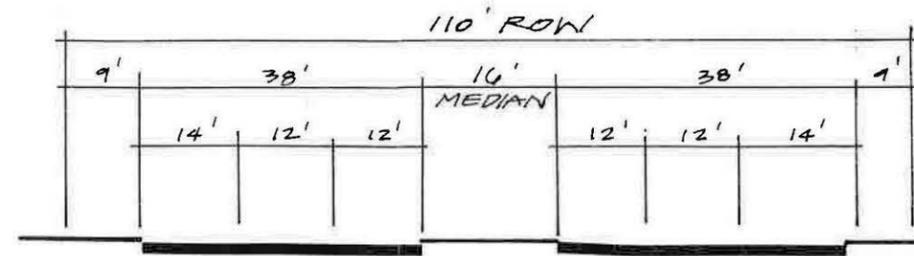
Typical Street Sections



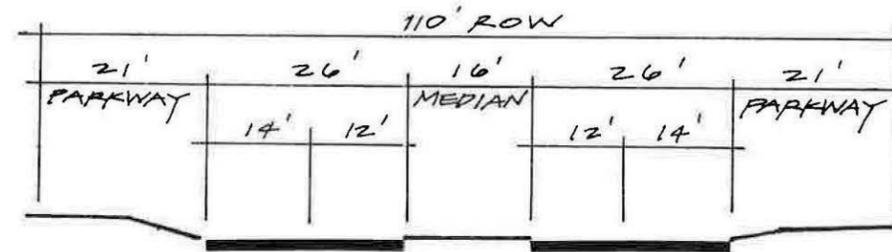
4 LOCAL ROAD SECTION



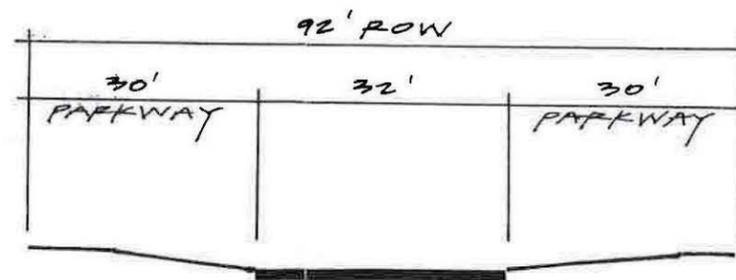
5 PARKING COLLECTOR SECTION



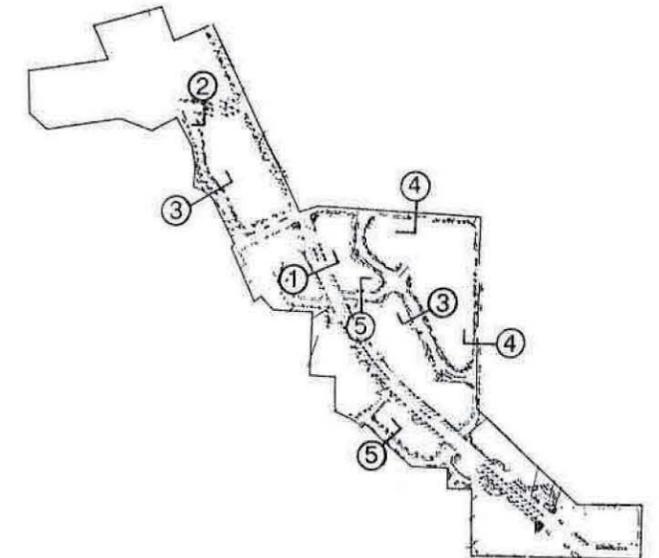
1 HIGHWAY 111 SECTION



2 MINOR ARTERIAL SECTION



3 BUFFER ROAD SECTION





#### **4. Landscape/Hardscape Concept**

The primary function of the Landscape/Hardscape concept is to establish character and a unifying visual theme for 111 West and to establish the Highway 111 right-of-way as the major design focus. The objective are to: enhance the resort character of Rancho Mirage using lush native and drought tolerant plant materials in landscaping, and to achieve a coordinated approach to the design and location of bus shelters, fountains, benches and other hardscape elements.

Primary landscaping elements include streetscape, edge treatments, major intersections, and entries within the Specific Plan area. In addition, signage and architectural guidelines are set both in subsequent chapters to promote a well-designed built environment. The intent is to establish high quality standards to complement the project's location within the City of Rancho Mirage (see Exhibit 14).

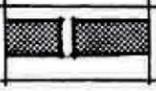
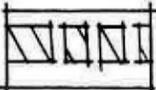
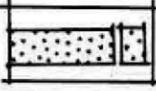
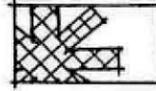
Landscaping for private development projects, as well as City right-of-way improvements, shall be composed from the plant palette found at the end of Chapter 8, Landscape Guidelines. It was developed in accordance with existing city guidelines and its use will produce the landscape concepts stated herein.

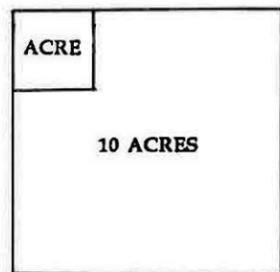
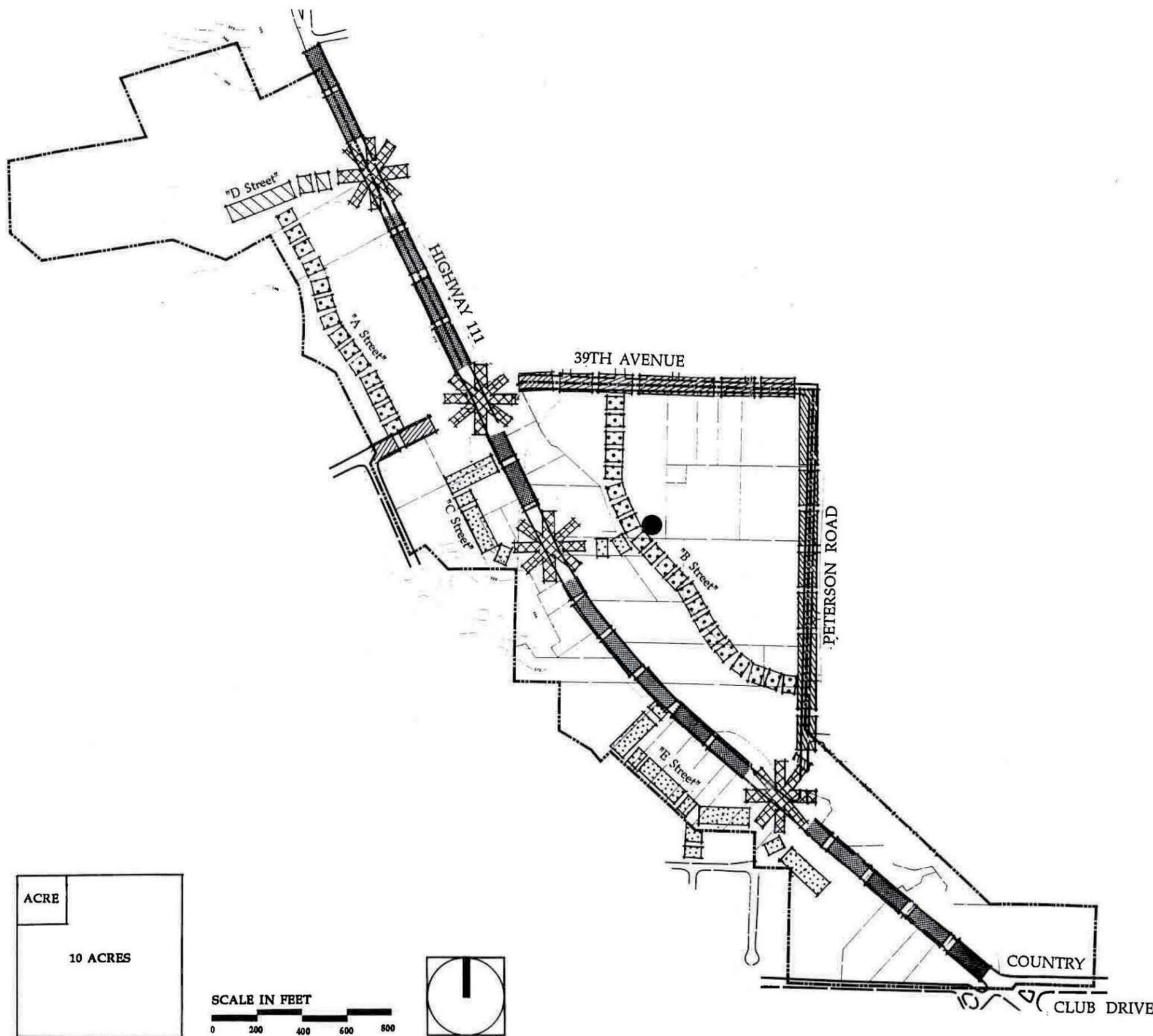


CITY OF RANCHO MIRAGE

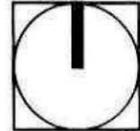
### EXHIBIT 14

Conceptual Landscape Plan

-  HIGHWAY 111 TREATMENT
-  MINOR ARTERIAL TREATMENT
-  BUFFER ROAD TREATMENT
-  PARKING COLLECTOR TREATMENT
-  LOCAL STREET TREATMENT
-  SPECIAL INTERSECTION
-  RESIDENTIAL ENTRY



SCALE IN FEET  
0 200 400 600 800





## EDGE TREATMENTS

The edge treatment concepts are designed for those edges and linear features that define the project area and separate its planning districts. The edge treatments are set forth in the guidelines below and in Exhibits 15 through 18.

### 1. Highway 111 Edge • (Sections A-1, A-2, A-3, & A-4)

This linear landscape feature will be the most visible and physically experienced space for users of the 111 West area. It functions as the unifying element that binds the planning districts together. The Highway 111 landscaping treatment will “set the tone” for the visual appearance of the planning area.

As the highway moves through the planning area, its cross section configuration changes. Exhibits 15 and 16 are representative of these various cross-section configurations.

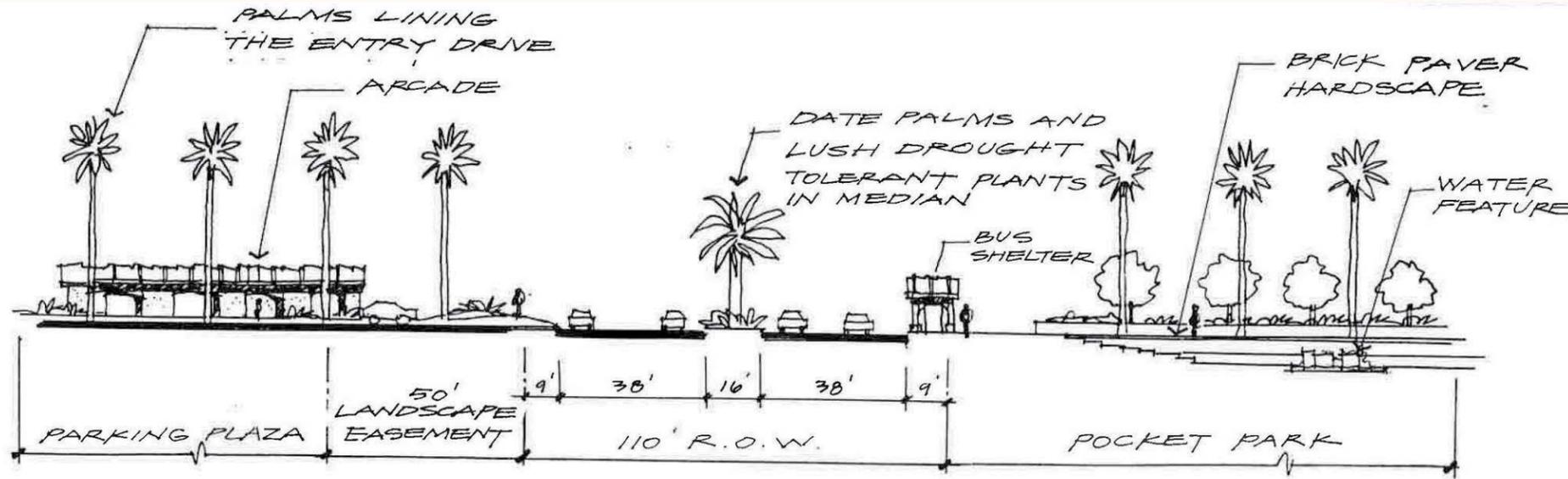
- Create a sense of unity throughout the project by lining each side of the highway with double rows of 25-foot tall date palms at 50-foot intervals. The center median will have a single row of date palms with the same configuration.
- Landscaped berms should be used to accentuate open space and screen parking areas from travelers on the highway.
- In those areas of Highway 111 where existing conditions limit the landscape easement to 10 feet, a ten-foot formal sidewalk using brick pavers with ceramic tile accents will be used. 1-foot, sand tinted, concrete headers will be used on either side of the sidewalk. Where there are right-of-way restrictions the sidewalk may be reduced to 6 feet in width.
- The center median shall not be less than 14 feet wide with 18 inch maintenance strips along each edge. These strips are to be of sand tinted stamped concrete.
- In accordance with the City of Rancho Mirage Highway Median Landscape Guidelines, 6-foot diagonal maintenance strips that span the median will be placed every 150 feet.



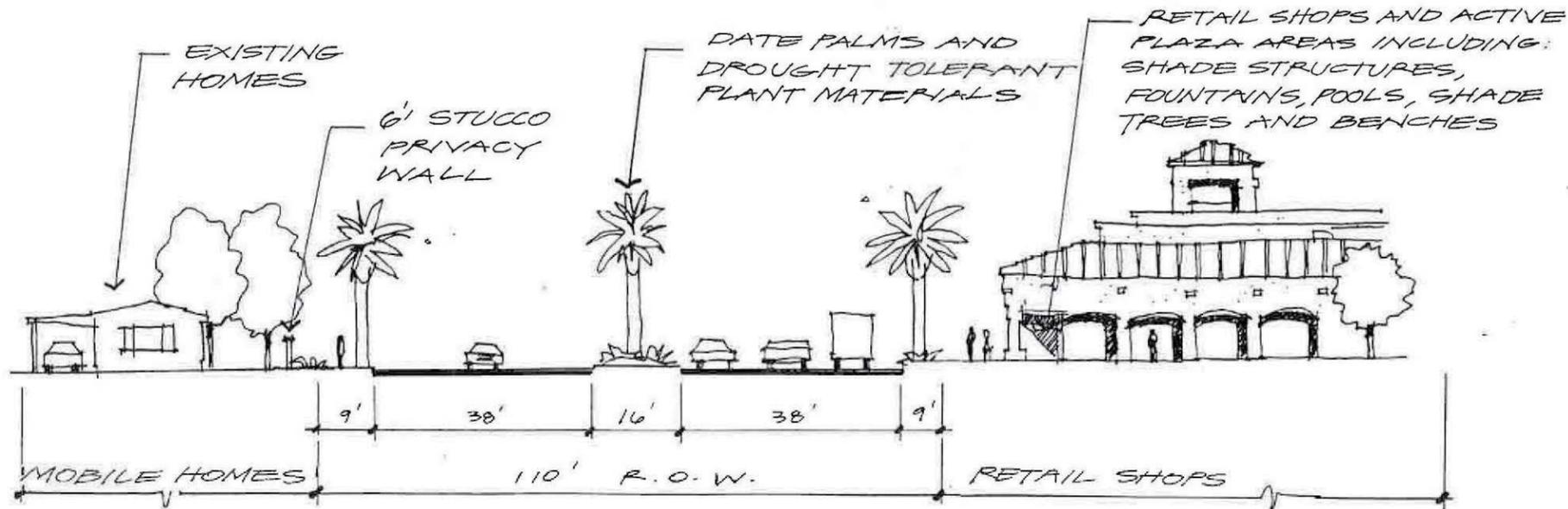
CITY OF RANCHO MIRAGE

**EXHIBIT 15**

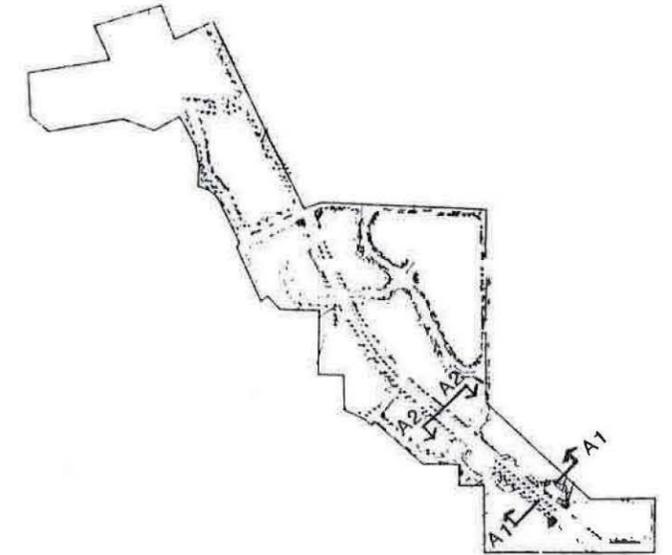
Roadway Landscape Sections  
A1-A1' and A2-A2'



**SECTION A1-A1** MAJOR ARTERIAL  
HIGHWAY 111 NOT TO SCALE



**SECTION A2-A2** MAJOR ARTERIAL  
HIGHWAY 111 NOT TO SCALE





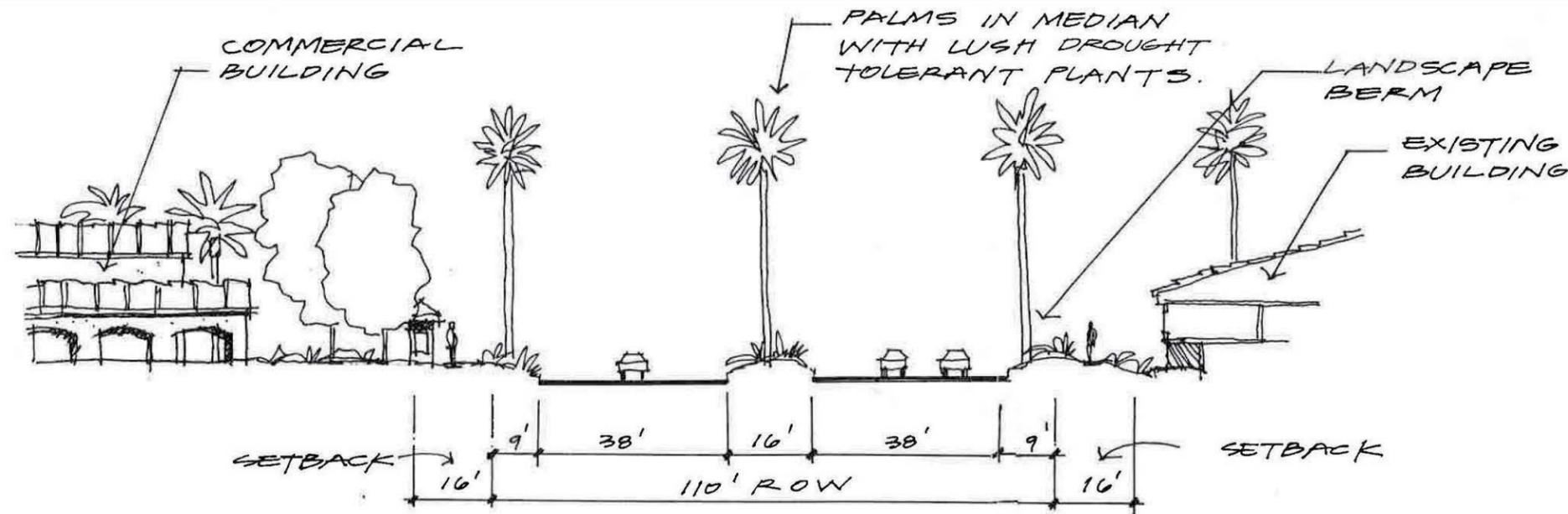
- Flowering plant materials should be strategically used to create accent points that highlight the right-in/out access entry points. Plantings at the base of monument signs at project entries are encouraged.
- Concealed, below grade landscape lighting should be used as a major design feature to emphasize plants, signage and buildings.
- Integrate existing landscape into new treatment to preserve the unique character of existing uses.
- Street furniture should be carefully located in areas of high use.



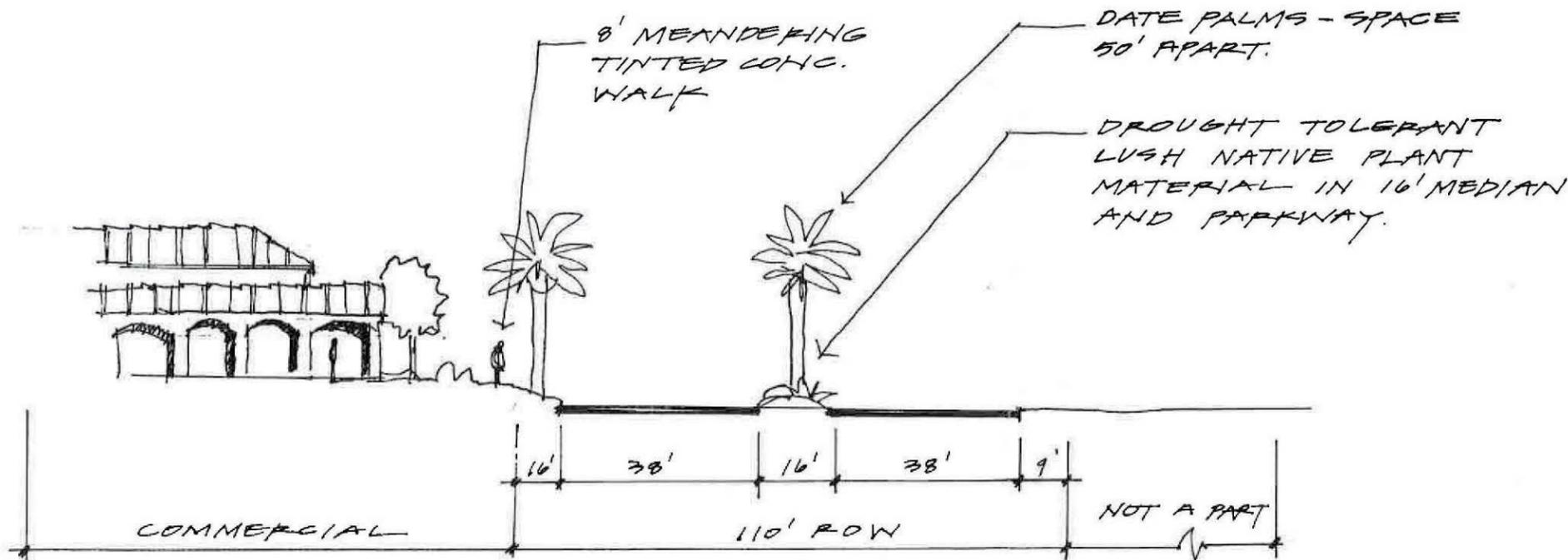
CITY OF RANCHO MIRAGE

**EXHIBIT 16**

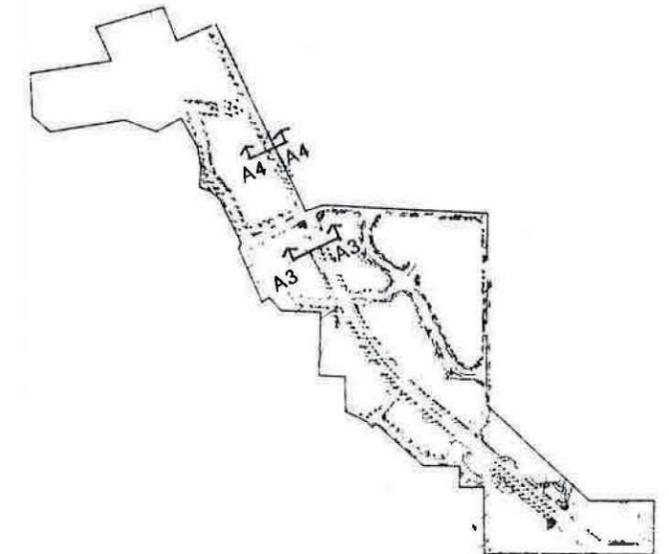
Roadway Landscape Sections  
A3-A3' and A4-A4'



**SECTION A3-A3** MAJOR ARTERIAL  
HIGHWAY 111 NOT TO SCALE



**SECTION A4-A4** MAJOR ARTERIAL  
HIGHWAY 111 NOT TO SCALE





### 2. Minor Arterial (Section B-B')

The minor arterial road provides primary access to Planning District 7, Commercial West, off of Highway 111. This road is intended to create a special arrival/access statement for District 7. The 110-foot right-of-way, landscaped median and 21-foot landscaped parkways are designed to achieve this effect (see Exhibit 17, Landscape Section B-B).

### 3. Buffer Road Treatment (Section C-C)

The buffer road landscape treatment (Exhibit 17, Landscape Section C-C) serves to screen residential areas from less compatible commercial districts. The buffer road treatment is designated to separate the restaurant row area of District 1 from the residential uses of District 3, and also the mixed commercial/offices of District 7 from existing residential uses to the south. The wide parkway easements and associated landscaping will create a pleasant pedestrian circulation system.

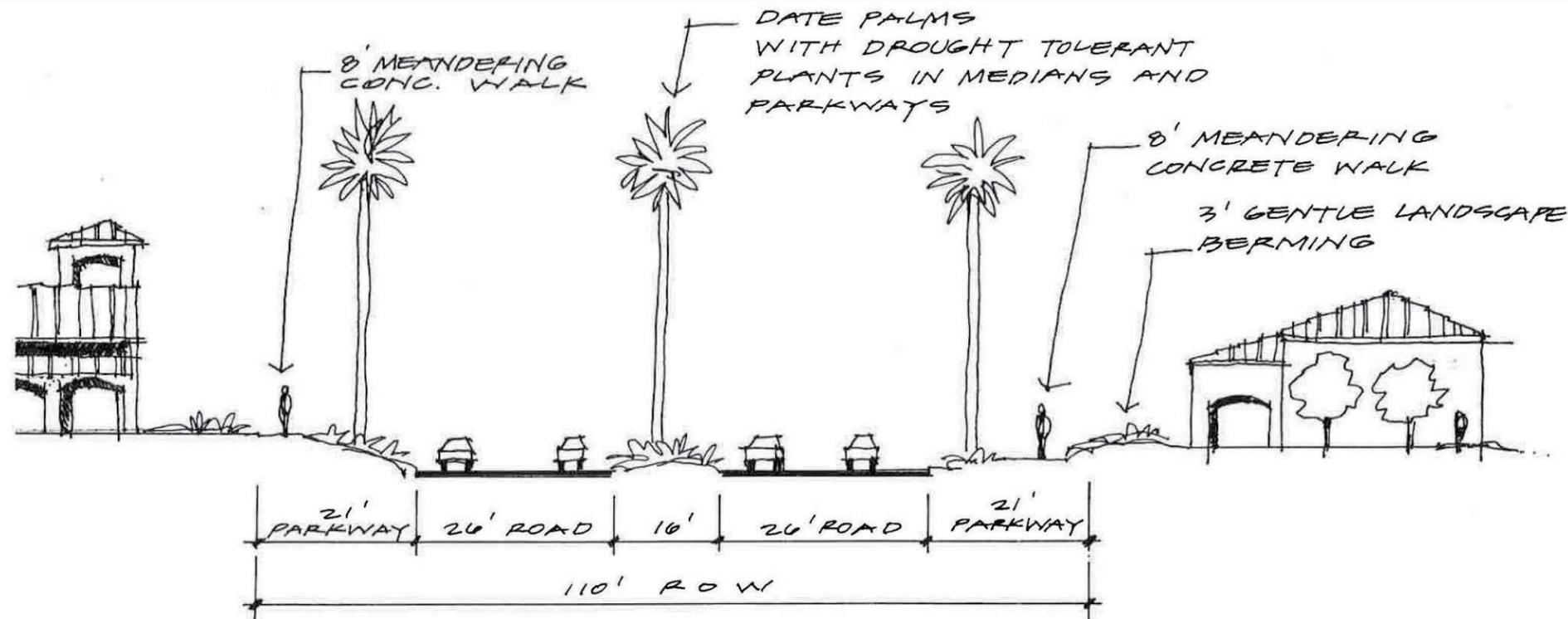
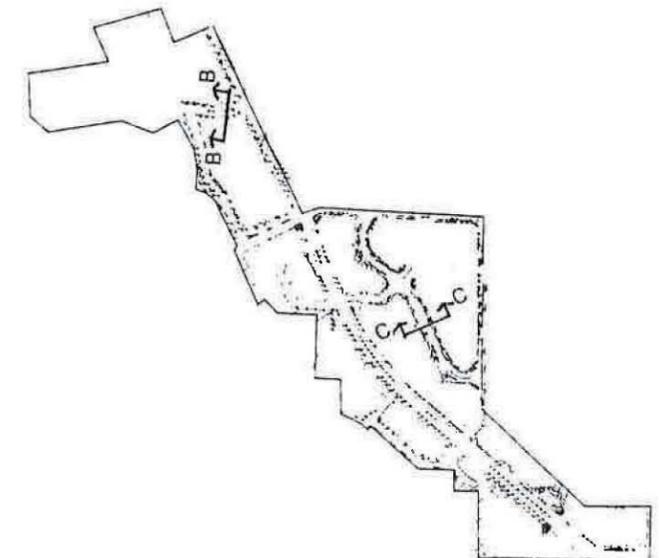
- 30' parkways within the right-of-way on each side of the road will provide buffer space supporting 8-foot meandering, tinted, sidewalks and gentle berming. The sidewalk tinting color should be different than landscape median tinting in order to provide contrast and identity.
- Rolled curbs should be used to create a gentle transition between the road and landscaping. They also provide less formal character.
- 3-foot screen walls along the edge of parking areas and 6-foot residential screen walls with view breaks and staggering should be used as part of the buffering process.
- Use enriched paving treatment at intersections for identity enhancement.
- Use vertical layering of landscaping for adequate shading and cooling of walkways – high palms, canopy trees, shrubs, groundcover.
- Pedestrian ways shall be property illuminated with low indirect lighting sources.



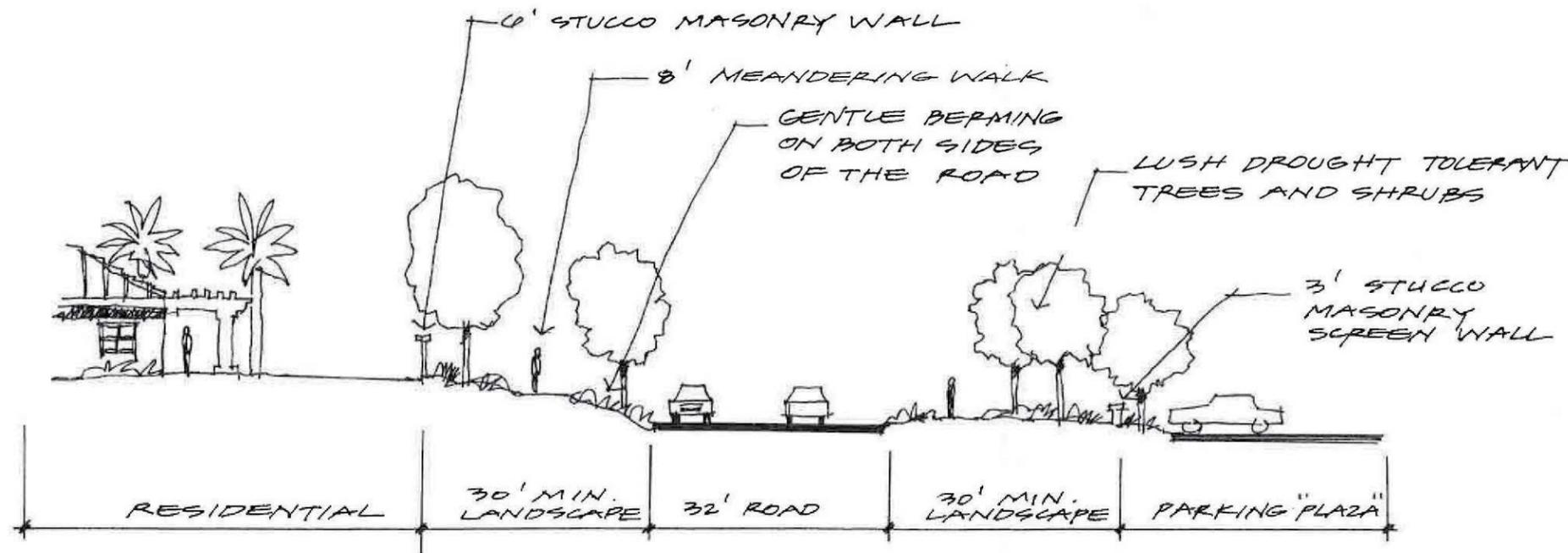
CITY OF RANCHO MIRAGE

**EXHIBIT 17**

Roadway Landscape Sections  
B-B' and C-C'



**SECTION B-B** MINOR ARTERIAL  
"D" STREET NOT TO SCALE



**SECTION C-C** BUFFER ROAD  
"A" STREET & "B" STREET NOT TO SCALE





#### 4. Local Street Edge Treatment (Section D-D)

The landscape treatment for the local roadway is shown in Exhibit 18 (Landscape Section D-D).

- The local street edge is to be defined by a 6-foot stucco masonry wall that steps in and out in order to provide visual interest. The wall should be broken periodically with wrought iron to provide views into and out of the project.
- The local street edge treatment should only happen on the project edge of the street.
- Landscaping should be developed with vertically layered tree plantings that maximize shade, and 8-foot tinted meandering walk, gentle berming and uplighting of landscaping, walls and signage.

#### 5. Parking Collector Road Treatment (Section E-E)

The landscape treatment for the parking collector road is shown in Exhibit 18 (landscape Section E-E)

- As a contrast to Highway 111 no date palms should be used on the parking collector roads.
- Use groups of Washingtonia Robusta as an overstory element and supplement with canopy trees and shrubs to maximize shade and cooling effect.
- All signage within the parking collector road landscape areas should be at auto eye level.

#### 6. Parking Edge Treatment

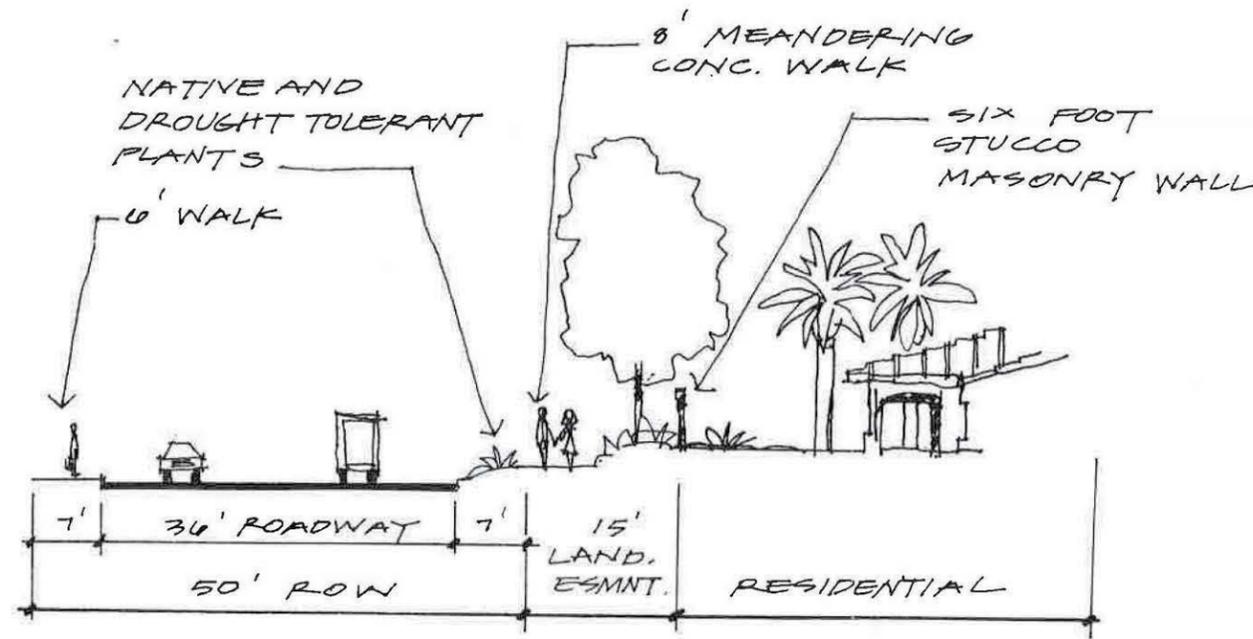
- Natural slopes – blend project landscaping into these areas and create focal points where natural topographic features encroach directly into the project area.



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### EXHIBIT 18

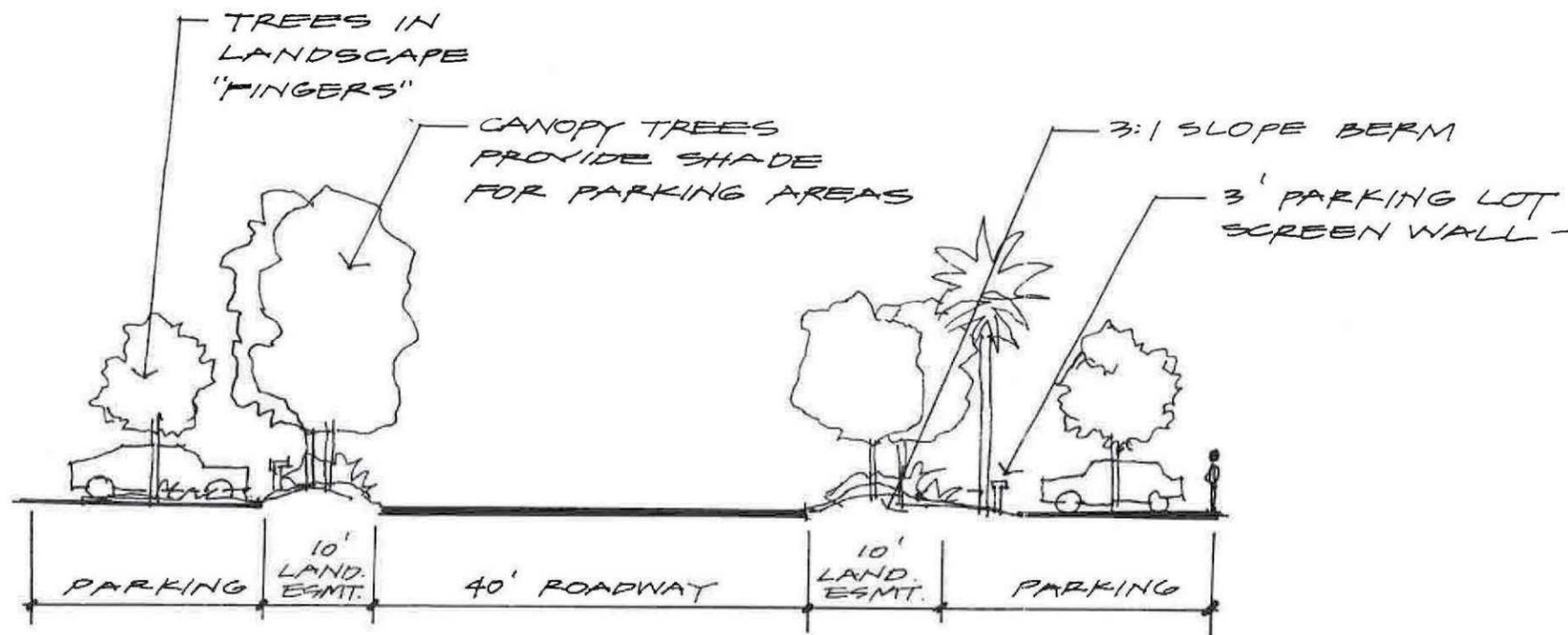
Roadway Landscape Sections  
D-D' and E-E'



#### SECTION D-D

39TH AVENUE  
PETERSON ROAD

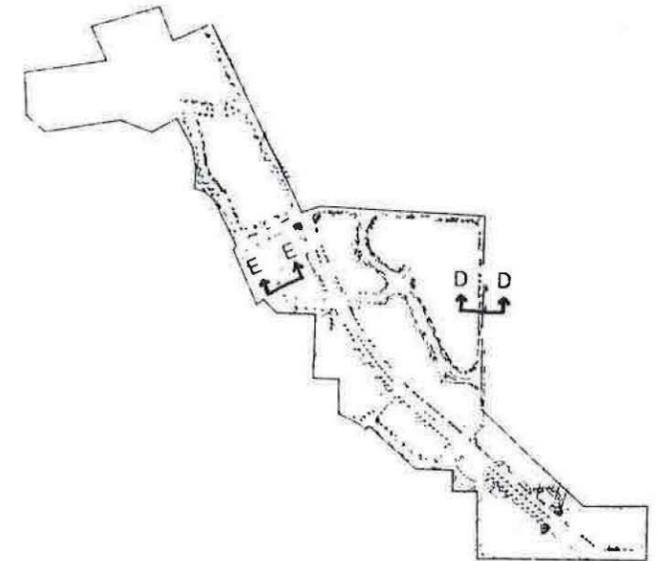
LOCAL ROAD  
NOT TO SCALE



#### SECTION E-E

"C" STREET  
"E" STREET

PARKING COLLECTOR ROAD  
NOT TO SCALE





### SPECIAL INTERSECTIONS

There are four primary intersections in the planning area as shown on the Conceptual Landscape Plan (Exhibit 14). They are located where Highway 111 intersects Peterson Parkway, “C Street” linking Districts 1 and 2, 39th Avenue, and “D Street” that serves District 7.

The Special Intersection Plan shown in Exhibit 19 depicts the landscaping/hardscaping concepts for these special intersections. The specific dimensions are stated only to communicate the main idea of the intersection design and are not intended as exact construction requirements. Actual construction shall incorporate the design elements stated herein, and shall be built as closely as possible to the specified dimensions.

In addition, the use of special intersection treatments at other intersection and crosswalk locations is encouraged where it would be beneficial for creating localized identity. Examples of possible additional locations include the intersection of “B and C Streets” at the District 3 Residential entry, and cross walks that link parking plaza areas.

All landscaping shall be composed from the plant palette for Special intersections found in the Landscape Design Guidelines (Section IV, Chapter 8).

### DESIGN CONCEPTS

Each Highway 111 special intersection shall be defined by the following guidelines:

#### Setbacks

- The area for ground for ground cover and accent flowers should be setback 95 feet from the point of intersection; the accent tree area should be setback an additional 25 feet (120 feet total) from the point of intersection; buildings should be setback 150 feet from the point of intersection.

#### Walls

- Walls shall be an extension of or consistent with theme walls elsewhere described.

#### Paving

- Enriched paving treatments, such as interlocking brick pavers, should be used to visually denote crosswalks from the interior of the intersection.

- Concrete limit lines or native stone paving treatments might be used to further differentiate the approach to pedestrian walkways.
- Other hardscape elements such as bollards, should be strategically placed behind the sidewalk edge to reinforce the project theme.

#### Special Treatments

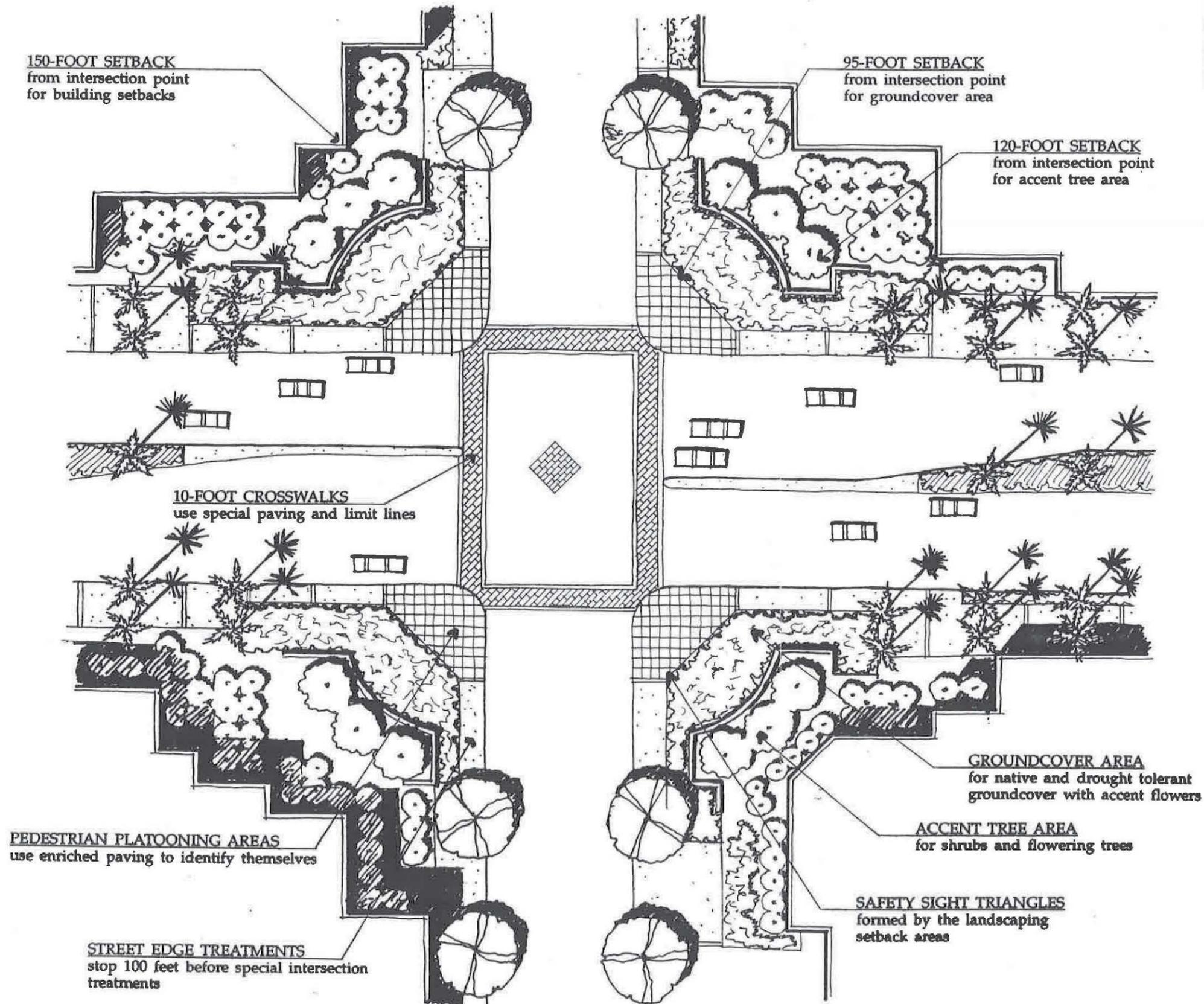
- Special landscaping should occur at designated major intersections as shown on the Landscape Concept Plan. Plantings in these areas should relate to major entry treatments to reinforce the community image. Clear views for traffic safety are important.
- Accent trees with flowering color should be used as identity plantings.
- Trees should be massed to create an effect similar to native plantings in undisturbed areas. The minimum tree planting container size is twenty four inch box.
- From the plant palette for major intersections, accent grouping of palms, specimen trees, shrubs, ground cover and annuals should be provided.
- Planters should be low to the ground.
- All signage and landscaping should be up-lit.



CITY OF RANCHO MIRAGE

### EXHIBIT 19

Special Intersection Concept





### RESIDENTIAL ENTRY TREATMENT

It is important for District 3, Attached Residential, to have a separate identity so that it may be perceived as distinct by residents and visitors alike. The neighborhood entry serves to further express the sense of place that should accompany one's residence (see Exhibit 20).

**Building Setback.** Buildings will be set back to a minimum of thirty feet from the edge of the street right of way at neighborhood entries for 300' from the corner of the closest intersection.

**Walls.** Each side of the roadway will have low matching entry walls to express the neighborhood identity. Walls will be visible from both approach directions. They will not exceed three feet in height; however, the length and width dimensions may vary between neighborhoods. Walls will be setback from the edge of the road and will not be located in the median. Walls will incorporate the predominant village color and architectural theme.

**Paving.** A ten foot wide zone of special road paving will connect the walls together across the road. The paving will be of a semi-rough texture, such as sand blasted concrete or exposed aggregate, and will contrast the predominant road pavement.

**Special Treatments.** The neighborhood entry will adopt a special identity symbol. It will not exceed 10 square feet in area and may be placed on the side or on top of the neighborhood entry walls.

- The landscape setback should be increased to 40 feet at the entry corners. The increased setback should extend far enough to accommodate the other provisions of the entry treatment.
- The monument walls on each side of the project entry should be designed to provide appropriate space for project identifying signage.
- Tree planting should be groupings of palms and accent trees selected from the plant palette, as should shrubs and ground cover.
- Annual flowers should be used in a consistent manner as accents.

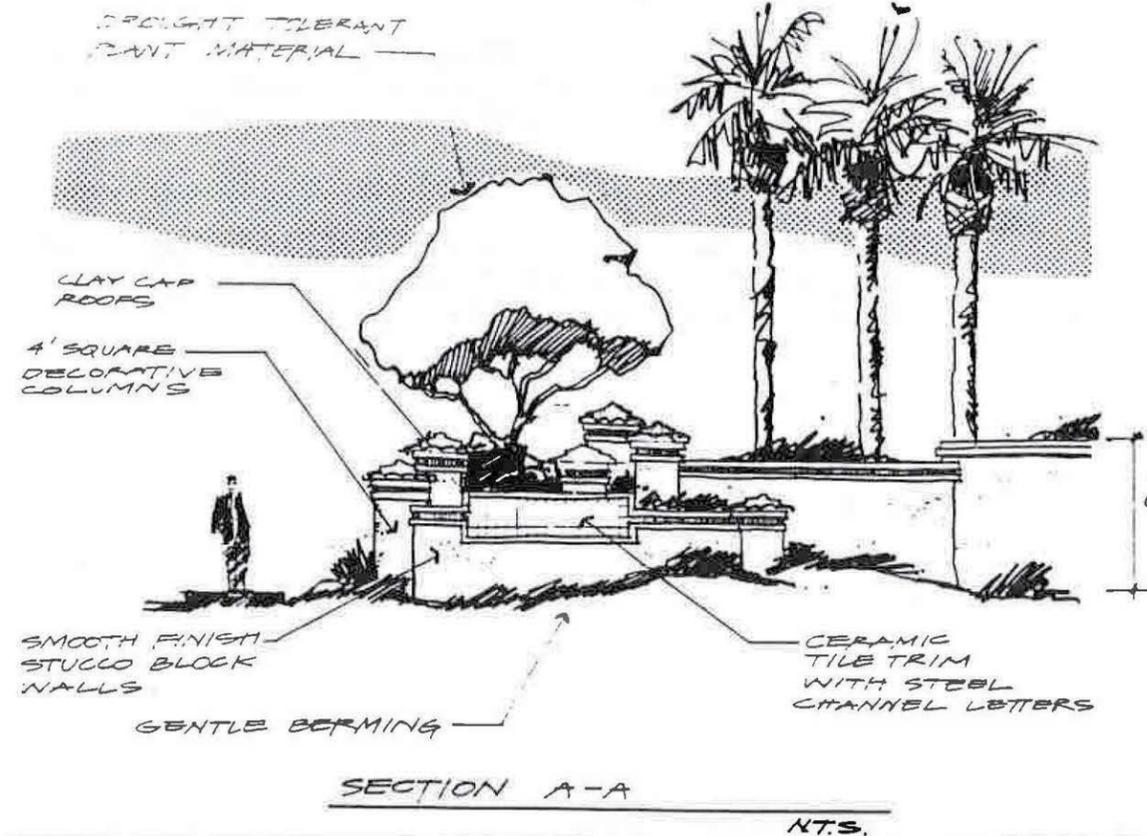
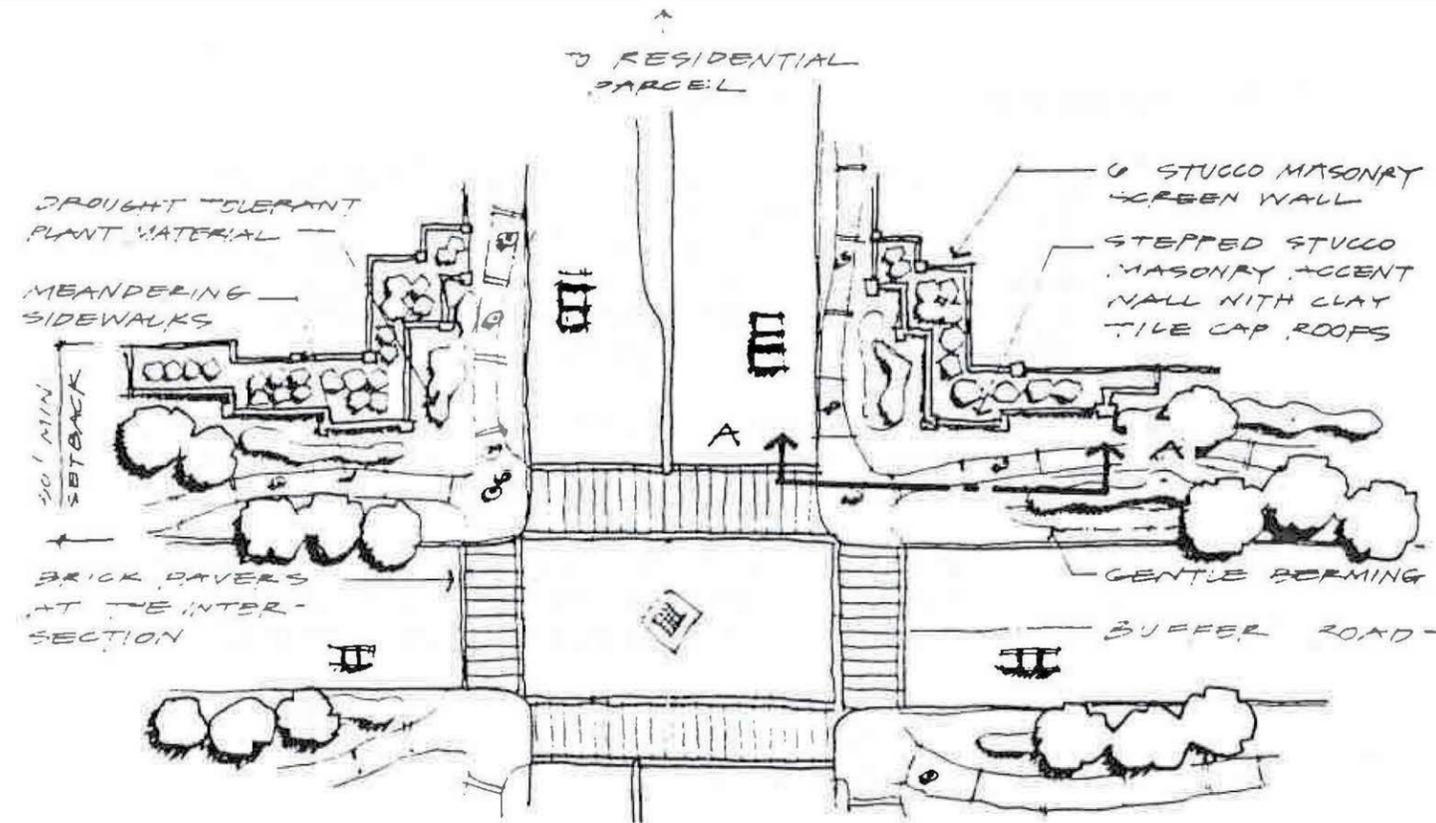
- The entry treatment should provide multi-level planters stepped away from the 6-foot stuccoed masonry screen wall. They should be stepped in both the vertical and horizontal dimension.
- All signage and landscaping shall be up-lit.



CITY OF RANCHO MIRAGE

### ILLUSTRATION 01

Residential Entry Plan



**PUBLIC PLAZAS AND NATURAL FEATURES**

Public plaza areas can be seen on the Illustrative plan in Exhibit 4. The largest is located within District 4, directly across from the District 6 retail plaza. It functions as pocket park of primarily hardscape design. The design elements listed here shall, to the extent possible, be incorporated or adapted to all public plaza spaces.

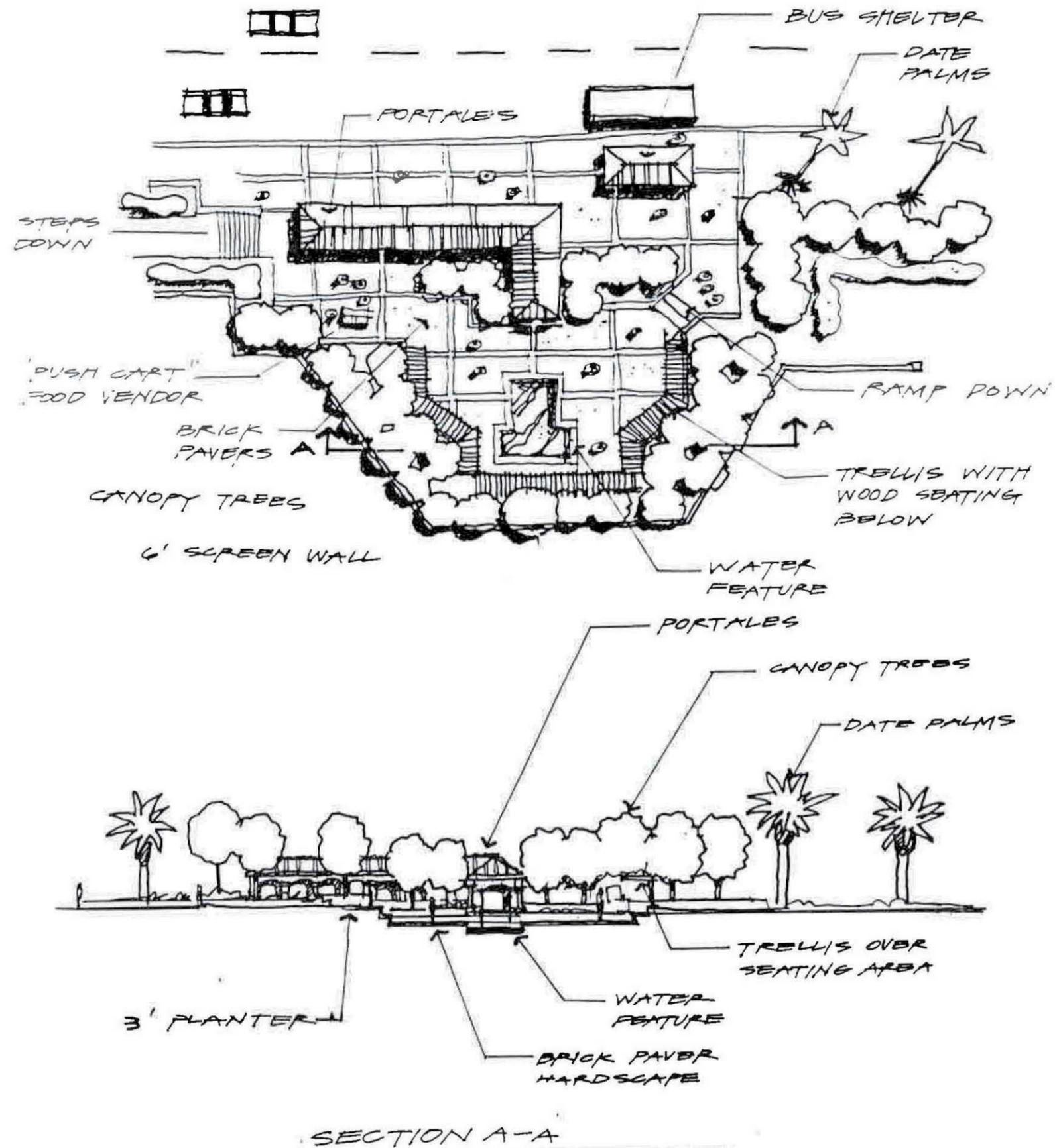
- “Portales” shade structures shall be used to create shaded resting areas and to provide covered walkways from buildings to the street side.
- Steps and ramps that may be needed to provide access and shall be creatively integrated into the site design and should not merely be after-thoughts.
- Landscaping shall reflect the edge treatment of the street it is located on. Canopy trees shall be used to provide shading.
- Trellises with wooden seating below shall be used to provide additional planting areas for accent flowers and climbing vines.
- Water Features shall be incorporated whenever possible. They shall utilize reclaimed water to the extent possible.
- In the case of the District 4 pocket park, and other areas where public spaces adjoin residential areas, a six-foot screen wall shall be used as a buffer.
- Special brick pavers shall be used in plaza areas.

Natural features will be emphasized though lighting and complimentary architecture. Landscaping of adjacent projects shall be blended into the natural vegetation.



CITY OF RANCHO MIRAGE

**ILLUSTRATION 02**  
Public Plazas and Natural Features





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# Section 3

## DEVELOPMENT REGULATIONS

### 5. Land Development Regulations

These regulations constitute the primary zoning provisions for the Highway 111 West Specific Plan. Exhibit 22 clearly notes the planning area and sub-districts which are subject to these regulations.

The regulations strengthen and replace chapters of the City of Rancho Mirage Zoning Ordinance (dated 12/17/87). When, in the future, the Zoning Ordinance is amended, new provisions will continue to apply to Specific Plan area projects except where less restrictive. Where there is a conflict, the regulations herein will prevail.

The overall intent of these regulations is to guide development within all planning districts so as to insure that each individual development project contributes to meeting the goals and objectives for the entire district. More specifically, the regulations are intended to:

- Provide opportunities for economically productive business enterprises.
- Establish uses and site development standards which contribute to the improvement of existing land use patterns.
- Provide for the scale and nature of uses which can make the most efficient use of the existing development pattern as well as parcel size and shape.
- Achieve gradual improvement of older commercial, office and residential buildings through expansion of use, redevelopment, rehabilitation, landscaping, design improvements or any combination of these devices desired by the property owner(s) and supportable by the City.
- Contribute to the improvements of both visual and functional attributes of the Corridor to help stimulate business activity.
- Expand the traffic handling capacity of arterial highways, local streets and driveways that serve the

area to accommodate new development and redevelopment or rehabilitation of existing areas; made possible in part by use changes or intensification.

- Establish a sense of place, as well as continuity and consistency of development standards, within the Corridor.



**PLANNING DISTRICTS**

Depicted in Exhibit 20 are the Land Use Districts and existing parcel configurations. The Exhibit establishes the official boundaries of the planning districts, or base district, to which the Permitted Uses, Building Regulations and Standards shall apply. Permitted Uses shall be located as outlined in the overall conceptual land use plan (Exhibit 3) and as described in the individual land use concept for each district. In District 4, Parcels 1 and 2 of Parcel Map 15442, which total 1.11 acres, shall be allowed all of the office and commercial uses which are listed on the Highway 111 West Specific Plan permitted Uses Matrix. With that exception noted, the land uses by planning district are summarized in Table 3 below.

**TABLE 3. PLANNING DISTRICT SUMMARY**

District	Adjusted Gross Acres	Net Buildable Area	Primary Use	Secondary Use
1	8.7	2.2	Dinner House Restaurant	Design Oriented Retail Commercial
2	10.6	2.6	Restaurants	Design Oriented Retail/Office
3	20.1	5.0	Attached Residential	Tennis Resort/ Residential Hotel
4	7.34	2.1	Mobile Home Park	Retail Commercial
	Parcels 1 & 2 of Parcel Map #15442	1.11	Professional Office Commercial	None
5	4.2	1.1	Professional Office	None
6	9.6	2.4	Retail Commercial	Professional Office
7	23.7	5.9	Retail Commercial	Professional Office
<b>TOTAL</b>	<b>85.2</b>	<b>21.3</b>		

\*Adjusted gross acreages are total gross district area minus the street right-of-ways. The net buildable area figures were derived from the adjusted gross figures, calculated by assuming an average lot coverage of 25 percent. This is consistent with past coverage experience in Rancho Mirage given parking, landscape, and setback requirements.

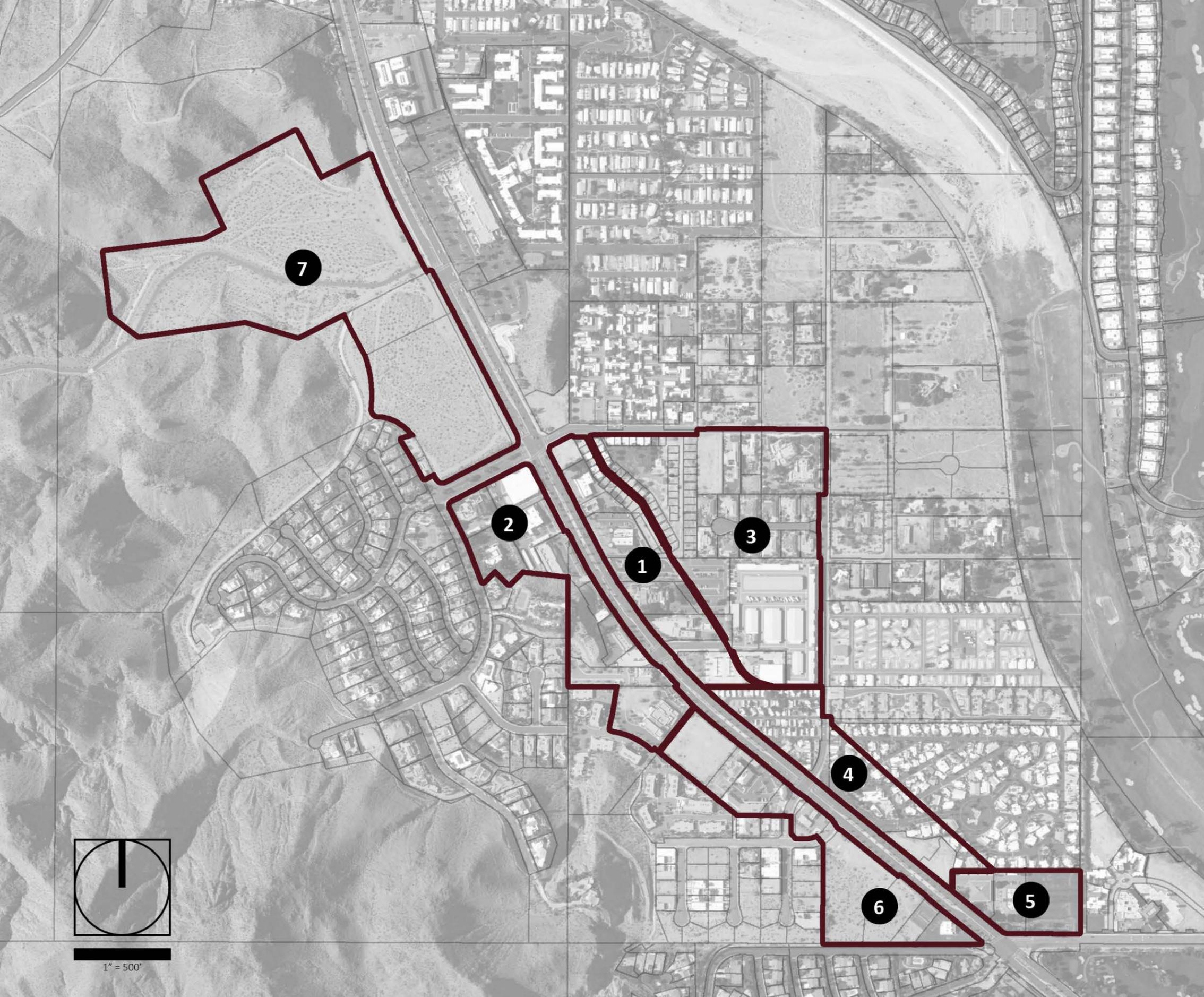


CITY OF RANCHO MIRAGE

## EXHIBIT 20

Land Use Districts

DISTRICT	PRIMARY USE	SECONDARY USE
1 Restaurant Row	Dinner house restaurant	Design oriented commercial
2 Restaurant/ Design Center	Restaurants	Design oriented commercial
3 Residential 6.5 du/AC	Townhomes	Tennis Resort/ Residential Hotel
4 Mobile Home Park		
5 Office Center	Professional office	
6 Commercial East	Retail commercial	Professional office
7 Commercial West	Retail commercial	Professional office





### PERMITTED USES

The Permitted Use Matrix depicted in Table 4 identifies a broad list of possible land uses for the Highway 111 West planning area. The list of uses is intended to be complete enough to adequately describe the permitted uses that are compatible with the desired land use concepts for the planning districts. The list of uses is not intended to be comprehensive.

A land use for a particular planning district can be designated as either: **Permitted** - a primary land use for that planning district; **Conditionally Permitted** - a secondary land use for that planning district; or **Prohibited**.

Permitted uses for a district are those that expressly fulfill the Primary Use designation for the district. Conditionally permitted uses are those which most closely match the Secondary Use designation for the district. Prohibited uses are incompatible with the established land use concept for the district.

### DETERMINATION OF USE

Whenever a use has not specifically been listed as being a permitted use in a particular use district classification, it shall be the duty of the Community Development Director to determine if said use is (1) consistent with the intent of the use district and (2) the said use is compatible with other listed permitted uses. Any person aggrieved by the determination may appeal that decision to the City Council.



TABLE 4 - PERMITTED USE MATRIX

		DISTRICT 1 – RESTAURANT ROW	DISTRICT 2 – RESTAURANT/COMMERCIAL	DISTRICT 3 – RESIDENTIAL PUD 6.5	DISTRICT 4 – MOBILE HOME PARK	DISTRICT 5 – OFFICE	DISTRICT 6 – COMMERCIAL EAST	DISTRICT 7 – COMMERCIAL WEST
⊗ Prohibited ◆ Permitted – Conditional Use Permit * In District 4, office and commercial uses are not permitted except on parcels 1 and 2 of Parcel Map 15442, which shall permit office and commercial uses listed in this permitted uses matrix.								
RESIDENTIAL	ATTACHED OR CLUSTERED HOUSING, OR S.F.D	⊗	⊗	◆	⊗	⊗	⊗	⊗
	TOWNHOUSES (5 DU/AC)	⊗	⊗	◆	⊗	⊗	⊗	⊗
	CONGREGATE CARE HOUSING	⊗	⊗	◆	⊗	⊗	⊗	⊗
	DAY CARE	–	–	–	–	–	–	–
	HOME OCCUPATION	⊗	⊗	–	–	⊗	⊗	⊗
	MOBILE HOME PARK	⊗	⊗	⊗	◆	⊗	⊗	⊗
	RECREATION CENTER	⊗	⊗	◆	◆	⊗	⊗	⊗
OFFICE	CLERICAL & LEGAL	⊗	–	⊗	*	◆	–	–
	COUNSELING SERVICES	⊗	–	⊗	*	◆	–	–
	FINANCIAL/ACCOUNTING	⊗	–	⊗	*	◆	–	–
	MEDICAL/DENTAL OFFICE/LAB	⊗	–	⊗	*	◆	–	–
	PROFESSIONAL SERVICES	◆	–	⊗	*	◆	–	–
	PUBLIC UTILITY OFFICE	⊗	–	⊗	*	◆	–	–
	REAL ESTATE OFFICE	⊗	–	⊗	*	◆	–	–
	TRAVEL AGENCY	⊗	–	⊗	*	◆	–	–
COMMERCIAL	ANTIQUES/COLLECTIBLES	–	◆	⊗	*	⊗	–	–
	APPAREL & ACCESSORIES	⊗	⊗	⊗	*	⊗	◆	◆
	APPLIANCES	⊗	⊗	⊗	*	⊗	–	◆
	ART GALLERY	–	◆	⊗	*	⊗	–	◆



TABLE 4 - PERMITTED USE MATRIX

⊗ Prohibited

◆ Permitted

– Conditional Use Permit

\* In District 4, office and commercial uses are not permitted except on parcels 1 and 2 of Parcel Map 15442, which shall permit office and commercial uses listed in this permitted uses matrix.

		DISTRICT 1 – RESTAURANT ROW	DISTRICT 2 – RESTAURANT/COMMERCIAL	DISTRICT 3 – RESIDENTIAL PUD 6.5	DISTRICT 4 – MOBILE HOME PARK	DISTRICT 5 – OFFICE	DISTRICT 6 – COMMERCIAL EAST	DISTRICT 7 – COMMERCIAL WEST
COMMERCIAL CONT'D	ART, HOBBY, PHOTO STUDIO/LAB	–	–	⊗	*	⊗	◆	◆
	BAKERIES (RETAIL)	–	–	⊗	*	⊗	◆	◆
	BARBER & BEAUTY	⊗	–	⊗	*	⊗	◆	◆
	BICYCLE SHOPS	⊗	◆	⊗	*	⊗	◆	◆
	BOOK STORES	⊗	⊗	⊗	*	⊗	◆	◆
	CANDY/ICE CREAM	–	–	⊗	*	⊗	◆	◆
	CARPET & DRAPERY	⊗	–	⊗	*	⊗	–	◆
	CAR WASHES	⊗	⊗	⊗	*	⊗	⊗	–
	CHURCHES	⊗	⊗	⊗	*	⊗	⊗	–
	COPY SHOP (RETAIL)	⊗	–	⊗	*	⊗	◆	◆
	DESIGN STUDIOS	–	◆	⊗	*	⊗	–	◆
	DELICATESSENS/CATERING	–	◆	⊗	*	⊗	◆	◆
	DEPARTMENT STORE	⊗	⊗	⊗	*	⊗	–	◆
	DRUG STORES	⊗	⊗	⊗	*	⊗	–	◆
	DRY CLEANING APPAREL SERVICES (RETAIL)	⊗	⊗	⊗	*	⊗	◆	◆
	ELECTRONICS	⊗	⊗	⊗	*	⊗	⊗	◆
	FLORISTS	–	–	⊗	*	⊗	◆	◆
FOOD MARKETS (SPECIALTY)	–	–	⊗	*	⊗	◆	◆	
FURNITURE (HOME FURNISHINGS)	–	◆	⊗	*	⊗	◆	◆	



TABLE 4 - PERMITTED USE MATRIX

		DISTRICT 1 – RESTAURANT ROW	DISTRICT 2 – RESTAURANT/COMMERCIAL	DISTRICT 3 – RESIDENTIAL PUD 6.5	DISTRICT 4 – MOBILE HOME PARK	DISTRICT 5 – OFFICE	DISTRICT 6 – COMMERCIAL EAST	DISTRICT 7 – COMMERCIAL WEST
⊗ Prohibited ◆ Permitted – Conditional Use Permit * In District 4, office and commercial uses are not permitted except on parcels 1 and 2 of Parcel Map 15442, which shall permit office and commercial uses listed in this permitted uses matrix.								
COMMERCIAL CONT'D	HARDWARE	⊗	⊗	⊗	*	⊗	◆	◆
	HEALTH CLUB	⊗	⊗	⊗	*	⊗	◆	◆
	INTERIOR DECORATING	–	–	⊗	*	⊗	◆	◆
	JEWELRY	⊗	⊗	⊗	⊗	⊗	◆	◆
	LABORATORIES (FILM, MEDICAL, DENTAL, ETC.)	⊗	⊗	⊗	*	–	–	–
	LAUNDROMATS	⊗	⊗	⊗	*	⊗	◆	◆
	LIQUOR	⊗	⊗	⊗	*	⊗	◆	◆
	MUSICAL INSTRUMENTS	⊗	⊗	⊗	*	⊗	◆	◆
	MUSIC (RETAIL)	⊗	⊗	⊗	*	⊗	◆	◆
	NOTION & NOVELTY STORE	⊗	⊗	⊗	*	⊗	◆	◆
	NURSERY & GARDEN	⊗	⊗	⊗	*	⊗	–	◆
	OFFICE SUPPLY	⊗	◆	⊗	*	⊗	◆	◆
	PAINT & WALLPAPER	⊗	◆	⊗	*	⊗	◆	◆
	PET SHOP	⊗	⊗	⊗	*	⊗	◆	◆
	REPRODUCTION (GRAPHICS)	⊗	⊗	⊗	*	⊗	◆	◆
	RESORT HOTEL	⊗	⊗	⊗	⊗	⊗	⊗	◆
	RESTAURANTS	◆	◆	⊗	*	⊗	◆	◆
SELF-STORAGE	⊗	⊗	◆	*	⊗	⊗	⊗	
SERVICE STATIONS	⊗	⊗	⊗	*	⊗	–	–	



**TABLE 4 - PERMITTED USE MATRIX**

⊗ Prohibited

◆ Permitted

– Conditional Use Permit

\* In District 4, office and commercial uses are not permitted except on parcels 1 and 2 of Parcel Map 15442, which shall permit office and commercial uses listed in this permitted uses matrix.

		DISTRICT 1 – RESTAURANT ROW	DISTRICT 2 – RESTAURANT/COMMERCIAL	DISTRICT 3 – RESIDENTIAL PUD 6.5	DISTRICT 4 – MOBILE HOME PARK	DISTRICT 5 – OFFICE	DISTRICT 6 – COMMERCIAL EAST	DISTRICT 7 – COMMERCIAL WEST
<b>COMMERCIAL CONT'D</b>	SPORTING GOODS	⊗	⊗	⊗	*	⊗	◆	◆
	SUPERMARKETS	⊗	⊗	⊗	*	⊗	◆	◆
	THEATERS (NON-DRIVE IN)	⊗	⊗	⊗	*	⊗	–	◆
	TOYS	⊗	⊗	⊗	*	⊗	–	◆
	VETERINARIAN	⊗	⊗	⊗	*	⊗	–	◆
	VIDEO RENTAL	⊗	⊗	⊗	*	⊗	◆	◆



**DISTRICT REGULATIONS**

The Specific Plan building regulations are presented in Table 5, the Building Regulations Matrix. All development in the 111 West area must conform to these regulations.

**Intent**

District Development Regulations are customized from the City’s existing land use zone regulations. The land use and design concepts that have been established for each planning district have direct bearing on this customization process. For example, the 111 landscape easement and front setback regulations provide the space resources necessary for implementation of the Specific Plan landscape concept, and side setbacks are designed to allow and encourage courtyards and intimate groupings of buildings as part of the commercial use design objectives.

The Development Regulations Matrix also identifies special conditions for certain districts. These conditions are primarily concerned with front setback conditions, special landscape requirements, and structure siting.



TABLE 5 GENERAL DEVELOPMENT STANDARDS	DISTRICT 1	DISTRICT 2	DISTRICT 3	DISTRICT 4	DISTRICT 5	DISTRICT 6	DISTRICT 7
	TOWNHALL	111 COMMERCIAL	111 COMMERCIAL	111 COMMERCIAL	111 COMMERCIAL	COMMERCIAL	COMMERCIAL
HIGHWAY 111 LANDSCAPE EASEMENT	YES	YES	NO	YES	YES	YES	YES
FRONT SETBACK [FT]	16	16 <sup>1</sup>	20	5	16 <sup>1</sup>	16	16
REAR SETBACK [FT]	10	10	25	10	20	10	10
SIDE SETBACK [FT]	5	5	---	5	10	5	5
STREET SIDE SETBACK [FT]	20	20	20	10	20	20	20
MAXIMUM HEIGHT [FT]	24 1 STORY	24 1 STORY	24 2 STORY	20 1 STORY	24 2 STORY	24 2 STORY	24 2 STORY
MINIMUM DISTANCE BETWEEN BUILDINGS [ FT]	10	10	15	10	20	10	10
PAD SIZE <sup>2</sup>	7,000 MIN	7,000 MIN	---	---	10,000	5,000	7,000
MINIMUM LOT SIZE	---	---	8,700	VARIES	---	---	---
MAXIMUM LOT COVERAGE	25%	25%	---	---	25%	25%	25%
MINIMUM OUTDOOR PRIVATE SPACE [SQ FT]	---	---	400	300	---	---	---
SPECIAL CONDITIONS	YES <sup>3</sup>	YES <sup>4</sup>	NO	YES <sup>5</sup>	NO	YES <sup>6</sup>	YES <sup>7</sup>

NOTE: Some commercial and office (i.e., "111 Commercial," Foothill Office," etc.) development regulations are distinguished by adjacent or near-by roads/features. This is intended to help identify and contextualize these uses based on their frontage or close proximity to roads/features.

1. May be minimally reduced where physical constraints or predetermined existing conditions dictate. Not to become less than ten (10) feet.
2. Pad size equals building footprint plus tributary drainage area (minimum five feet).
3. 25 percent of total roof area may extend to 35 feet in height. This is to allow for innovative architectural treatments of roof areas. Lot configurations in District 1 may support uses that are located away from Highway 111 on the rear portions of lots. These uses would be located along the buffer road that separates the Restaurant Row area from the Urban Residential area (District 3). These uses adhere to the following variations to the District 1 development regulations: Front Setback 15 ft.; Rear Setback 10 ft.; Maximum Pad Size 7,000 sq. ft.; Height Limit 24 ft. These standards are designed to allow smaller less intensive uses that will be subordinate to the primary District 1 uses along Highway 111. Consideration was also given to the adjacent residential areas.
4. The 25 ft. front setback along Highway 111 may be reduced to 10 ft. in some locations. These locations are limited to those areas on the south side of 111 that experience physical site constraints due to the encroachment of mountain foothills or where Highway 111 widening is a constraining factor.
5. The Highway 111 front setback requirement of 16 ft. is reduced to 5 ft. to accommodate the existing mobile home park. This front setback reduction applies only to that length of District 4 located west of Peterson Parkway. The east end of District 4, fronting along the north side of Highway 111, shall be developed and maintained as a public open space. It will support a hardscape space with a water feature and landscaping. This pocket park shall be designed in conformance with the Specific Plan Design Guidelines for entry treatments.
6. The 16 ft. Highway 111 front setback may be reduced to 10 ft. west of Peterson Parkway in order to mirror the setback conditions of the mobile home park across the street. Additionally, it may be expanded to 50 ft. along those areas across from the pocket park. This will accentuate the open space effect of the park.
7. Architecture must make reference to the mountain foothills to the south. Building massing shall provide for unobstructed view corridors to the foothills and any two-story elements shall be stepped back an additional 10 ft. from the original setback requirements. Roof lines shall blend with the natural slope of the foothills. Setbacks along "D" Street shall be 4 ft. from the right-of-way.



### SPECIAL CONDITIONS

Maximum height: Maximum building heights are listed in the Building Regulation Matrix. They are given in terms of maximum height in feet and number of allowed stories.

#### District 1: Restaurant Row

25 percent of total roof area may extend to 35 feet in height. This is to allow for innovative architectural treatments of roof areas.

Lot configurations in District 1 may support uses that are located away from Highway 111 on the rear portions of lots. These uses would be located along the buffer road that separates the Restaurant Row area from the Urban Residential area (District 3). These uses adhere to the following variations to the District 1 development regulations:

Front Setback:	15 ft.
Rear Setback:	10 ft.
Maximum Pad Size:	7,000 sq. ft.
Height Limit:	24 ft.

These standards are designed to allow smaller less intensive uses that will be subordinate to the primary District 1 uses along Highway 111. Consideration was also given to the adjacent residential areas.

#### District 2: Restaurant/Design Center

The 25-foot front setback along Highway 111 may be reduced to 10 feet in some locations. These locations are limited to those areas on the south side of 111 that experience physical site constraints due to the encroachment of mountain foothills or where 111 widening is a constraining factor.

#### District 4: Mobile Home Park

The Highway 111 front setback requirement of 16 feet is reduced to 5 feet to accommodate the existing mobile home park. This front setback reduction applies only to that length of District 4 located west of Peterson Parkway.

The east end of District 4, fronting along the north side of Highway 111, shall be developed and maintained as a public open space. It will support a hardscape space with a water feature and

landscaping. This pocket park shall be designed in conformance with the Specific Plan Design Guidelines for entry treatments.

#### District 6: Commercial East

The 16-foot Highway 111 front setbacks maybe reduced to 10 feet west of Peterson Parkway in order to mirror the setback conditions of the mobile home park across the street. Additionally, it may be expanded to 50 feet along those areas across from the pocket park. This will accentuate the open space effect of the park.

#### District 7: Commercial West

Architecture must make reference to the mountain foothills to the south. Building massing shall provide for unobstructed view corridors to the foothills and any two story elements shall be stepped back an additional 10 feet from the original setback requirements. Roof lines shall blend with the natural slope of the foothills. Setbacks along "D" Street shall be 4' from the right-of-way.

### GENERAL PROVISIONS

The following regulations apply to all Districts and uses within the Specific Plan area. Many of the area wide general regulations come directly from the City Zoning Ordinance and are noted in parentheses. Where the regulations have been added to or changed to achieve specific goals, they have been designated with the heading of "Additional Specific Plan Regulations" and are demarcated with a "bullet": ●.

#### Design Considerations (17.18.030D)

- a. The proposed development shall be of a quality and character which is consistent with the community design goals and policies including but not limited to scale, height, bulk, materials, cohesiveness, colors, roof pitch, roof eaves and the preservation of privacy.
- b. The design shall improve community appearance by avoiding excessive variety and monotonous repetition.
- c. Proposed signage and landscaping shall be an integral architectural feature which does not overwhelm or dominate the structure or property.
- d. Lighting shall be stationary and deflected away from all adjacent properties and public streets and rights-of-way.

- e. Mechanical equipment, storage, trash areas, and utilities shall be architecturally screened from public view.
- f. With the intent of protecting sensitive land uses and proposed design shall promote a harmonious and compatible transition in terms of scale and character between areas of different land uses.
- g. All building elevations shall be architecturally treated.
- h. Parking structures shall be architecturally compatible with the primary structure.
- i. Both sides of all perimeter walls or fences shall be architecturally treated.

#### Height Determination (Buildings and Structures 17.18.030K)

All buildings and structures shall meet the following standards relating to height:

- a. The building's height shall not exceed the standard for the zone in which it is located. The building height shall be determined from the finished grade to the highest point of the structure, excluding chimneys and vents.
- b. Pad elevations shall be determined by the Director and the City Engineer based on the following criteria:
  1. Flood control;
  2. Site drainage;
  3. Viewshed protection from both public and private property;
  4. Protection of privacy of surrounding properties including consideration of the location of windows, doors, balconies and decks;
  5. Building setback in relationship to building height and property lines;
  6. Sightline and building envelope analysis;
  7. Sewer line grade and location; and



8. Necessary slopes and retaining walls.

- c. Perimeter fences, or walls, shall not exceed six (6) feet in height. The height shall be measured from the finished grade of the property.
- d. Architectural walls integral to the building design, attached to the building or structure may exceed six feet in height, subject to review by the Community Development Director.
- e. To protect safe sight-distance for vehicular movement, sight obstructing fences, or walls, or other obstructions shall not exceed thirty-six (36) inches in height when located in a front setback.
- f. Free-standing flagpoles and radio and television antennas may not exceed the building height restrictions of the zone in which they are located.

#### **Additional Specific Plan Regulations**

##### ***Additional Height Restrictions***

- In addition to the minimum required setbacks for one (1) story buildings, the setbacks shall be increased a minimum of two (2) feet for each one (1) foot of additional building height above the maximum allowed in a particular planning district; however, the Council in exercising their discretion may require significantly greater setbacks.
- Enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated in making height decisions.
- In order to ensure the preservation and enhancement of scenic viewsheds, a visual analysis relating building proportions, massing, height and setback shall be conducted.
- The need and appropriateness of the additional height shall be demonstrated.
- Compatibility and harmony with surrounding development, land use designations, and zoning shall be demonstrated.

#### **Exterior Building Walls (17.18.030 F)**

The following standards shall apply to all exterior building wall construction:

- a. Since walls will always be a main architectural and visual feature in any major development, restraint must be exercised in the number of permissible finish materials. The harmony of materials and particularly color treatment is essential to achieve unity in the project. Because the desert setting suggests a textured wall surface, the recommended exterior wall finish materials are textured stucco, concrete, or concrete blocks. Stucco coloring in residential developments and concrete coloring in other buildings shall be restricted to being generally light in tone.
- b. The following designs are deemed unacceptable in any development and therefore shall be prohibited:
  1. Stucco walls divided by wood dividers;
  2. Non-colored-anodized and/or unpainted aluminum treatments; and
  3. Metal grills and facades. However, grills and facades of unique design and in keeping with the general decor of the development may be permitted subject to prior approval by the Director..

#### **Projections/Constructions Permitted Into Setbacks (17.18.030 N)**

The following list represents the only projections/construction that shall be permitted with the required setbacks:

- a. Front Setback: Roof overhangs.
- b. Rear Setback: Roof overhangs, pools, patio covers, tennis courts, gazebos, and other accessory uses, provided there is no projection within ten (10) feet of the property line. In Planned Unit Developments, this setback may vary.
- c. Side Setback: Roof overhangs.

#### **Solar Energy Design Standards (17.18.030 U)**

Passive heating and cooling opportunities shall be incorporated in all developments in the following manner:

- a. Future buildings shall be oriented to maximize solar access opportunities.
- b. Streets, lot sizes, and lot configurations shall be designed to maximize the number of buildings oriented so that the south wall and roof area face within 45° of due south.
- c. The proposed lot size and configuration shall permit buildings to receive cooling benefits from both prevailing breezes and existing and proposed shading.
- d. Any pool or spa facilities owned and maintained by a homeowners association shall be equipped with solar cover and solar water heating system.
- e. No building, wall, fence, or other structure shall be constructed or vegetation placed so as to obstruct solar access on an adjoining lot.
- f. All solar collectors and appurtenant equipment shall be located such that they are not visible to the public.

#### **Land Use Screening (17.18.030 S)**

Wherever property zoned for commercial, hospital, or institutional purposes abuts a residential zone, there shall be erected along the property line abutting the residential zone a solid decorative wall with a stucco or equivalent finish and material that is six (6) feet in height. The wall shall be architecturally treated on both sides.



## Landscaping

### **Applications (17.20.020)**

A comprehensive landscaping plan shall be submitted as part of a permit application pursuant to Chapter 17.46 (Applications and Fees) of the Rancho Mirage Zoning Ordinance.

### **General Regulations (17.20.030)**

In addition to the following regulations, the Director may require additional information to assure effective implementation of a landscaping plan.

- a. The City may require that landscaping plans be prepared by a landscape architect registered to practice in the State of California.
- b. Landscape designs shall be in harmony with the surrounding desert environment.
- c. Landscape designs and construction shall include desert plants.
- d. A fully dimensioned comprehensive landscaping plan shall include, but not be limited to:
  1. List of Plants (Common & Latin)
  2. Size
  3. Location
  4. Irrigation Plan
  5. Hardscape
  6. Water Elements
  7. Any other information deemed necessary.
- e. The planting of trees and shrubs shall comply with the following installation requirements:
  1. Trees shall be at least 15-gallons in size, or of comparable height if bare-rooted, at planting time. In addition, mature specimen trees in 24-inch and 36-inch boxes shall be provided in sufficient quantity to provide variety and emphasis of focal areas in the landscaping plan.

2. Trees and shrubs shall be planted so that at maturity they do not interface with service lines, Traffic Safety Sight Area, basis property rights of adjacent property owners, particularly the right of solar access, pursuant to Section 17.18.030 U. *Solar Energy Design Standards*).

3. Trees planted near public bicycle trails or curbs shall be installed in such a manner as to prevent physical damage to sidewalks, curbs, gutters and other public improvements.

- f. When inorganic groundcover is used, it shall be in combination with live plants and shall be limited to any accent feature.

- g. All landscaping shall have an automatic irrigation system.

### **Parkway Treatment Standards (17.20.040)**

Landscape plans for parkway areas shall include but not be limited to the following:

- a. Provide a design to assure compatibility with established parkways, including a sensitive transition between diverse landscape types and patterns.
- b. Incorporate mounding within the overall design, with landscaped slopes not exceeding twenty percent (20%) slope or three feet (3') in height.
- c. Incorporate walls and fences into the landscape design, including the special treatment of meandering walls, and wall breaks or openings where the design shall complement the interior landscaping of the adjacent development.

### **Corner Treatment Standards (17.20.050)**

Landscape plans for planned unit developments involving corner lots shall include additional special design requirements, including but not limited to the following:

- a. A minimum landscape area of thirty-six hundred (3,600) square feet for corner areas adjacent to a "major arterial" or "arterial" street, and sixteen hundred (1,600) square feet for corner areas adjacent to a "collector" street.

- b. Incorporate significant landscape and water features, including specimen trees, coordination with wall breaks or openings, and special "city entry" image treatment where appropriate.
- c. Assure that any corner landscape plan with the "Traffic Safety Sight Area", as defined, shall be designed to protect public safety.

### **Installation of Landscaping (17.20.060)**

All required landscaping shall be properly installed, irrigated, and maintained prior to use inauguration or occupancy.

### **Maintenance of Landscaping (17.20.070)**

- a. Maintenance of approved landscaping shall consist of regular watering, pruning, fertilizing, clearing of debris and weeds, the removal and replacement of dead plants, and the repair and replacement of irrigation systems and integrated architectural features.
- b. With the final approval of the development plan or recording of a subdivision map, the following shall occur:
  1. A covenant to assure continued maintenance of required landscaping by the property owners, homeowners association, or other liable entity shall be recorded in the Office of the Riverside County Recorder.
  2. A three (3) year landscaping maintenance agreement and letter of credit shall be executed between the property owner and the City.

Landscaping for commercial and residential projects within the 111 West area shall conform to the landscape guidelines found in Chapter 8 of the Specific Plan Design Guidelines.



**Additional Specific Plan Regulations**

**Flexibility in Coverage**

- A reduction in landscaping coverage may be sought and approved in recognition of quality design. For purposes of this provision, quality considerations include: superior visual buffering; visually creative massing of materials; maximum provision of shade; visually pleasing combinations of landscape/hardscape/textures/contouring; noise attenuation value: theme reinforcement; use of courtyards, atriums and other effective treatment of ground floor public space; creative use of water, sculpture or art work; effective use of specimen trees; overall visual interest: ease of maintenance in mature condition and water conservation effectiveness. The amount of such reduction, if deemed appropriate, shall not reduce the basic requirement by more than five percent.

**Landscaping Maintenance**

- Lawn and groundcover is to be trimmed or mowed regularly. All planting areas are to be kept free of weeds and debris
- All plantings are to be kept in a healthy and growing condition. Fertilization, cultivation and tree pruning shall be a part of regular maintenance.
- Irrigation systems shall be kept in working condition. Adjustments, replacements, repair and cleaning shall be a part of regular maintenance.
- Stakes, guys and ties on trees shall be checked regularly for correct function. Ties are to be adjusted to avoid creating abrasions or girdling on trunks or branches.

**Fences, Walls, and Hedges (17.18.030G)**

The following standards shall apply to the installation of all fences, walls and hedges:

- On a corner lot, no fence, wall, hedge, sign or other structure, shrubbery, mounds of earth, or other visual obstruction over Thirty-six (36) inches in height above the nearest street curb elevation shall be erected, placed, planted, or allowed to grow within a Traffic Safety Sight Area. The foregoing provision shall not

apply to public utility poles; trees trimmed (to the trunk) to a line at least six (6) feet above the level of the intersection; saplings or plant species of open growth habits and not planted in the form of a hedge, which are so planted and trimmed as to leave at all seasons a clear and unobstructed cross-view; supporting members of appurtenances to permanent buildings existing on the date this Title becomes effective; official warning signs or signals.

- A maximum six (6) foot high fence, wall or hedge may be located anywhere on the lot except in a Traffic Safety Sight Area, a required front setback or street-side setback which shall have a thirty-six (36) inch maximum height or as required by other regulations within this Title.
- When there is a difference in the ground level between two adjoining lots, the height of any fence or wall constructed along the property line shall be determined by using the finished grade of the highest contiguous lot.
- Only one (1) type of fence or wall design shall be permitted on any one (1) parcel or development. The design may include an appropriate mix of materials subject or the approval of the Director.
- The provisions of this Section shall not apply to a fence or wall necessary as required by any law or regulation of the City or the State of California or any agency thereof.
- The use of chain link, barbed wire, electrified fence, or razor wire fence in conjunction with any fence, wall, or hedge, or by itself within any zone, with the exception of chain link used for tennis courts, is prohibited unless required by any law or regulation of the City, the State of California or any agency thereof.

**Parking**

**Purpose**

The purpose of this section is to assure the provision and maintenance of safe, adequate, well designed off-street parking facilities in conjunction with any use or development. The intent is to reduce street congestion and traffic hazards and to promote an attractive environment through design and landscaping standards for parking areas.

**Provision and Design Requirements**

- Off-street parking will be allocated and provided in the 111 West area in accordance with the Standards found in Chapter 17.24 of the City of Rancho Mirage Zoning Ordinance.
- Off-street parking for commercial office and retail uses will be constructed in the form of landscaped parking plazas. These plazas will be shared and maintained by adjacent commercial users.
- Parking plaza design requirements will be in accordance with the guidelines included in this Specific Plan and are in addition to City design standards. Specific Plan design guidelines will take precedence in the event of conflict.

**Reciprocal Parking Opportunities**

- If there is a strong opportunity for a viable reciprocal parking program between two or more properties the required number of spaces may be reduced by the number of shared spaces available. Indicators of a viable reciprocal parking program are as follows:
  - spaces to be shared are within 350 feet of the respective projects.
  - spaces to be shared must experience a different peak usage period as determined by the "Representative Hourly Accumulation by Percentage of Peak Hour" table in Section 17.24.070 of the Rancho Mirage Zoning Ordinance.
  - the spaces to be shared will be available on a continuous basis as evidenced by an agreement between property owners, non-exclusive easement or other instrument as may be approved by the City Attorney.



### **Commercial Vehicle Parking**

- Vehicles that display any form of advertising for a commercial enterprise, including names, phone numbers, logos or associated artwork, are prohibited from parking in street right-of-ways or private parking lots within public view.
- Parking for commercial vehicles within the 111 West area is limited to properly screened loading areas and other approved off-street parking that is properly screened from public view.
- Commercial vehicles include, but are not limited to, cars, trucks, vans, trailers and motorized cycles.

### **Access (CRM 17.18.030A)**

Every building or use shall have frontage upon a public street or permanent means of access to a public street by way of a public or private easement.

### **Signage (17.22)**

All signs shall comply with the provisions of Chapter 17.22 of the Rancho Mirage Zoning Ordinance (Sign Standards).

- The Signage Design Guidelines are found in Chapter 10 of the Specific Plan. Where there is conflict with the Zoning Ordinance, those standards or guidelines that are more restrictive shall apply.

### **Application (17.22.020)**

No non-exempt sign shall be constructed, displayed or altered without a sign permit or sign program approved by the City. A sign program shall be reviewed as part of a Development Plan Permit or Conditional Use Permit application or as deemed necessary by the Director to assure compliance with the provisions of this Chapter. The Director shall review all signs totaling less than twenty (20) square feet. However, the Director may refer a sign application to the Commission. Any sign application or sign program exceeding twenty (20) square feet shall be reviewed by the Commission, consistent with the requirements contained in Chapter 17.46 (Applications and Fees).

### **Applicability (17.22.030)**

No signs shall be erected or maintained in any zone as established by this Title, except those signs specifically

enumerated in this Chapter. The number and area of signs as outlined in this Chapter are intended to be maximum standards which do not necessarily insure architectural compatibility. Therefore, in addition to the enumerated standards, consideration shall be given to a sign's relationship to the overall appearance of the subject property as well as the surrounding community. Compatible design, simplicity, sign effectiveness are to be used in establishing guidelines for sign approval.

- Signage shall be regulated as provided by the Specific Plan Design Guidelines.

### **Outdoor Lighting**

#### **Glare (17.18.030J)**

No glare incidental to any use shall be visible beyond any boundary line of the parcel.

#### **Lighting (CRM17.18.03L)**

Exterior lighting shall be shielded or recessed so that direct glare and reflections are contained within the boundaries of the parcel. No lighting shall blink, flash, or be of unusually high intensity or brightness. All lighting fixtures shall be appropriate in scale, intensity, and height to the use it is serving.

- Outdoor lighting for the illumination of landscaping, buildings, signage, parking plazas and pathways shall be in conformance with the Specific Plan Design Guidelines. Lighting shall be designed and installed so that all direct rays are confined to the site and adjacent properties are protected from glare. Light standards shall not exceed fifteen (15) feet in height or the height of the building, whichever is less.

### **Trash Storage (17.18.030Q)**

Every parcel with a building or structure shall have a trash receptacle on the premises. The trash receptacle shall be of sufficient size to accommodate the trash generated. The receptacle shall be screened from public view on at least three (3) sides by a solid wall six (6) feet in height and on the fourth side by a solid gate not less than five (5) feet in height. The gate shall be maintained in working order and shall remain closed except when in use. The wall and gate shall be architecturally compatible with the surrounding buildings and structures. Trash receptacles for single family homes shall be stored within the enclosed garage.

### **Equipment Areas**

#### **Storage (17.18.030W)**

There shall be no visible storage of motor vehicles, trailers, airplanes, boats or their composite parts; loose rubbish, garbage, junk, or their receptacles; tents, or building materials in any portion of a lot; with the exception of recreational vehicles pursuant to Section 17.06.030 (M). No storage shall occur on any vacant parcel.

Building materials for use on the same premises may be stored on the parcel during the time that a valid building permit is in effect for construction.

- Equipment, and mechanical devices shall not be located in any required setback area or side yard except for electrical or telephone equipment installed by the utilities. Screening shall be provided so that equipment located in the area is screened from view from all adjacent streets and properties. Screening of equipment areas shall be accomplished with materials and designs that are compatible with the architectural character of the building.

### **Loading and Delivery Areas**

- Off-street loading spaces will be allocated and provided for in accordance with the standards found in Chapter 17.26 of the City of Rancho Mirage Zoning Ordinance.
- Areas for receiving and loading of materials on the premises of commercial uses must be located away from the public street to which the use is oriented. To the greatest extent possible, loading areas should be screened from all public streets. This shall be accomplished through careful site planning, and the use of screen walls and landscaping in conformance with the Specific Plan Design Guidelines.

### **Undergrounding of Utilities (17.18.030Y)**

Utilities shall be placed underground pursuant to Rancho Mirage Municipal Code Chapter 12.16. In the event an above-ground electrical transformer is located outdoors on any site, it shall be screened from view with a solid wall or landscaping and not located in any setback area. If it cannot be screened it shall be located in an underground vault.



**Nonconforming Buildings Structures and Uses**

***Purpose (CRM17.30.010)***

These provisions provide for the orderly termination of nonconforming buildings, structures, and uses to promote the public health, safety, and general welfare, and to bring these buildings, structures and uses into conformity with the goals and policies of the General Plan. This Chapter is intended to prevent the expansion of nonconforming buildings, structures, and uses, to establish the criteria under which they may be continued, and to provide for the correction or removal of these zoning nonconformities in an equitable and timely manner.

It is hereby declared that nonconforming buildings, structures, and uses within the City are detrimental to both orderly and creative development, and the general welfare of citizens and property. It is further declared that nonconforming buildings, structures, and uses shall be eliminated as rapidly as possible without infringing upon the constitutional rights of property owners.

***Nonconforming Buildings and Structures (17.18.020)***

A building or structure which lawfully existed prior to the effective date of this Title in nonconforming, if the building or structure fails to conform to the present requirements of the zoning district in which it is located.

- a. A nonconforming building or structure which is damaged to an extent of one-half (½) or more of its replacement cost immediately prior to such damage may be restored only if made to conform to all provisions of this Title. The replacement cost shall be determined by the Chief Building Official, whose decision may be appealed to the Council.
- b. Any additional development of a parcel with a nonconforming building or structure will require that all buildings and structures be brought into conformance with this Title.
- c. If the use of a nonconforming building or structure is discontinued for a period of six (6) or more consecutive calendar months, the building shall lose its nonconforming status, and shall be removed or altered to conform to the provisions of this Title. A use of a nonconforming building or structure shall be

considered discontinued when any of the following apply:

- 1. The intent of the owner to discontinue use of the nonconforming building or structure is apparent.
- 2. Where characteristic furnishings and equipment of the use have been removed and not replaced with equivalent infrastructure during this time.
  - When any basic utility services are disconnected for a period of six months or more.

***Nonconforming Uses (17.30.030)***

A nonconforming use is one which lawfully existed prior to the effective date of this Title, but which is not now permitted in the zone in which it is located. The continuance of a nonconforming use is subject to the following:

- a. Change of ownership, tenancy, or management of a nonconforming use shall not affect its nonconforming status, provided that the use and intensity of use does not change.
- b. If a nonconforming use ceases for a period of six (6) or more consecutive months, it shall lose its nonconforming status, and the continued use of the property shall be required to conform with the provisions of this Title.
- c. Additional development of any property on which a nonconforming use exists shall require that all uses conform to the provisions of this Title.
- d. If a nonconforming use is converted to a conforming use, the nonconforming use may not be resumed.
- e. No nonconforming use may be replaced by another nonconforming use, nor may any nonconforming use be expanding.

***Building Permits or Certificates of Occupancy Prohibited (17.30.040)***

When any nonconforming building, structure, or use is no longer permitted pursuant to the provisions of this Chapter, no building permit shall thereafter be issued for further continuance, alteration, or expansion. Any permit issued in error shall not be construed as allowing the continuation of the nonconforming building, structure, or use.

***Removal of Illegal Nonconforming Buildings, Structures, and Uses (17.30.050)***

Nothing contained in this Chapter shall be construed or implied so as to allow for the continuation of illegal nonconforming buildings, structures and uses. Said buildings, structures, and uses shall be removed immediately subject to the provisions of Chapter 17.48 (Enforcement of Provisions) and State law.

# Section 4

## DESIGN GUIDELINES

### 6. Guideline Framework

#### PURPOSE AND INTENT

The Design Guidelines provided herein have been devised as a method of achieving a high quality, cohesive design fabric for the Highway 111 West Specific Plan Area.

Additional **objectives** of these guidelines are:

- To provide the City of Rancho Mirage with the necessary assurance that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- To serve as design criteria to developers, builders, engineers, architects, landscape architects, and other professionals in preparing plans for construction; and
- To lend guidance to staff, the Planning Commission and the City Council in the review and evaluation of future development projects in the Specific Plan area.

There are certain key design elements which will contribute significantly to the visual order and consistency of the entire Specific Plan area. These common features - site planning, architecture, landscape design, parking, signage, lighting, and other details - are the subject of this Chapter.

The guidelines express the desired character of future development. Each guideline shall be considered in terms of how it applies to a given project. The intent of the guidelines must be met in order for a project to be approved during the Development Plan Review process.

The Design Guidelines are intended to be flexible and are illustrative in nature. Over time, they can therefore respond to unanticipated conditions, such as changes in taste, community desires and the marketplace.

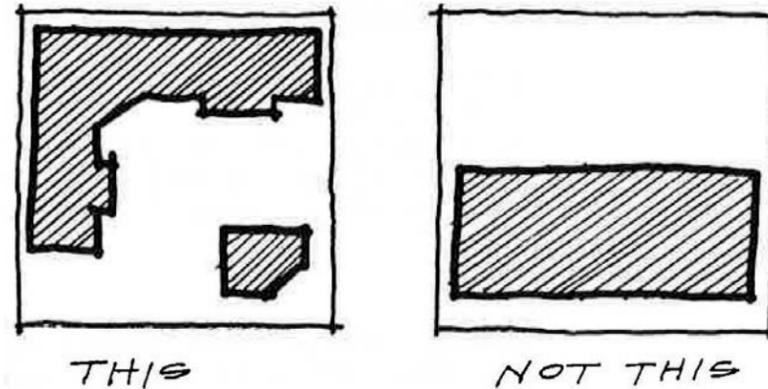
### 7. Site Planning Guidelines

Site planning concentrates on the proper placement of buildings, roads and services.

#### ORIENTATION OF STRUCTURES

A key design issue that can allow building expansion, or prevent it, is the location of structures on site.

- In districts that encourage pedestrian activity and connections between various developments, buildings shall be arranged in “U” and “L” shapes that open up to streets to allow visibility into, and circulation in front of, major structures. This type of design will allow linkages between different parcels and encourages designs that prevent structures from blocking circulation.



- On corner sites or at major developments, satellite buildings shall be located closer to the street or at the corner, while the bulk of the building is placed far enough back from the street to allow circulation across the front of the parcels.
- Developments that are inward facing and that preclude through access are not encouraged, except when major attractions are destinations.
- Courtyards and outdoor areas are encouraged.
- Other ancillary structures, service areas, and trash enclosures shall be planned as part of the project, and placed or screened in a position of low prominence.

#### SITE COVERAGE

Coverage regulates the area of the site that may be covered by the building. Buildings with usable, internal floor area that spans above or overhangs ground area must include this “shadow” as part of the structural coverage. Covered walkways, roof structure overhangs and other solar protection or aesthetic structural elements should not be included in building coverage calculations. These guidelines also help protect area dedicated to landscape, access, or parking.

4. Coverage of a site by a building structure shall not exceed 25 percent of the total site for commercial uses as set forth by the Building Regulation Matrix. This coverage may be increased, subject to the approval of the Community Development director, if the project demonstrates superior response to the Specific Plan Design Guidelines. In no case shall site coverage exceed 35 percent.
5. When parking structures are provided, such structures shall not cover more than an additional 30 percent of the groundplane.
6. In all site plan configurations, 35% of the site shall be landscaped open areas at ground level, except landscaped trellises to shade parking spaces may count toward fulfilling landscape coverage requirements, subject to the approval of the Community Development Director.

In this ultimate case 55-65 percent of the area would be a built environment, while 35-45 percent of the site must be open at the ground level. Of this, a minimum of 25 percent shall be dedicated to landscape, leaving the remainder for buildable entries, circulation, service areas, and surface parking.

#### GRADING

Grading guidelines have been formulated with the intent of preserving and enhancing the natural terrain whenever feasible.

- Parking lots shall drain and have minimum and maximum percent grades as set forth by the City Engineer.
- Grading shall conform to natural topography and result in a harmonious transition of the man-made grades with the natural terrain.
- Man-made land forms shall be graded to avoid unnatural sharp or straight edges and planes. The top



and toe of graded slopes shall be rounded to avoid a harsh machine-made appearance.

- Grading design should complement and reinforce architectural and landscape design by helping to screen parking, loading and service areas, by reducing the perception of height and mass of larger buildings, by providing reasonable transitions between on-site uses and adjacent lots, and by providing elevation transitions contributing to the efficiency of on-site and off-site circulation systems.
- Berms are to be graded in gentle, undulating naturalistic forms: no straight or steep slopes. Provisions are to be made for drainage around or through berms as required. Generally, a height of 36 inches from top of adjacent curb is the maximum desired.
- Graded slopes shall meet City Grading and Drainage Ordinance requirements.
- Steep slopes of 2.5:1 or more shall be broken with retaining walls, cribbing steps of approved materials (see Walls and Fences).
- All graded slopes shall be stabilized and planted with approved trees, shrubs and groundcover as listed in the Landscape Guidelines.

#### DRAINAGE

- Natural drainageways shall be preserved whenever feasible.
- Improved drainageways, if lined, shall use rock native to the area.
- Drainage velocities shall be minimized to protect from erosion, debris accumulation on streets and drainageways, and to prevent hazardous flow conditions.
- Swales (small ditches) are to be graded shallow, but wide, to slow runoff. Avoid steep cuts for a natural look; buried vertical drain lines are the preferred alternative.

- All proposed drainage shall comply with the drainage standards and ordinances administered by the City.
- Natural erosion control is encouraged. Plant species useful for slope stabilization and erosion control are listed in the Landscape Guidelines.

#### LOADING STORAGE, REFUSE AND SERVICE AREAS

- Off-street loading spaces will be allocated and provided for in accordance with the standards found in Chapter 17.26 of the City of Rancho Mirage Zoning Ordinance.
- Loading standards may be relaxed in light of superior project design so long as the adequacy of the proposed loading spaces can be demonstrated.
- Loading docks or staging areas shall be located toward the rear or storage side-yard of buildings wherever possible, set back and recessed and/or screened so as not be visible from neighboring properties or public rights-of-way.
- In no event shall a loading dock be closer than seventy-five (75) feet from a property line fronting upon a street unless it is at the rear of a building, and no storage area may extend into a setback area, unless otherwise approved in writing by the Community Development Director.
- Provisions shall be made on each site for all necessary vehicle loading. No on-street vehicle loading shall be permitted.
- No materials, supplies or equipment, including trucks or other motor vehicles, shall be stored upon any site in any commercial area except inside a closed building or behind a barrier screen so as not to be visible from neighboring properties and streets.
- Temporary buildings such as portable modules, mobile homes and metal system buildings are prohibited. Trailers associated with on-site construction activity are permissible only during construction.
- Trash enclosures, rubbish bins, transformers, processing equipment and any other unsightly

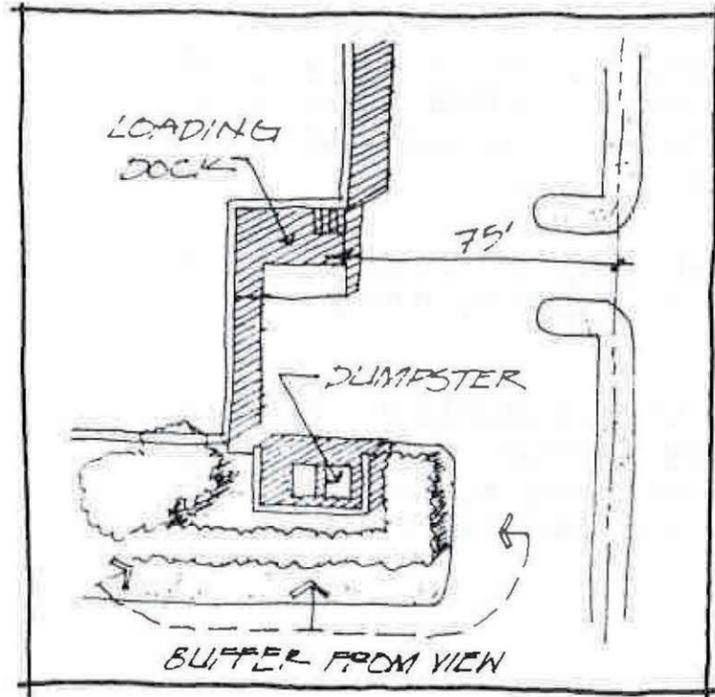
apparatus shall be situated away from the street and located toward rear yard areas, and shall be architecturally screened.

- Refuse collection areas shall be effectively designed and located on the lot to conveniently contain all refuse generated on-site and deposited between collections. Deposited refuse shall not be visible from outside the refuse enclosure.
- Refuse collection areas shall be located upon the lot as to provide clear and convenient access to refuse collection vehicles.
- No refuse collection areas shall be permitted between a street and the front of a building.
- Trash storage areas shall be provided in accordance with the requirements of Section III, Chapter 5 of the Specific Plan (Development Regulations).
- All outdoor refuse containers shall be visually screened within a durable six (6) foot non-combustible enclosure of approved materials and finishes, so as not to be visible from neighboring properties or streets.
- Trash enclosures shall be provided with full-height wood or metal doors on metal frames, with landscape on at least two sides.
- Screen walls and refuse collection enclosures should be visually connected to the primary structure where possible and designed to be unified and harmonious in material, finish and color with the overall architectural theme.

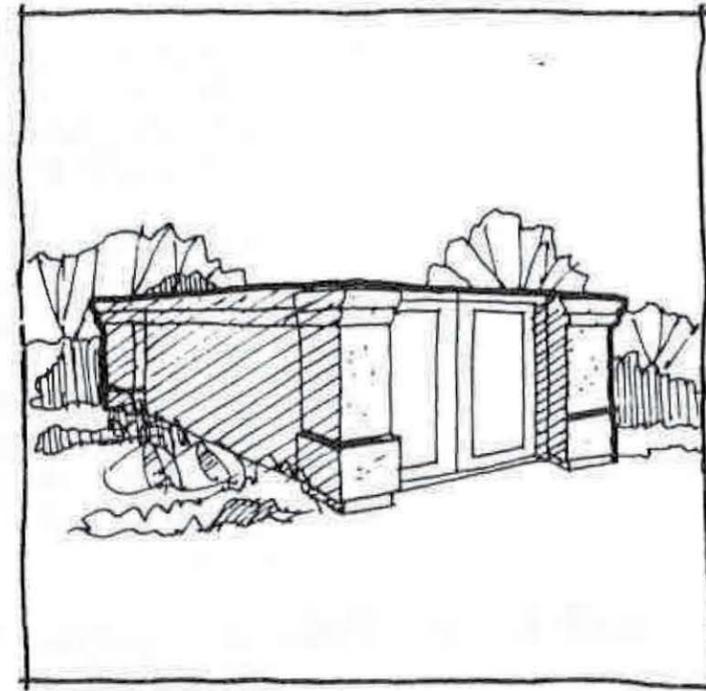


CITY OF RANCHO MIRAGE

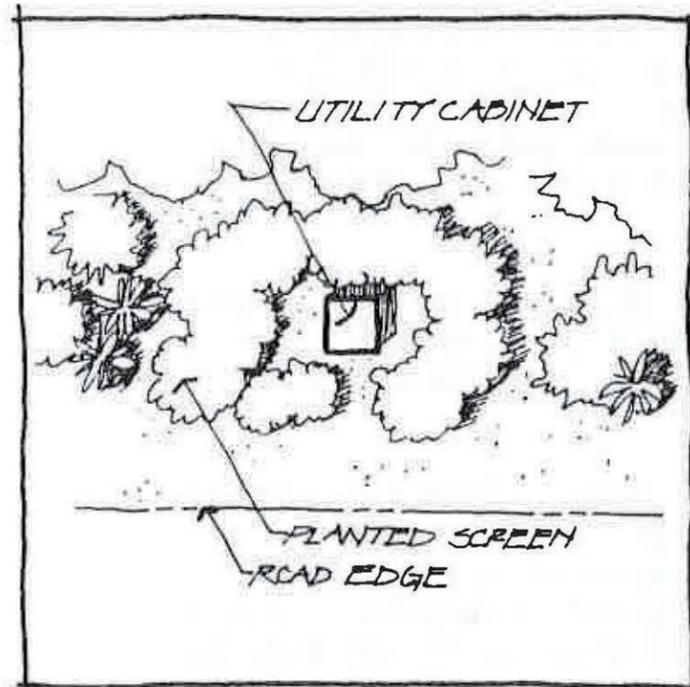
**ILLUSTRATION 03**  
Loading Area and Utility Screening



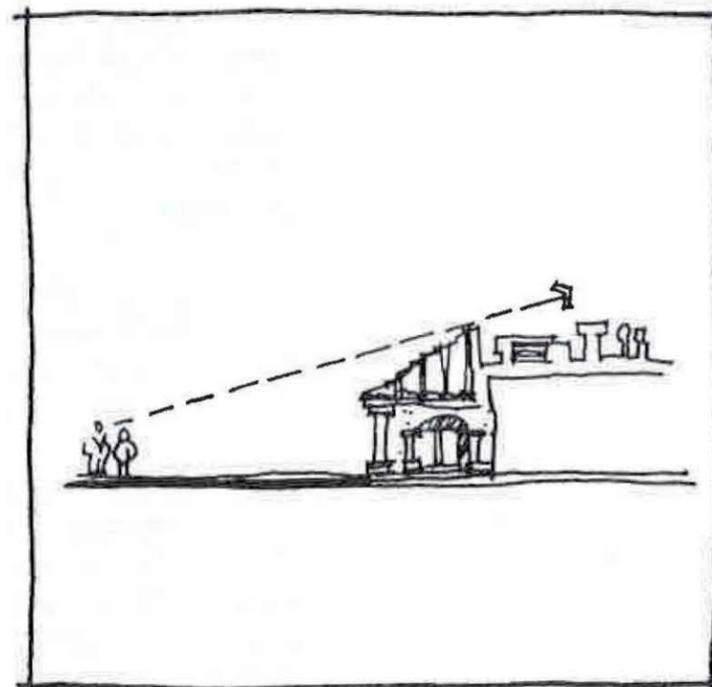
Refuse and loading areas shall be located to the rear or side of buildings and screened.



Refuse enclosures shall reflect the architecture of surrounding buildings.



Utility cabinets shall be screened with plantings.



Rooftop mechanical equipment shall not exceed the height of the parapet.



## UTILITIES AND EXTERIOR EQUIPMENT

- All utilities, including drainage systems, sewer, gas and water lines, electrical, telephone, and communications wires and related equipment shall be installed and maintained underground.
- Temporary overhead power and telephone facilities shall be permitted only during construction.
- No antenna or device for transmission or reception of any signals including telephone, television and/or radio, shall be placed on any lot so that it is visible from 5 feet above the ground or ground floor level at a distance of 500 feet in any direction, unless specific written approval is granted by the Community Development Director.
- No mechanical equipment (including, but not limited to, components of plumbing, processing, heating, cooling, and ventilating systems) shall be visible on-site or from adjacent property.
- No exterior components of such mechanical equipment (e.g. piping, stacks and ductwork, fans and compressors) shall be mounted on any building wall unless they are an integrated architectural design feature. Any such components shall only be permitted with the written approval of the Community Development Director.
- Roof mounted mechanical equipment shall be hidden from view by building parapets of equal height.
- If building parapets do not provide adequate screening of mechanical equipment from the upper floors of adjacent buildings, screening shall be installed as an integral part of the overall architectural design, and painted such a color as to allow its blending with its visual background.
- Electric transformers, utility pads, cable TV and telephone boxes shall be located out of public rights-of-way and undergrounded or screened with walls, fences or vegetation or otherwise enclosed in a manner harmonious with the overall architectural theme.

## 8. Landscape Guidelines

The landscape concept for the Highway 111 Corridor has been conceived to organize and present a memorable image for the Corridor as a whole and allow for the identification of individual districts within the Specific Plan area. Its purpose is to establish a unified landscape framework that provides continuity and enhances the streetscape while encouraging a variety of distinct district theme statements. A high degree of landscape quality is to be maintained throughout the Specific Plan Area. Key elements to implementation of the concept are set forth in Exhibit 24, Landscape Plan.

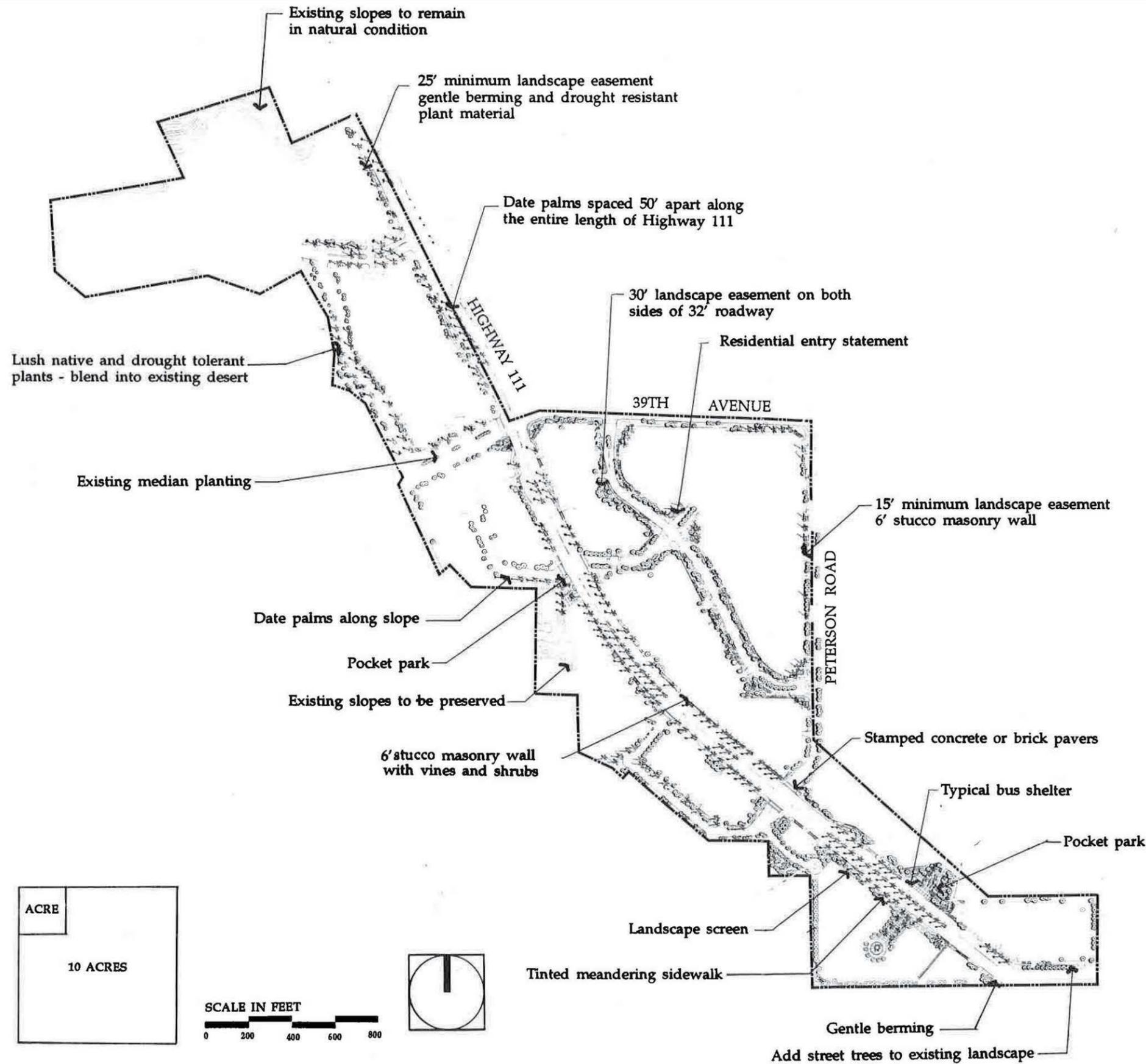
### GENERAL PROVISIONS

1. A simple design character that is easy to maintain and is unique relative to other commercial areas along the Corridor is sought. The overriding goal is to provide a positive visual experience for all persons within the Corridor.
  - Trees, both in lines and masses, shall be utilized to enclose exterior spaces and provide physical protection from the sun and wind in paved areas.
  - Areas with adjacent parcels reserved for future expansion shall fully install the required landscape areas adjacent to street at the time and the first phase of development occurs.
2. The form and massing of street front plant materials shall be consistent in application from district to district, but the materials and colors may change to conform to the district theme. Each district uses a suggested planting palette which lists appropriate landscape materials that may be used in that area. Substitutions may be granted based upon Community Development Department staff approval.
3. Landscape plant materials must be “long-lived” varieties. “Short-lived” materials such as flowering annuals and perennials may be utilized to accent or augment the “longer-lived” elements which form the landscape framework.
4. Landscape elements within the front building setback visible from the public right-of-way shall blend and connect with street edge landscaping.
5. Where hedges are used, they may be clipped and geometric in shape, or unpruned and naturalistic where appropriate and in context with the design theme, avoiding vehicular sight hazards.
6. Building perimeter and parking lot landscape areas shall be planted with trees, shrubs and groundcover selected from the appropriate plant selection guide (see each District) to conform to the design theme.



CITY OF RANCHO MIRAGE

**EXHIBIT 21**  
Landscape Plan





**SPECIFICATIONS**

- All front, rear and side yard areas of each lot and adjacent right-of-way shall be landscaped and maintained by the property owner.
- A minimum of at least 25% of the lot area within the property lines (including setbacks) shall be devoted to landscape materials, unless otherwise approved in writing by the Community Development Director. Priority will be placed on street fronting areas.
- The minimum tree size for all areas shall be 24-inch box. In addition, mature specimen trees larger than 24-inch box shall be provided in sufficient quantity to provide variety and emphasis of focal areas in the landscaping plan.
- Plantings shall screen and soften structures where necessary without obscuring tenant recognition or creating hazards to users. The location and type of plant material is critical for screening unattractive areas of buildings and framing views from buildings.
- Walls or other ancillary structures with more than 160 square feet of continuous surface area shall be required to have vines or trellis plants along them.

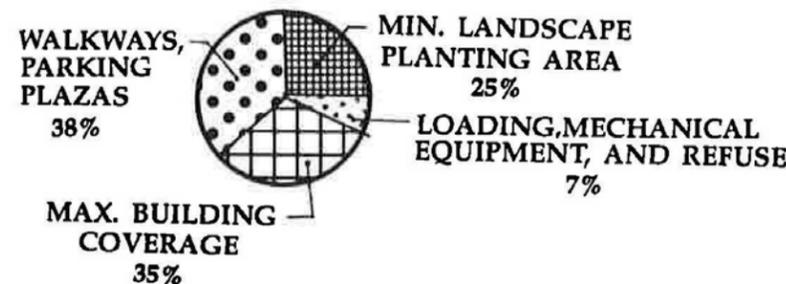
**LANDSCAPING OF COMMERCIAL PARKING AREAS**

- Where possible, siting parking areas lower than adjacent roadways and continuing streetscape grading, berms, hedges, and other landscape treatment into parking areas is encouraged, with intent to reduce their visual impact and to screen the parking from the adjacent roadway.
- Every commercial parking lot shall have a wall or hedge (not to exceed 3 feet in height on those edges facing public streets) if not surrounded by a berm.
- Planting “islands” between parking bays shall measure six (6) feet from the outside edge of the curb (or five (5) feet inside dimension) to provide adequate space for tree trunks, hedges or parking lot light standards.
- To increase the parking lot landscaped area, a maximum of 2½ feet of the parking space depth may be added to the planting islands and landscaped in lieu of asphalt while maintaining the required parking dimensions.
- As appendages to or at the ends of planting islands, planting fingers shall be used to provide additional planting area for trees and ground cover. However, trees shall not be located in areas into which car doors might open. Planting fingers shall be separated by no more than an average of five (5) parking spaces, and shall have minimum dimensions of six (6) feet wide by twenty (20) feet long. Maximum separation between planting fingers shall be no more than eight (8) parking spaces.
- Within parking areas, trees shall be installed at a rate of one for every three (3) parking spaces. In uncovered parking space areas, trees shall be spaced no more than thirty (30) feet on center.
- The City may require up to 50% of required parking spaces be covered by shade structures, in addition to shade tree requirements.
- Shade structures shall be separated from uncovered parking spaces or other shade structures by minimum six (6) feet wide by twenty (20) feet long planting fingers. A single shade structure “unbroken” by planter

fingers shall cover no more than eight (8) parking spaces.

- Parking “plaza” landscaping shall be vertically layered through the creation of an overstory of fan palms, a mid-story of canopy trees and an understory of shrubs and groundcover.

**TYPICAL LOT**

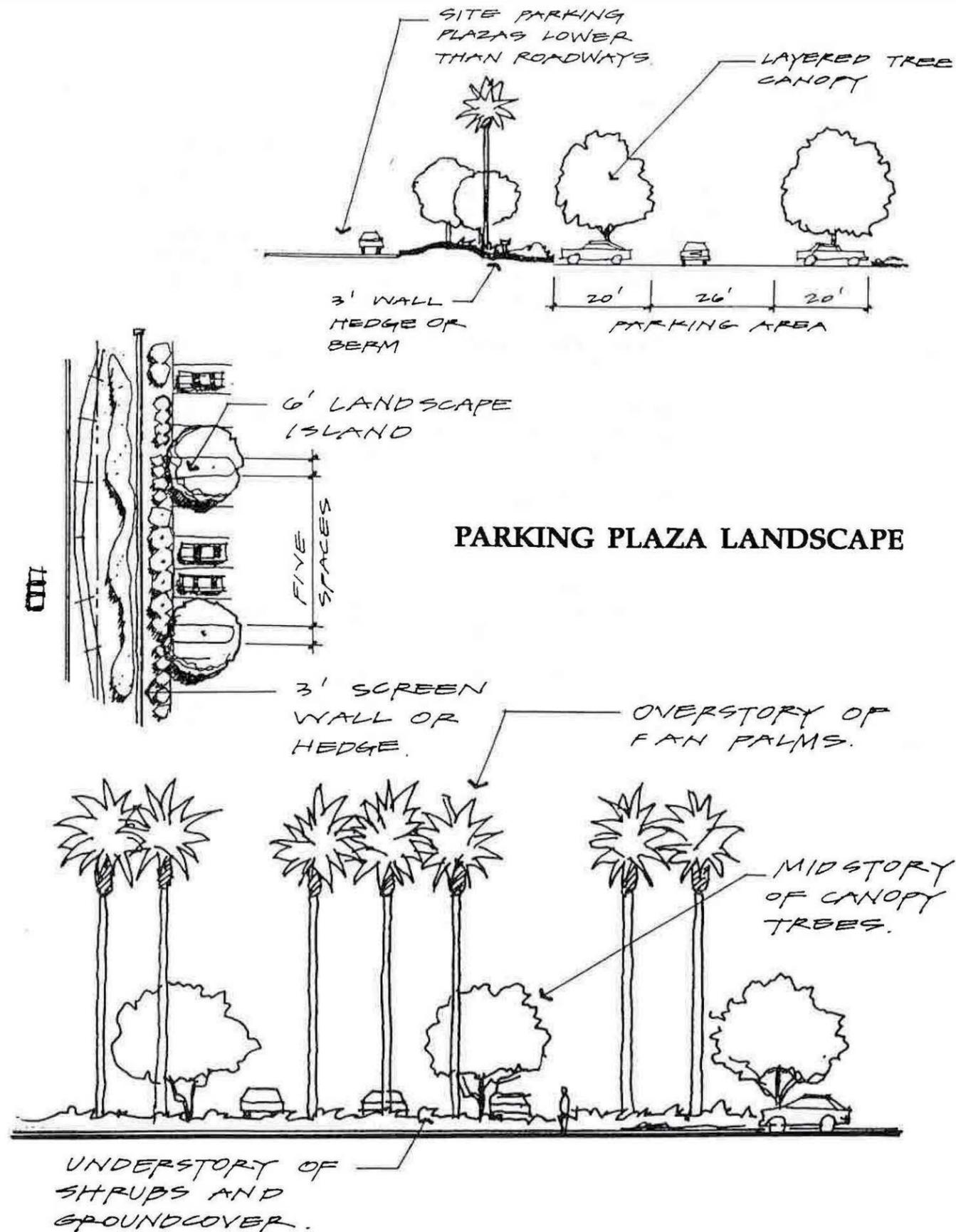




CITY OF RANCHO MIRAGE

### ILLUSTRATION 04

Parking Plaza Landscape



**LANDSCAPE PLANT PALETTE RECOMMENDATIONS**

The following plant palette has been derived to enhance the Rancho Mirage desert landscape theme. The plant material has been selected to accent the desert environment while providing important functional characteristics. This list is intended to be a menu from which the landscape architects can mix and match plant materials for various situations. The following general criteria have been established to determine plant selections:

1. **Desert Compatible Plants** -- Plants must be tolerant of the harsh desert environment.
2. **Low/Moderate Water Consumption** -- Plants should not be high water consumers. Vast expanses of lawn areas are to be discouraged, except for park areas.
3. **Broadleaf Deciduous Trees** -- These plants should be used in areas where the need for summer shade and winter sunshine is important.
4. **Groundcover** -- Use of drought tolerant groundcover and decomposed granite should be encouraged to reduce blowing sand and dust.
5. **Maintenance of Established Themes** -- Maintain the traditional Rancho Mirage Highway 111 Streetscape theme (i.e., Date Palms evenly spaced and desert shrubbery throughout).



## RANCHO MIRAGE OVERALL PLANT PALETTE

PALM		LARGE SHADE TREES		DESERT TREES	
<b><u>BOTANICAL NAME</u></b> <i>Arecastrum romanzoffianum</i> <i>Brahea armata</i> <i>Chamaerops humilis</i> <i>Phoenix dactylifera</i> <i>Washingtonia filifera</i> <i>Washingtonia robusta</i>	<b><u>COMMON NAME</u></b> Queen Palm Mexican blue palm Mediterranean fan palm Date palm California fan palm Mexican fan palm	<b><u>BOTANICAL NAME</u></b> <i>Brachychiton populneus</i> <i>Ficus nitida</i> <i>Fraxinus velutina</i> <i>Platanus Wrightii</i> <i>Olea europaea</i>	<b><u>COMMON NAME</u></b> Bottle tree Indian laurel fig Arizona ash Arizona sycamore "Swan Hill" olive	<b><u>BOTANICAL NAME</u></b> <i>Acacia stenophylla</i> <i>Cercidium floridum</i> <i>Parkinsonia aculeata</i> <i>Prosopis alba</i> <i>Prosopis chilensis</i>	<b><u>COMMON NAME</u></b> Shoestring acacia Blue palo verde Mexican palo verde Argentine mesquite Chilean mesquite
STREET TREES (NON-DESERT)		STREET TREES (DESERT THEME)		ACCENT TREES	
<b><u>BOTANICAL NAME</u></b> <i>Brachychiton populneus</i> <i>Eucalyptus species</i> <i>Fraxinus velutina</i> <i>Platanus wrightii</i>	<b><u>COMMON NAME</u></b> Bottle tree Various Arizona ash Arizona sycamore	<b><u>BOTANICAL NAME</u></b> <i>Cercidium floridum</i> <i>Olea europaea</i> <i>Parkinsonia aculeata</i> <i>Prosopis alba</i> <i>Prosopis chilensis</i>	<b><u>COMMON NAME</u></b> Blue palo verde 'Swan Hill' olive Mexican palo verde Argentine mesquite Chilean mesquite	<b><u>BOTANICAL NAME</u></b> <i>Blue palo verde</i> <i>'Swan Hill' olive</i> <i>Mexican palo verde</i> <i>Argentine mesquite</i> <i>Chilean mesquite</i>	<b><u>COMMON NAME</u></b> Silk tree Purple orchid tree Jacaranda Crape myrtle Yellow oleander
LARGE SHRUBS		MEDIUM/SMALL SHRUBS		GROUNDCOVER	
<b><u>BOTANICAL NAME</u></b> <i>Baccharis sarothoides</i> <i>Caesalpine pulcherrima</i> <i>Cassia nemophylla</i> <i>Cistus purpureus</i> <i>Cistus ladaniferus</i> <i>Dodonaea viscosa</i> <i>Fouquieria splendens</i> <i>Leucophyllum frutescens</i> <i>Liqustrum japonicum</i> <i>Nerium oleander</i> <i>Photinia fraseri</i> <i>Pittosporum tobira</i> <i>Pittosporum tobira riegata'</i> <i>Thevetia peruviana</i> <i>Xylosma congestum</i>	<b><u>COMMON NAME</u></b> Desert broom Red bird of paradise Senna Orchid rock rose Crimson spot rock rose Hopseed bush Ocotillo Texas Ranger Japanese privet Petite Pink & 'Petite Salmon' Fraser photinia Mock orange Variegated tobira Yellow oleander Shiny xylosma	<b><u>BOTANICAL NAME</u></b> <i>Agave shawii</i> <i>Aloe species</i> <i>Encelia farinosa</i> <i>Euonymus japonica</i> <i>Euryops virides</i> <i>Ferocactus species</i> <i>Hemerocallis</i> <i>Hesperaloe parviflora</i> <i>Lantana camara</i> <i>Lysiloma thornberi</i> <i>Opuntia violacea</i> <i>Santolina virens</i> <i>Simmondsia chinensis</i>	<b><u>COMMON NAME</u></b> Shaw's century plant Aloe Encelia Euonymus Green euryops Barrel cactus Daylily Red yucca Radiation lantana Fern of the desert 'San Rita' prickly pear Santolina Jojoba	<b><u>BOTANICAL NAME</u></b> <i>Acacia redolens 'prostrata'</i> <i>Baccharis pilularis</i> <i>Lantana sellowiana</i> <i>Lonicera japonica</i> <i>Santolina chamaecyparissus</i> <i>Trachelospermum jasminoides</i>	<b><u>COMMON NAME</u></b> Trailing acacia 'Centennial' Trailing lantana Honeysuckle Lavender cotton Star jasmine



## RANCHO MIRAGE OVERALL PLANT PALETTE

VINES		TURF			
<b><u>BOTANICAL NAME</u></b> <i>Bougainvillea spectabilis</i> <i>Ficus pumila</i> <i>Lonicera japonica</i> <i>Rosa banksiae</i>	<b><u>COMMON NAME</u></b> Various Creeping fig Hall's honeysuckle Lady Bank's rose	<b><u>BOTANICAL NAME</u></b> <i>Tifgreen or 'Santa Ana'</i>	<b><u>COMMON NAME</u></b> Hybrid bermuda		



## MAJOR INTERSECTIONS/HIGHWAY 111

TREES		LARGE SHRUBS		SMALL SHRUBS	
<b><u>BOTANICAL NAME</u></b> <i>Brahea armata</i> <i>Jacaranda mimosifolia</i> <i>Olea europaea</i> <i>Phoenix dactylifera</i> <i>Washingtonia species</i>	<b><u>COMMON NAME</u></b> <i>Mexican blue palm</i> <i>Jacaranda</i> <i>'Swan Hill' olive</i> <i>Date palm</i> <i>Fan palm</i>	<b><u>BOTANICAL NAME</u></b> <i>Baccharis sarothoides</i> <i>Cassia nemophylla</i> <i>Caesalpinea pucherrima</i> <i>Fouquieria splendens</i> <i>Leucophyllum frutescens</i> <i>Thevetia peruviana</i>	<b><u>COMMON NAME</u></b> <i>Desert broom</i> <i>Feathery cassia</i> <i>Mexican bird of paradise</i> <i>Ocotillo</i> <i>'Green Cloud'</i> <i>Yellow oleander (background)</i>	<b><u>BOTANICAL NAME</u></b> <i>Aloe species</i> <i>Encelia farinosa</i> <i>Ferocactus species</i> <i>Hesperaloe parviflora</i> <i>Nerium oleander</i> <i>Opuntia violacea</i> <i>Simmondsia chinensis</i>	<b><u>COMMON NAME</u></b> <i>Aloe</i> <i>Encelia</i> <i>Barrel cactus</i> <i>Red yucca</i> <i>'Petite salmon' or petite pink</i> <i>'San Rita' prickly pear</i> <i>Jojoba</i>
GROUNDCOVERS		VINES			
<b><u>BOTANICAL NAME</u></b> <i>Annuals</i> <i>Acacia redolens</i> <i>Baccharis pilularis</i> <i>Lantana sellowiana</i> <i>Hybrid bermuda</i> <i>Decomposed Granite</i>	<b><u>COMMON NAME</u></b> <i>Various</i> <i>Trailing acacia</i> <i>'Centennial'</i> <i>Trailing lantana</i> <i>Lawn</i> <i>1/2" minus golden</i>	<b><u>BOTANICAL NAME</u></b> <i>Bougainvillea spectabilis</i> <i>Rosa Banksiae</i>	<b><u>COMMON NAME</u></b> <i>Various</i> <i>Lady Bank's rose</i>		



## RESIDENTIAL ENTRY

TREES		LARGE SHRUBS		SMALL SHRUBS	
<b><u>BOTANICAL NAME</u></b> <i>Albizia julibrissin</i> <i>Bauhinia variegata (accent)</i> <i>Brachychiton populneus</i> <i>Fraxinus velutina</i> <i>Washingtonia speciosa</i>	<b><u>COMMON NAME</u></b> Silk tree (accent) Purple/white orchid tree Bottle tree Arizona ash Fan palm	<b><u>BOTANICAL NAME</u></b> <i>Caesalpine pulcherrima</i> <i>Chamaerops humilis palm</i> <i>Dodonaea viscosa</i> <i>Nerium oleander</i> <i>Pittosporum tobira variegata</i> <i>Xylosma congestum</i>	<b><u>COMMON NAME</u></b> <i>Red bird of paradise</i> Mediterranean fan <i>Hopseed bush</i> <i>'Petite Salmon' or Petite Pink</i> <i>Variegated tobira</i> Shiny xylosma	<b><u>BOTANICAL NAME</u></b> <i>Buccharis sarothoides</i> <i>Lantana camara</i> <i>Simmondsia chinensis</i>	<b><u>COMMON NAME</u></b> Desert broom Radiation lantana Jojoba
GROUNDCOVERS					
<b><u>BOTANICAL NAME</u></b> <i>Annuals</i> <i>Baccharis pilularis</i> <i>Lantana sellowiana</i> <i>Trachelospermum jasminoides</i> <i>'Tifgreen' or 'Santa Ana'</i>	<b><u>COMMON NAME</u></b> Various 'Centennial' Trailing lantana Star jasmine Hybrid bermuda				



## HIGHWAY 111 STREETScape

PALMS		TREES		SHRUBS	
<b><u>BOTANICAL NAME</u></b> <i>Phoenix dactylifera</i> <i>Washingtonia robusta</i>	<b><u>COMMON NAME</u></b> Date palm (Theme) Mexican fan palm (Accent)	<b><u>BOTANICAL NAME</u></b> <i>Acacia stenophylla</i> <i>Brachychiton populneus</i> <i>Cercidium floridum</i> <i>Eucalyptus species</i> <i>Olea europaea</i> <i>Parkinsonia aculeata</i> <i>Prosopis alba</i> <i>Prosopis chilensis</i>	<b><u>COMMON NAME</u></b> Shoestring acacia Bottle tree Blue palo verde Various 'Swan Hill' olive Mexican palo verde Argentine mesquite Chilean mesquite	<b><u>BOTANICAL NAME</u></b> <i>Agave shawii</i> <i>Aloe species</i> <i>Baccharis sarthoides</i> <i>Caesalpinea pulcherrima</i> <i>Cassia nemophylla</i> <i>Cistus ladaniferus</i> <i>Cistus purpureus</i> <i>Dodonaea viscosa</i> <i>Ferocactus species</i> <i>Fouquieria splendens</i> <i>Hesperaloe parviflora</i> <i>Leucophyllum frutescens</i> <i>Lysiloma thornberi</i> <i>Opuntia violacea</i> <i>Thevetia peruviana</i>	<b><u>COMMON NAME</u></b> Shaw's century plant Aloe Desert broom Mexican bird of paradise Cassia Crimson spot rock rose Orchid rock rose Hopseed bush Barrel cactus Octotillo Red yucca Texas Ranger Fern of the desert 'San Rita' prickly pear Yellow oleander
GROUNDCOVER		VINES			
<b><u>BOTANICAL NAME</u></b> <i>Acacia redolens 'prostrata'</i> <i>Baccharis pilularis</i> <i>Santolina chamaecyparissus</i> <i>Santolina virens</i>	<b><u>COMMON NAME</u></b> Trailing acacia 'Centennial' Lavender cotton Santolina	<b><u>BOTANICAL NAME</u></b> <i>Bougainvillea spectabilis</i> <i>Rosa banksiae</i>	<b><u>COMMON NAME</u></b> Various Lady Bank's rose Add lawn in "Pocket Parks"		



## RESIDENTIAL STREETSCAPE

TREES		LARGE SHRUBS		SMALL SHRUBS	
<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>
<i>Acacia stenophylla</i>	Shoestring acacia	<i>Caesalpinea pulcherrima</i>	Mexican bird of paradise	<i>Encelia farinosa</i>	Encelia
<i>Bauhinia variegata</i>	Purple orchid tree (accent)	<i>Cistus species</i>	Rockrose	<i>Euryops virides</i>	Euryops
<i>Brachychiton populneus</i>	Bottle tree	<i>Leucophyllum frutescens</i>	Texas Ranger	<i>Hemerocallis</i>	Daylily
<i>Citrus species</i>	Ornamental & producing	<i>Nerium oleander</i>	'Petite Salmon' or Petite Pink	<i>Lantana camara</i>	Radiation lantana
<i>Eucalyptus species</i>	Various	<i>Photinia fraseri</i>	Fraser photinia	<i>Simmondsia chinensis</i>	Jojoba
<i>Fraxinus velutina</i>	Arizona ash	<i>Xylosma congestum</i>	Shiny xylosma		
<i>Jacaranda mimosifolia</i>	Jacaranda				
<i>Lagerstroemia indica</i>	Crape myrtle (accent)				
GROUNDCOVER		VINES			
<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>		
<i>Baccharis pilularis</i>	'Centennial'	<i>Bougainvillea spectabilis</i>	Various		
<i>Lantana sellowiana</i>	Trailing lantana	<i>Rosa banksiae</i>	Lady Bank's rose		
<i>Lonicera japonica</i>	Honeysuckle				
Decomposed Granite (Add turf in "Pocket Parks")					



## OFF STREET PARKING AREAS

TREES		SHRUBS		GROUNDCOVER	
<b><u>BOTANICAL NAME</u></b> <i>Brachychiton populneus</i> <i>Ficus nitida</i> <i>Fraxinus velutina</i> <i>Jacaranda mimosifolia</i> <i>Platanus wrightii</i> <i>Olea europaea</i> <i>Washingtonia species</i>	<b><u>COMMON NAME</u></b> Bottle tree Indian laurel fig Arizona ash Jacaranda Arizona sycamore 'Swan Hill' olive Fan palm	<b><u>BOTANICAL NAME</u></b> <i>Caesalpine pulcherrima</i> <i>Euonymus japonica</i> <i>Euryops virides</i> <i>Lantana camara</i> <i>Nerium oleander</i> <i>Photinia fraseri</i> <i>Pittosporum tobira 'variegata'</i> <i>Simmondsia chinensis</i> <i>Thevetia peruviana</i> <i>Xylosma congestum</i>	<b><u>COMMON NAME</u></b> Mexican bird of paradise Euonymus Euryops 'Radiation' lantana 'Petite Salmon' or Petite Pink Fraser photinia Variegated tobira Jojoba Yellow oleander Shiny xylosma	<b><u>BOTANICAL NAME</u></b> <i>Acacia redolens 'prostrata'</i> <i>Baccharis poluaris</i> <i>Lantana swllowiana</i> <i>Santolina species</i> <i>Trachelospermum jasminoides</i>	<b><u>COMMON NAME</u></b> Trailing acacia 'Centennial' Trailing lantana Lavender cotton Star jasmine
<b>VINES</b>					
<b><u>BOTANICAL NAME</u></b> <i>Bougainvillea spectabilis</i> <i>Ficus pumila</i> <i>Lonicera</i> <i>Rosa banksiae</i>	<b><u>COMMON NAME</u></b> Various Creeping fig (north or east exposure) Honeysuckle Land Bank's rose				



## COMMERCIAL/OFFICE PLANTING

TREES		SHRUBS		GROUNDCOVER	
<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>
<i>Albizia julibrissin</i>	Silk tree	<i>Bougainvillea spectabilis</i>	Various	<i>Acacia redolens</i>	Trailing acacia
<i>Arecastrum romanzoffianum</i>	Queen palm	<i>Caesalpinea pulcherrima</i>	Mexican bird of paradise	<i>Lantana sellowiana</i>	Trailing lantana
<i>Brachychiton populneus</i>	Bottle tree	<i>Chamaerops humilis</i>	Mediterranean fan palm	<i>Lonicera japonica</i>	Hall's honeysuckle
<i>Brahea armata</i>	Mexican blue palm	<i>Dondonaea viscosa</i>	Hopseed bush	<i>Trachelospermum jasminoides</i>	Star jasmine
<i>Fraxinus velutina</i>	Arizona ash	<i>Hemerocallis</i>	Daylily	<i>Tifgreen or 'Santa Ana'</i>	Hybrid bermuda
<i>Jacaranda mimosifolia</i>	Jacaranda	<i>Lantana camara</i>	'Radiation' lantana		Decomposed Granite ½" minus golden



## 9. Hardscape Elements

Hardscape should be used in coordination with architecture and landscaping to provide a link between the street edge and individual developments. Attention to hardscape details can create visual unity by relating different developments to a unifying theme. In addition, proper hardscaping can improve pedestrian safety and movement, and the visual enjoyment of public areas.

Components addressed in this section include curbing, enriched paving treatments, walls and fences, street furniture (i.e., benches, bollards, etc.), and public amenities such as water features and sculpture.

### CURBING AND CURB CUTS

The improvement plans for Highway 111 may allow for the consolidation and reduction in curb cuts on the abutting property frontages. Curb cuts along Highway 111 have happened on a parcel-by-parcel basis creating so many points of conflict that travel on the outside lane of the highway adjacent to the curb is occasionally abruptly halted due to right turn traffic. It is important to note that accessibility is critical to business success, and several measures could improve overall circulation.

- consolidation of access points;
- radius cut driveways to allow for better acceleration and deceleration into and out of traffic lanes;
- driveways designed to make a smooth transition into parking areas; and
- direct through access from parcel to parcel.

Circulation patterns routing vehicles off Highway 111 and connecting the driveways and parking areas of multiple parcels is encouraged. Establishing off-street circulation systems will further reduce traffic conflicts on Highway 111.

### Specifications

6. Curb cuts shall be set back a minimum of 250 feet from the center-lines of major arterials at intersections to avoid conflict with vehicular “stacking” areas or acceleration/deceleration lanes.
7. Curb cuts shall not occur closer than 200 feet from each other, to provide uninhibited acceleration and

deceleration distances, and to minimize the number of conflict points.

8. Curb cuts and driveway aisles shall be shared at property lines between parcels whenever possible.
9. Access to “land locked” parcels (though such parcels are discouraged) shall be accommodated and planned for, with routes to connect through properties. No project shall preclude an adjacent property from potential access to a major street.
10. Although contrary to existing state standards, traffic flows may be improved if curb cuts were redesigned into radius cut driveway entries instead of “dust pan” style driveway entries.

### Rolled curbs

Rolled curbs shall be used in lieu of standard vertical curbing on local roads where integration of roadbed and parkway and a heightened level of pedestrian activity is to be encouraged.

11. The integration of rolled curbs with special paving materials to enhance the “courtyard” ambience of parking “plazas” is encouraged.

### PAVING

Patterned pavement and special paving materials are encouraged in limited areas to emphasize entry, pedestrian activity and places of special interest.

Large areas of unbroken asphalt shall be minimized by interspersing with alternate paving materials used as edges, walks and entry treatments.

The places appropriate for such emphasis are as follows:

- Community and project entries
- Major intersections
- Plazas
- Atriums and courtyards
- Residential entries and driveways
- Cul-de-sac courts
- Pedestrian spaces of any kind.

Acceptable alternative paving materials include:

- Integrated color cement (earth tones, salt finish)
- Integral color “Bomanite” or stamped concrete
- Exposed aggregate concrete
- Decomposed granite
- Brick
- Native stone
- Pre-cast pavers
- Approved combinations of above.

The maximum number of paving materials to be used in any one place shall be limited to three unless approved by the Community Development Director.

Pavement patterns shall be approved by the Community Development Director.

Utility wire lines and pipelines shall not be installed under such special materials if at all possible.

### WALLS AND FENCES

Walls and fences will help set a theme for the Specific Plan area. Where such elements face public streets and view corridors, or are constructed around public facilities, they shall appear consistent in material, style and height and act as a unifying element throughout the community.

### Solid Walls

Solid walls along major public roads, including Highway 111, shall stand six (6) feet in height, and must be planted with vines or screened with other plant materials to reduce their visual impact. This will contribute to the shade and pedestrian environment to be created by overstory trees along streets.

Wall materials shall consist of simple masonry construction finished with a light colored stucco, or mortar washed brick, as set forth hereafter.

### Building Materials

- Cement stucco over cement or adobe block



- Wrought iron
- Mortar washed brick
- Native stone or rock (accent only; less than 2 feet high; color and type to be consistent throughout plan area.)
- Approved combinations of above
- Ceramic tile banding is permitted as a design accent.

#### Colors

Predominant wall colors may be chosen from the following list of desert pastel colors:

- Beige
- Pale Peach
- Tan
- White
- Sand
- Off-white

Colors which are discouraged include yellow, blue, green, pink or violet.

Accent colors may be used in a 4 inch wide (max.) accent strip recessed in walls. Accent colors are to be used in moderation and are not to be used in combination unless approved by the Community Development Director:

- Red Clay
- Slate Blue
- Mint Green
- Aquamarine
- Jade
- Cobalt Blue
- Burnt Sienna
- Ancient Gold

Other accent colors may be permissible with the demonstration of compatibility with wall colors proposed and with adjacent uses.

#### Specifications

- Pilasters at least 16" square block with consistent stucco finish and cap shall occur at each corner, and at each property line where it intersects the wall.
- The horizontal mass of continuous walls shall be softened by landscape planting and vines.
- All walls shall be designed as an integral part of the overall architectural and site design, being constructed with materials that are complementary to the style of adjacent buildings and incorporating the same finishes and colors.
- Construction materials of walls built to screen ancillary structures adjacent to buildings, such as transformer or trash enclosures, should be consistent with main structures and other walls.
- Solid walls to screen views from the street of parking lots shall not exceed three (3) feet in height.

#### Retaining walls

Retaining walls shall be consistent in style, material and color with adjacent building or "theme" walls to which they most closely relate.

- Walls shall be permitted to elevate planting beds adjacent to buildings or to enhance bermed landscape effects, but shall not exceed a height of three (3) feet in building setback areas.
- Walls retaining more than three feet of soil are allowed only in special conditions. Such walls shall be designed by a Registered Professional Engineer, subject also to approval by the Community Development Director.

#### View Fences

View fences shall be permitted to break the monotony of the walls where appropriate. This type of fencing shall be used with discretion to avoid inappropriate views from and to public spaces into more private areas.

- Masonry pilasters (similar in vocabulary to theme walls) should be provided every ten (10) feet.
- Wrought iron pickets shall be a minimum of one half (1/2) or three quarters (3/4) inch thick on four (4) inch centers.

- Fencing shall be painted white or a color to complement the color scheme of adjacent architecture and to decrease fence visibility.
- The recommended height of view fences is between five (5) and seven (7) feet.

#### Interior Walls and Fencing

Interior walls or fencing between parcels shall be used only where absolutely necessary and shall complement adjacent architecture. The use of berming to visually and physically separate properties is preferred.

- Freestanding walls located between buildings and fronting streets are discouraged.
- Walls on lot lines that prevent integration of and linkages between different developments shall not be permitted.
- Design of interior fences and walls not visible from major public rights of way shall be left to the discretion of the individual developer, subject to review by the Community Development Director.
- No internal wall or fence shall exceed six (6) feet in height.
- No chain link fencing will be allowed within the project area except for the fencing of tennis courts which shall provide the exception to the six (6) foot height limitation. Landscape hedges and other screening measures should be used to reduce views of such fences, but no metal slats are allowed.

#### Access Gates

Access gates penetrating walls and fences shall be of substantial but compatible design and preferably of metal construction.

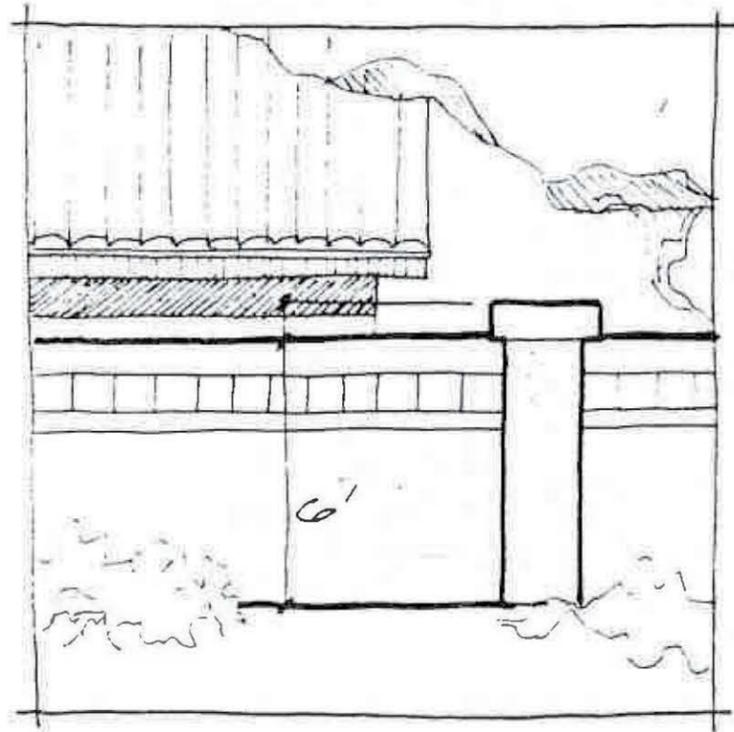
- Gates over ten (10) feet total width are subject to approval by the Community Development Director.
- All such gates shall be painted or stained to complement the architectural theme.
- Wood fences and gates may be used in internal site areas not visible from public streets.



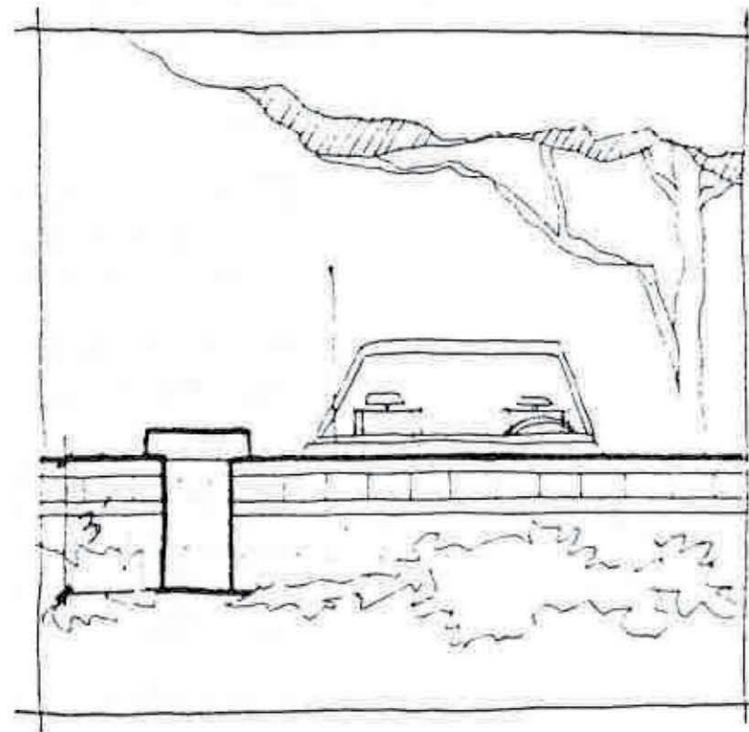
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**ILLUSTRATION 05**

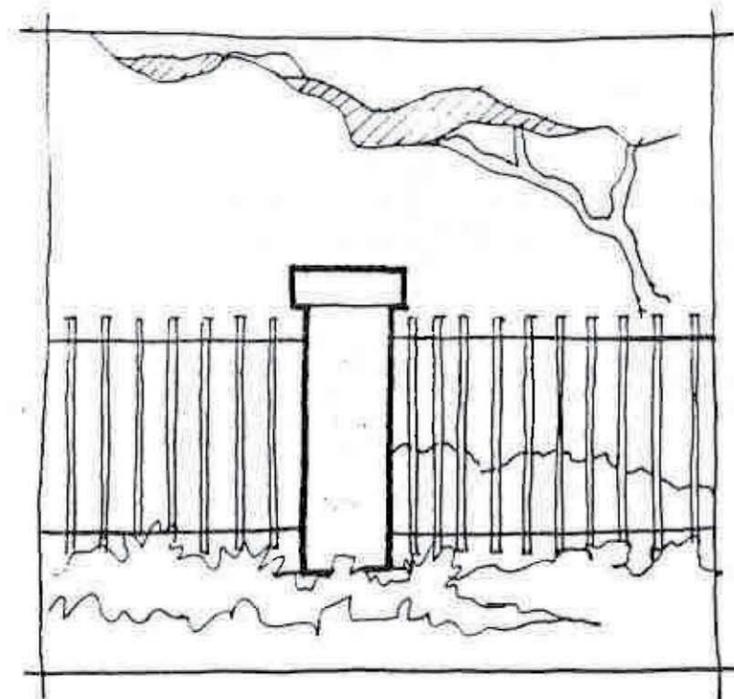
Fences and Walls



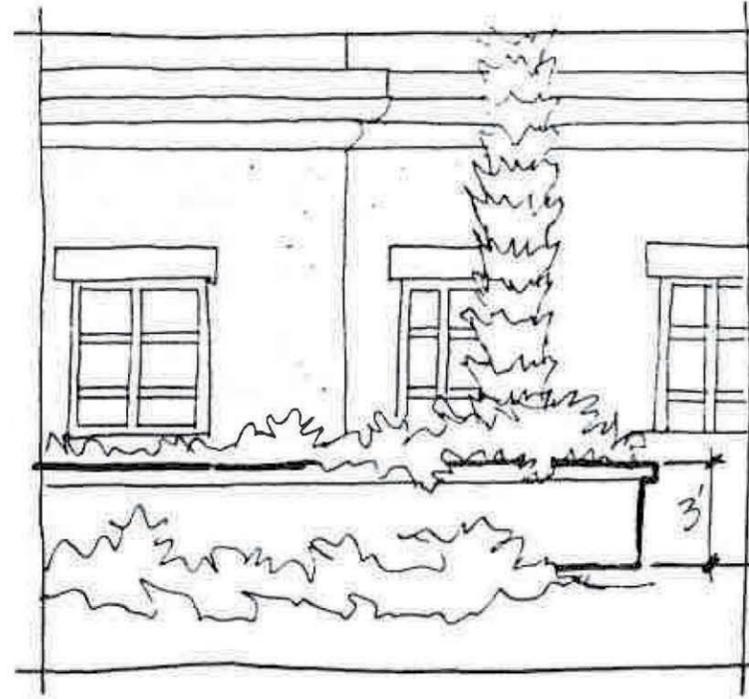
Screen wall.



Parking wall.



View fence.



Retaining wall planter.



## STREET FURNITURE

Street furnishings serve aesthetic as well as functional purposes. Elements such as mailboxes, benches, bus shelters and trash receptacles create opportunities to reinforce the overall design theme.

### General Provisions

- Street furnishings should be simple and functional in design and should consider the security, safety, comfort and convenience of the user, including the handicapped.
- The form, color and scale of street furniture should be compatible with surroundings and individual components shall be considered as parts of a total design framework.
- Street furniture shall not obstruct stairs, ramps, building entrances or exits, or loading areas.
- To the greatest extent possible street furniture should be incorporated in landscaped spaces and off street amenity areas.
- Street furniture should be constructed of durable, vandal-resistant materials capable of withstanding the climatic conditions of the desert.

### Mailboxes

- Multi-tenant developments shall have clustered mailboxes, the basic design of which shall conform to USPS regulations.
- Communal mailbox structures shall be located near major entries or amenity areas.
- Mailboxes shall be incorporated in a wall or pilaster or designed as a freestanding kiosk integrated with adjacent architectural themes. Exposed mailboxes are discouraged.

## Trash Receptacles

Freestanding refuse containers in retail or public areas shall be constructed of cast concrete, ceramic or wrought iron, with an inset for a trash can. The containers shall be natural in color(s) and/or of a design commensurate with surrounding architectural themes.

## Bollards

Bollards may be used to physically separate pedestrians and vehicles in high traffic areas as to protect street furnishings or other elements of the streetscape.

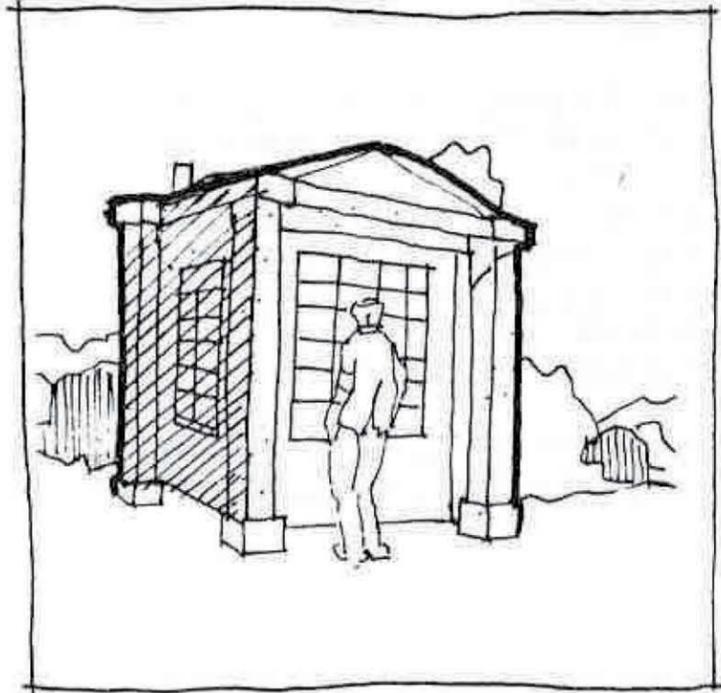
- Any bollards used adjacent to public streets should meet placement and design conventions of regulatory agencies.
- The design of the bollards must be consistent on each site or on two or more sites if visually connected.
- Bollards should be constructed of concrete, mortar washed brick or masonry with a stucco finish (or a combination thereof) and color shall integrate the bollard with its surroundings while not compromising its safety function.
- Proportions should be heavy or massive, suitable for the material used, rather than tall and thin. Height should be adjusted to a level always visible to automobile drivers at the closest distance they will approach the bollards.
- Height of bollards shall range from 18 inches minimum to 3 feet maximum, unless incorporating lighting in which case 4 feet shall be the maximum height.



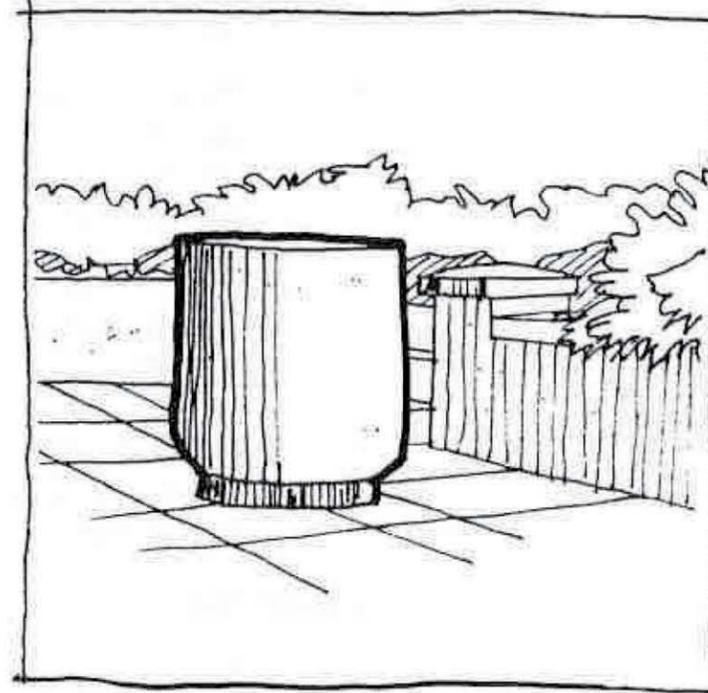
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**ILLUSTRATION 06**

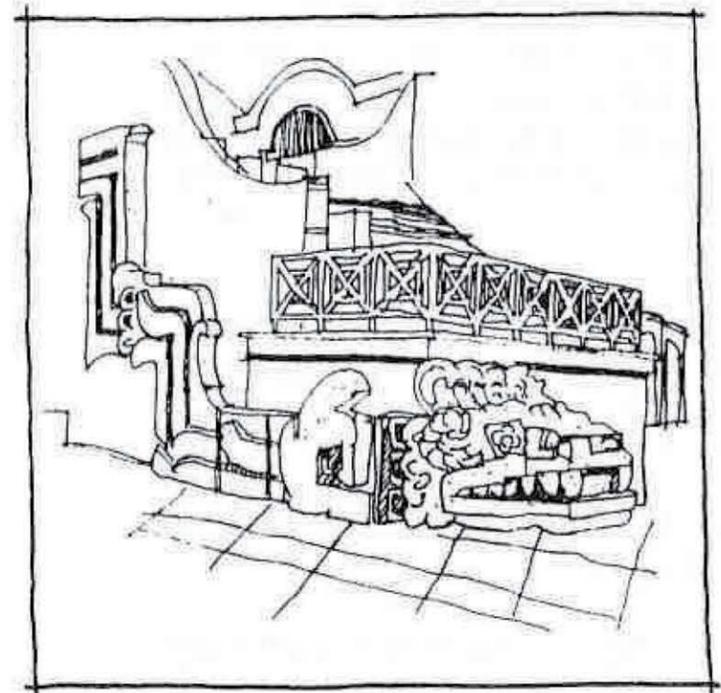
Street Furniture 1



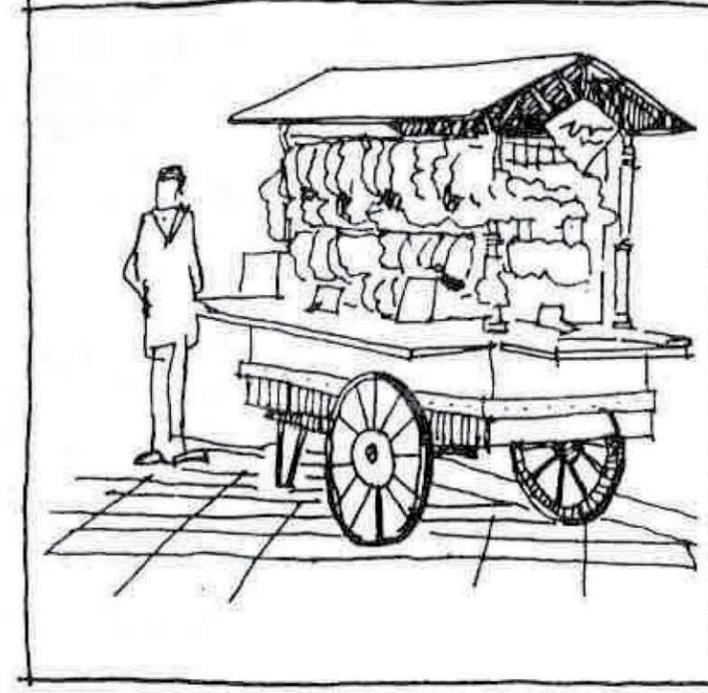
Kiosk mailbox structure.



Ceramic trash enclosure with drop-in trash can.



Pre-cast concrete sculpture integrated into a wall creates interest.



"Push cart" vendor.



**Newspaper Stands and Food Vendors**

- Plazas, courtyards, entrance spaces and bus shelters should be designed to integrate newspaper stands in an aesthetically pleasing manner using materials and styles that complement the architecture. Stands should be built in where possible. Free-standing newspaper stands shall be surrounded on three sides by a solid wall consistent in design with the architecture of the nearest building.
- Vendors should be limited to “push carts” selling food and drink, and “canteen” services at lunch time by permit only.

**Bus Shelters**

- Bus shelters shall be installed in areas which generate the highest passenger volume. Locations shall be coordinated with the City Traffic Engineer and the Transit District.
- Bus shelters shall contain no advertising or signage except for identification purposes.
- Bus shelters shall be constructed using materials and styles as discussed in the architectural guidelines.
- Design of shelters shall protect the user from adverse weather conditions, provide a place to sit, a light and trash receptacle. Optional equipment encouraged includes a telephone, drinking fountain and newspaper stand.

**Benches**

- Benches encourage pedestrian use by providing a comfortable place to rest or wait. The most common places they will be used in the streetscape is a bus stop, either with or without a shelter, and in plazas and malls. However, their frequent use throughout the community is encouraged.
- The benches should be sited in such a way that they do not interfere with pedestrian traffic and have ample shade in the summer.
- Benches may be made from masonry and stucco, wrought iron, wood or combinations thereof approved by the Community Development Director.

**Water Features and Sculpture**

Water features, such as fountains or ponds, and sculpture can create visual excitement at entries and in plazas and courtyards and are encouraged as elements of the outdoor landscape. They should be encouraged as focal points in appropriate locations.

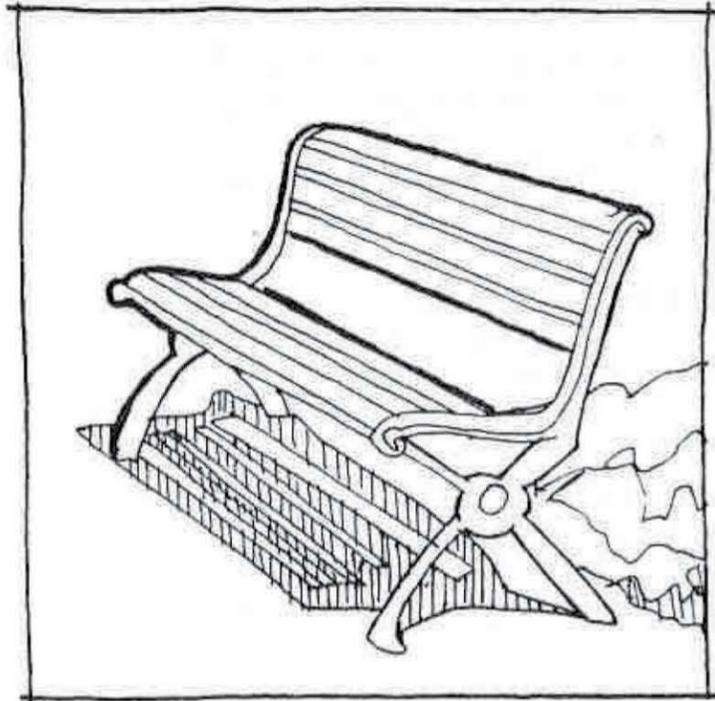
- Fountains or water features should be located within development parcels near building entries, courtyards, or sitting areas along pedestrian walkways.
- All associated mechanical equipment shall be concealed from view from adjoining properties and roadways.
- No colored, tinted or otherwise unnatural variation on water translucency is allowed.
- Sculpture should be permanent in character with finishes using durable materials and colors. The use of stone, ceramics, or metal is encouraged.



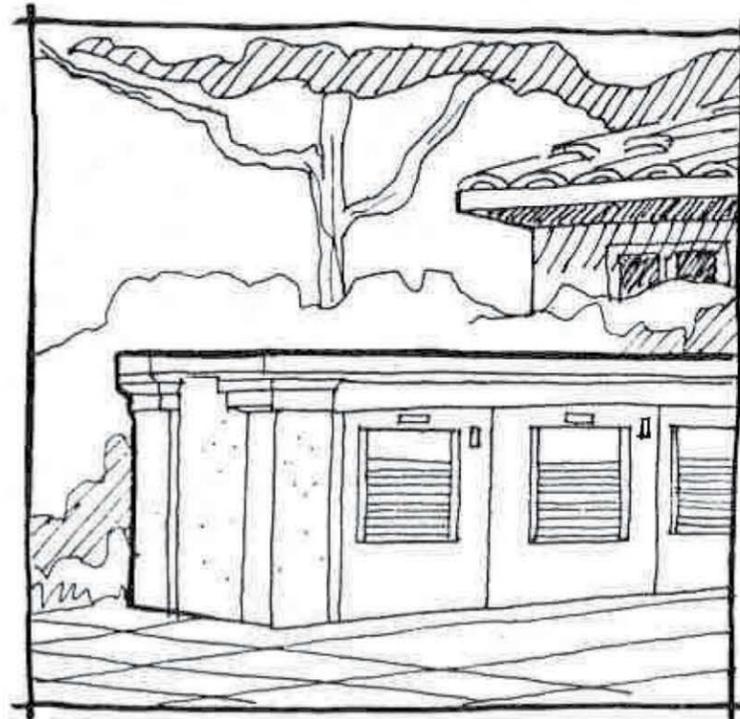
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## ILLUSTRATION 07

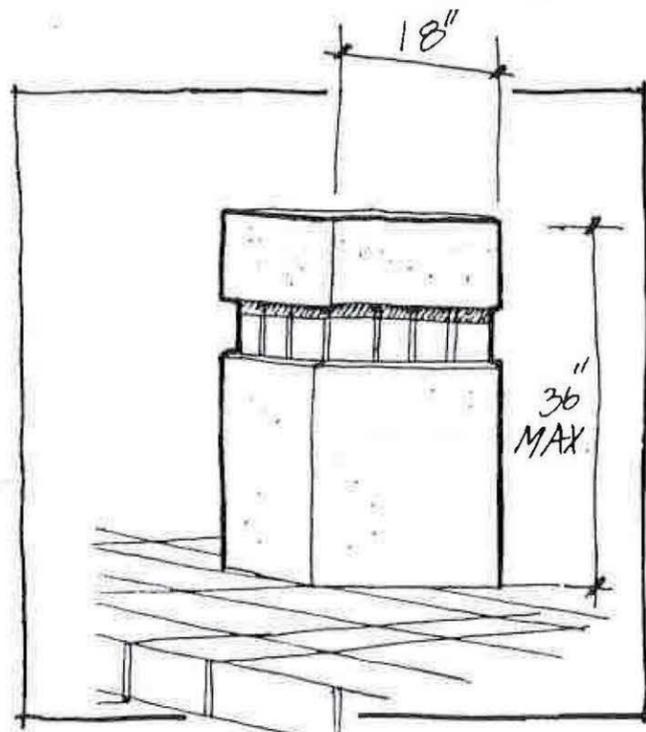
Street Furniture 2



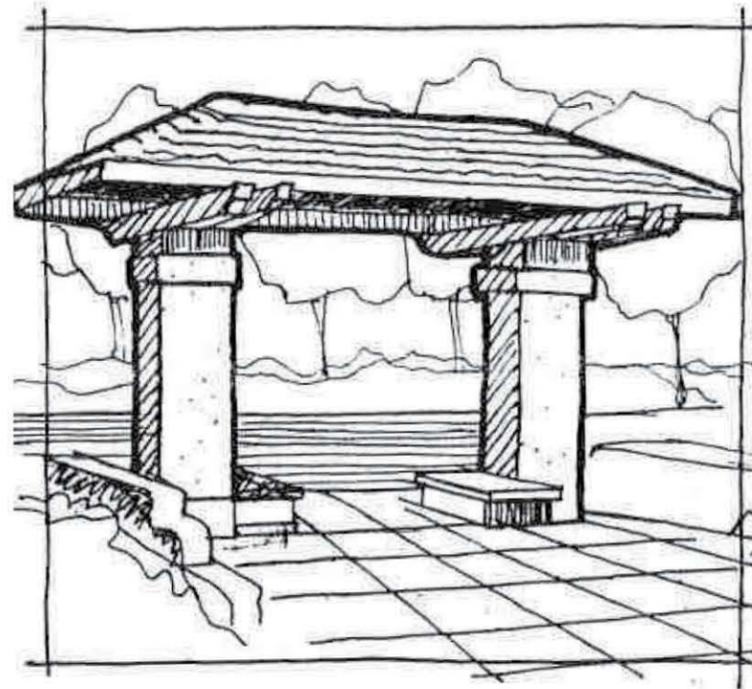
Benches shall be constructed of wrought iron or stucco masonry.



Newspaper vending stands shall reflect the architecture of surrounding buildings.



Bollards separate pedestrians and vehicles. Lighting may be incorporated.



Locate bus shelters in areas that have the highest passenger volume.



## 10. Sign Guidelines

All signs within the Highway 111 East Specific Plan Area will be designed to provide consistent reinforcement of a desired Rancho Mirage “character”. Signs must communicate only specific information but shall, in themselves, add to the attractiveness and vitality of the area.

Signs may provide an address, identify a place of business, a building complex or multi-use area, locate tenants, reinforce “district” themes or generally provide directions and information. Signage shall also provide for public safety and ease of circulation. Regardless of function, signs shall be architecturally compatible and contribute to the character of the area. Signs shall not compete with each other or dominate the setting via inconsistent height, size, shape, number, color, lighting or movement.

All signs in and for projects within the Specific Plan Area shall conform to the guidelines set forth herein. If a condition is not addressed in this Specific Plan then the provisions of the City of Rancho Mirage Sign Standards, Chapter 17.22 of the Zoning Ordinance, shall prevail. If there is a conflict the more restrictive shall prevail.

Each development represents only a small portion of the project as a whole but contributes significantly to the overall visual image of the community. The uniform application of these signage guidelines, addressing size, location, type and quality of both temporary and permanent signs, will provide the basis for the integrated visual character envisioned for Rancho Mirage.

### GENERAL PROVISIONS

12. The design of identification and directional signage shall require approval by the City and must be in accordance with existing sign ordinances, except as noted herein.
13. No signs unless lawfully permitted shall be placed upon or project into or above public property or right-of-way, except as noted herein.
14. Any sign which does not conform to the provisions contained herein shall be made to conform or shall be removed per the provisions of the Zoning Ordinance.
15. Application for sign approval shall be accompanied by sketches and drawings to scale and dimensions

showing details of construction, and shall delineate the size, shape, design, material, coloring, lettering, lighting and position in relationship to the building form or place where it will be displayed. Scaled sketches of existing signs on the premises, including signs for which valid permits exist, whether or not such signs are in existence, shall accompany the application.

16. A Master Identification Sign Program is required for additions to existing buildings or for the development of multi-tenant buildings or parcels and shall be approved prior to the issuance of building permits for new construction or redevelopment.
17. Individual buildings shall have their addresses displayed in numbers at least six (6) inches but not more than eighteen (18) inches in height in clear view of the street the building is facing.
18. Public signs shall be limited to those absolutely necessary for:
  - a. Good traffic flow
  - b. Safety
  - c. Public information
19. It is the responsibility of the land owner to remove any sign abandoned or not in use for a period in excess of ninety (90) days.
20. Clear sight triangles shall be observed and enforced at all intersections, corners, parking areas and drives at all times.
21. Billboards and pole signs are not allowed.

### DESIGN, CONSTRUCTION AND MAINTENANCE

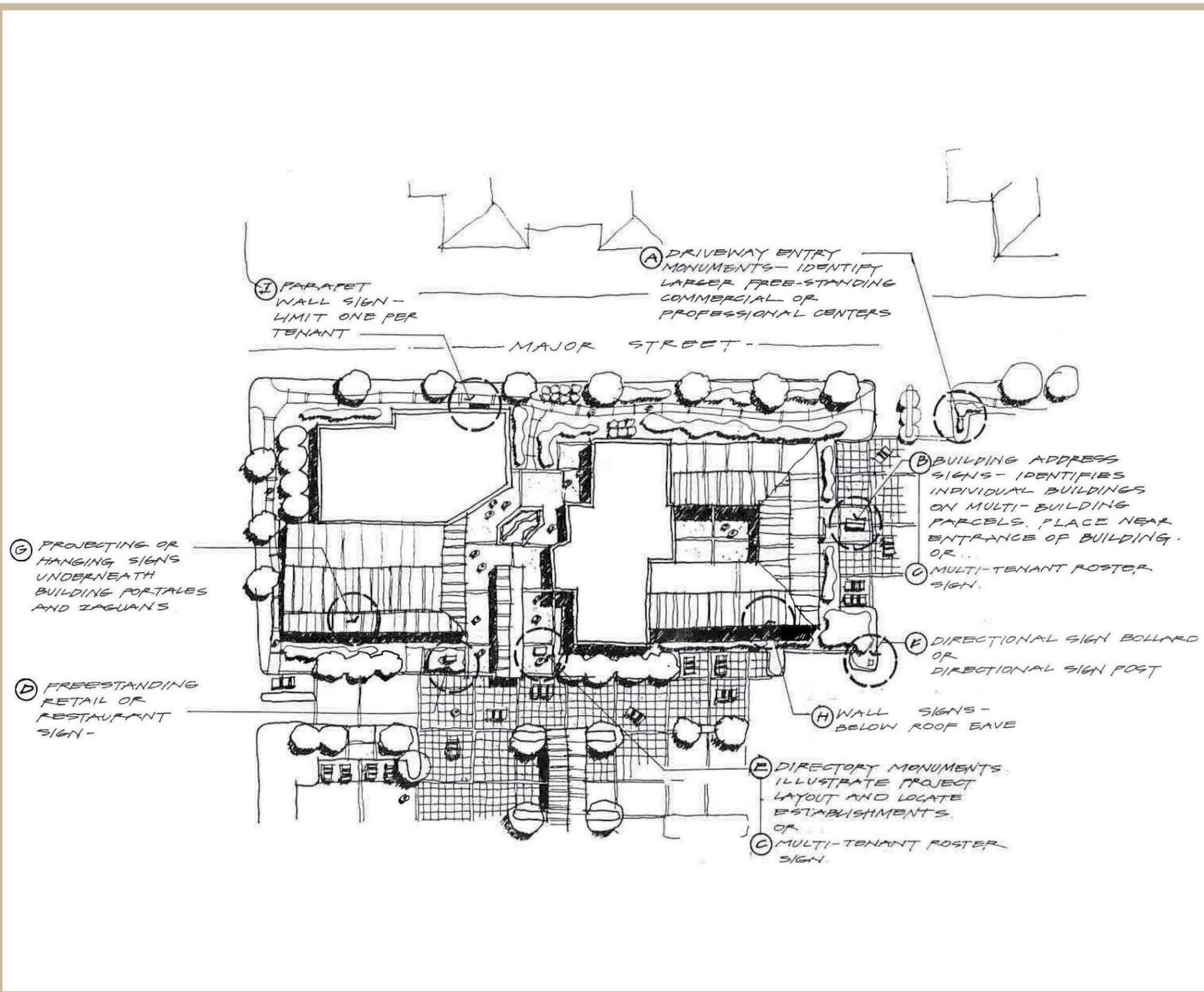
22. All signage shall be maintained in good repair, including the display surface, which shall be kept neatly painted or finished.
23. The exposed backs of all signs visible to the public shall be suitably finished and maintained.
24. All signage shall be designed free of bracing, angle-iron, guy wires, cables or similar devices, except for commercial advertising signs as hereafter noted.
25. Painted signs shall use fade-resistant, durable, exterior finish paint. No luminous, fluorescent or iridescent paints or plastics are permitted. Permanent wooden signs or materials that are degraded by the harsh desert climate shall be avoided unless adequately sheltered.
26. An effort shall be made to achieve consistency between building style and sign design. In all cases, signage shall be complimentary to the exterior treatment of the building or location involved.
27. Color schemes for signage shall relate to other signs, graphics and color schemes in the vicinity, in order to achieve an overall sense of identity.
28. Signs may be lighted; however, no light that flashes or blinks or affects changes in hue or intensity of illumination is permitted. Illumination sources for any sign shall be hidden from view.



CITY OF RANCHO MIRAGE

## EXHIBIT 22

Conceptual Sign Location Plan





## GROUND SIGNS

### Driveway Entry Monuments

Entry monuments shall identify entries to the theme restaurant, commercial and professional centers within the Specific Plan area. They shall include clearly displayed address(s) and name(s) for the center.

These monument signs are intended for larger commercial and office uses of free-standing or attached design. Individual uses on very small parcels may install project entry monuments of a size relative to its street frontage.

One sign per project or one sign per major user is permitted in the parkway area, with never more than two such signs per parcel.

Entry monuments shall reinforce the “district” design theme. Logo, typeface and color schemes shall be consistent throughout the area being identified, and compatible throughout the project.

Design of such monumentation shall be coordinated with street landscaping.

Entry monuments shall be illuminated externally, with glare carefully controlled and lighting sources concealed.

### Specifications

- Signs shall be constructed on a base of stucco over concrete block, of mortar washed brick or of formed concrete to match the architecture on the parcels they identify.
- Combined sign and base shall be a maximum of three (3) feet in height and ten (10) feet in length. Monument shall be integrated with berming and landscape materials with established parkway design themes.
- Concealed lighting flush with grade shall be used to illuminate signs. Internally illuminated or “canned” signs near streets are not allowed.

### Building Address Signs

The following guidelines are appropriate for a professional office complex or business park and should be applied only under such circumstances.

- This sign will identify individual buildings on multi-building parcels at the front door area.
- The design of the sign shall be integrated with the surrounding architecture and landscaping.
- A stuccoed block or mortar washed base with tile accent with concrete cap and recessed letters similar in design to the directional ground sign or bollard is recommended.
- This sign shall be limited to a maximum of four (4) feet in height by five (5) feet in length by one foot (12”) wide.
- Eight (8) inch steel channel letters, bronze or gold in color, or embossed lettering in the flat concrete cap is acceptable.
- Concealed lighting sources located flush with grade are recommended.

### Multi-Tenant Roster Signs

Two types of multi-tenant roster signs shall be allowed. These signs shall be located in plazas or along walkways that lead to multi-tenant buildings. Only one sign per multi-tenant building shall be allowed. Signage may occur on backs of signs.

### Vertical Format Signs

29. Maximum dimensions of these signs shall be eight (8) feet high by six (6) feet wide by eight (8) inches wide.
30. The sign shall be constructed of stuccoed block or metal with tile or metal plate copy area that shall not be larger than three (3) feet by six (6) feet or the project name and one (1) foot by six (6) feet for individual establishments.
31. There shall be a minimum of three (3) establishment panels and a maximum of twelve (12) establishment panels.
32. Lettering on the panels shall be painted, tile, or raised steel channel letters.
33. Lighting sources shall be concealed by landscape plantings or flush with grade.

### Horizontal Format Signs

34. Materials shall be same as vertical format signs.
35. Signs shall be a maximum of four (4) feet high by eight (8) feet wide by eight (8) inches thick.
36. The project identification panel shall be four (4) feet by one (1) foot (max.) and individual tenant panels shall have maximum dimensions of two (2) feet wide by one (1) foot high.
37. There shall be a minimum of ten (10) tenant panels and a maximum of fifteen (15) tenant panels for these signs to occur.
38. Lighting shall be concealed.

### Freestanding Retail or Restaurant Sign

Only one sign shall be allowed per building. These signs shall be located at entry points from parking plazas adjacent to walkways in planting beds.

39. Signs shall be constructed of stuccoed masonry block with maximum dimensions of four (4) feet in height by ten (10) feet in length by sixteen (16) inches in thickness.
40. Lettering shall have a maximum height of twelve (12) inches by seven (7) feet in length.
41. Letters shall be channel steel, bronze or copper.
42. A twelve (12) inch high street address of the same material as the letters shall also be allowed.
43. Lighting shall be concealed by plants or flush with grade.

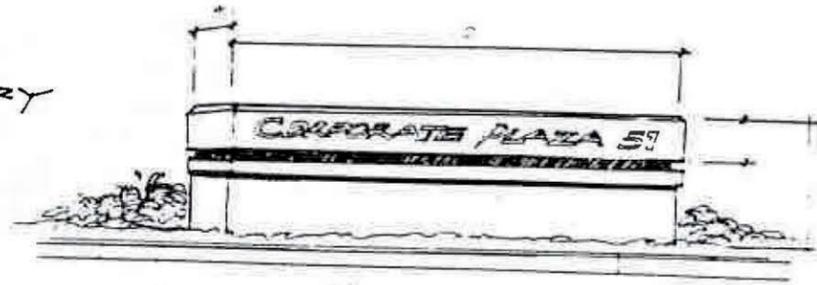


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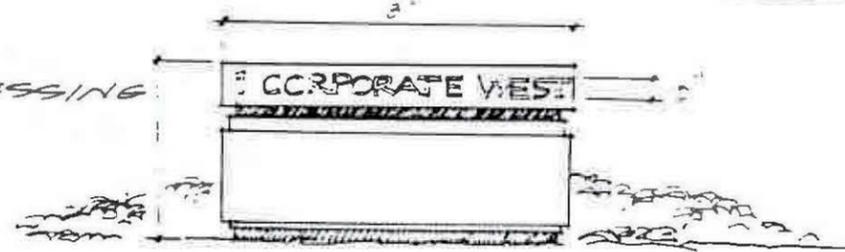
### ILLUSTRATION 08

Ground Signs

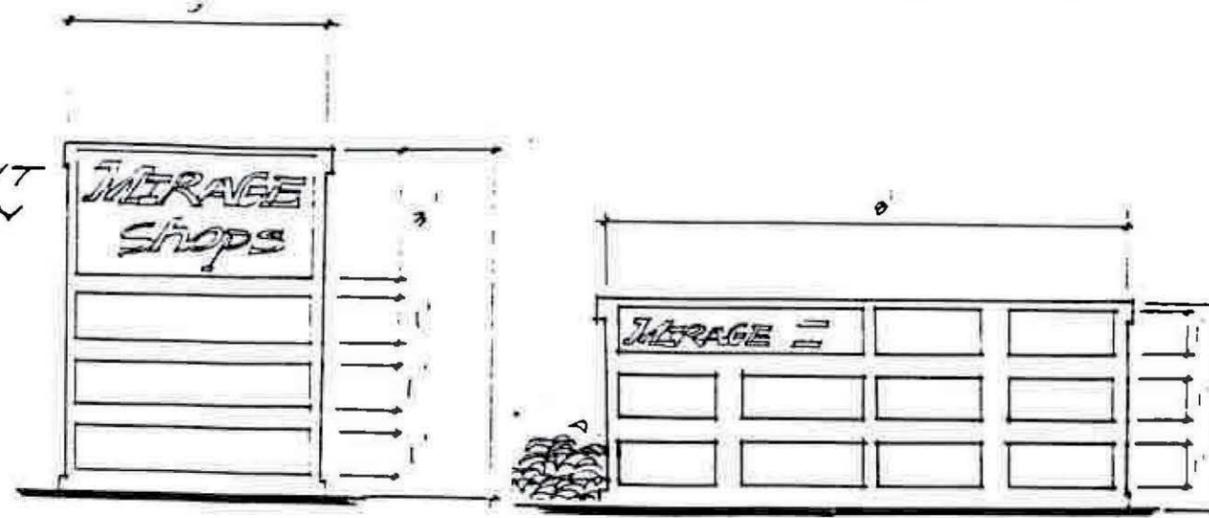
(A) DRIVEWAY ENTRY MONUMENTS



(B) BUILDING ADDRESSING SIGN



(C) MULTI-TENANT ROSTER SIGN



(D) FREESTANDING RETAIL OR RESTAURANT SIGN



(F) DIRECTIONAL SIGN BOLLARD OR SIGN-POST





### Directional Signs

Directional signage, to identify and direct vehicular and pedestrian traffic to various on-site destinations, shall be provided along roadways and within all multi-parcel and multi-tenant developments.

Directional signage shall be of consistent design throughout the project. All such signage shall be fabricated from the same materials, with a consistent color palette and common graphic theme. The use of materials compatible with the architectural design of each project and its corresponding area is encouraged.

Directional signs shall be small scale and of consistent dimensions, and located in a visually logical order.

These signs will provide on-site directional information and not be used for advertising. They shall include the following:

- Directory monuments
- Public service signs
- Traffic safety signs
- Residential directories

### Directory Monuments

Directory monuments are permitted as ground signs to illustrate the project layout and list and locate the establishments therein. The sign area of such location markers shall not exceed four (4) feet in height or four (4) feet in width. The location and number of these markers shall be determined by the Community Development Director.

### Public Service Signs

Service signs shall only provide general public information to direct the way to such public places or facilities as information centers, food, restrooms, telephones, emergency stations, plazas, village commercial area, etc. Service signs shall be permitted as wall or ground signs and will be limited in number and location as outlined below.

### Traffic Safety

Traffic safety signs will be subject to the standards of the City, State and Federal highway agencies.

### Residential Directories

Directional signs may reference residence and establishment names, occupants' names, and address numbers. Such signs shall be panelized and each shall meet the panel size requirements set forth below.

### Directional & Service Sign Specifications

45. Signs are permitted on the faces of buildings or structures provided that such signs shall not be placed higher than five (5) feet above the immediately adjacent ground level or above the eave line, whichever is the lesser.
46. Signs setting forth the location of or directions to parking or buildings located on the premises, or regulating the flow of on-premise traffic are permitted to locate on a separate free-standing ground structure (bollard) provided that the structure and/or sign affixed to same does not exceed four (4) feet in height and twelve (12) inches on a side.
47. Such signs may include pictorial and decorative designs. The signs may include the minimum words and numbers necessary to accurately communicate the required information. The signs shall be designed to be compatible with their immediate built environment.
48. Maximum size for a single sign face or panel shall not exceed a maximum of nine (9) square feet.
49. Total number of signs to be used for this purpose shall not exceed four (4) per establishment and shall be combined when possible.

### Street Signs

All street signs shall be consistent with the City of Rancho Mirage Street Sign Program.

### TENANT IDENTIFICATION SIGNS

#### Parapet and Wall Signs

These guidelines apply to office, hotel and other single tenant uses.

- Parapet signs will identify the major tenant within the building. Any corporate logo must be included within the message area calculation.

- Signage shall be located on building walls underneath eaves or roughly two (2) feet below the cornice line.
- Only one (1) sign per building is allowed, unless the building has two street frontages. Major streets include Highway 111 and the minor arterial in District 7. In this case two (2) building parapet/wall signs identifying the major tenant are allowed, and never more than two (2) signs shall be allowed per building. They shall never be located next to each other at the corner.
- Sign text and logo area is limited to fifteen (15) feet in length and eighteen (18) inches in height for the first floor, with six (6) inches of additional height allowed for a second floor.
- Back-lit steel channel letters with a polished bronze or gold color finish or individually illuminated letters and logo are recommended.
- For major tenants in large buildings, signs of high quality fiberglass or other synthetic material with interior illumination may be appropriate.

These guidelines apply to multi-tenant retail, commercial, restaurant, and similar uses.

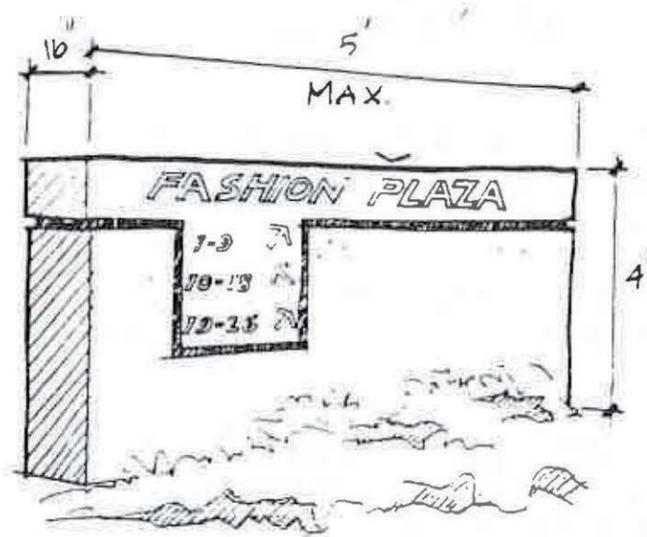
- Signs shall be placed underneath roofs on building walls.
- Individual tenant identification shall be limited to one wall or parapet sign per tenant.
- Sign text and logo area is limited to fifteen (15) feet in length and eighteen (18) inches in height for the first floor, with six (6) inches of additional height allowed for a second floor.
- Sand blasted or painted wood signs are encouraged. Internally illuminated individual letters integrated with the architectural style can be acceptable if carefully designed. Architecture shall not be distorted or obscured with misplaced or unusually large sign bands.
- Suggested materials include painted and/or sandblasted wood, ceramic tile, metal, carved wood, and hardboard.



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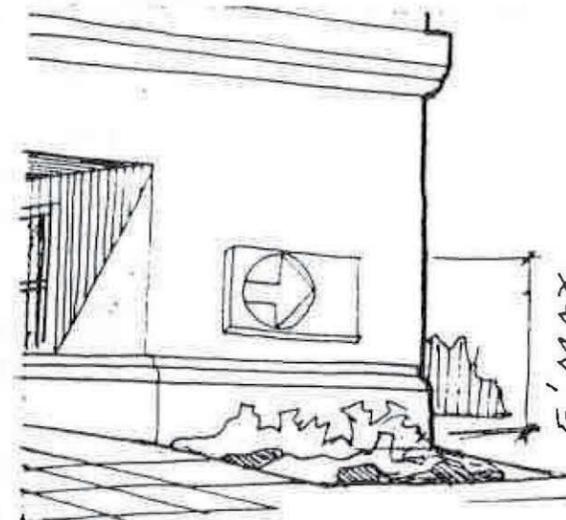
### ILLUSTRATION 09

Directional Signs

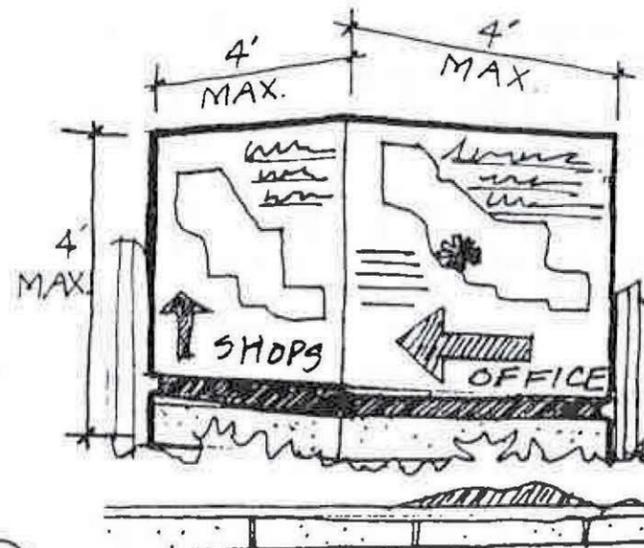


LOCATES BUSINESSES WITHIN AN INDIVIDUAL BUILDING COMPLEX.

WALL MOUNTED OR INTEGRATED INTO THE BUILDING -

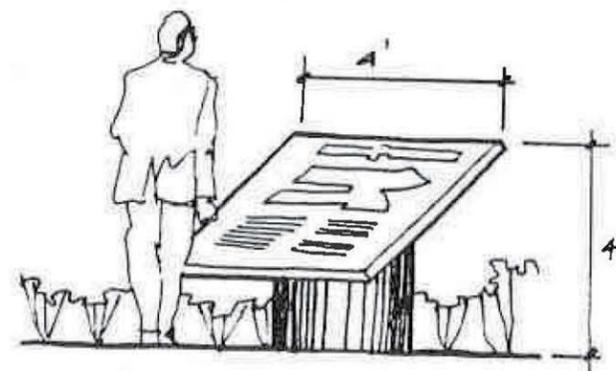


5' MAX. HEIGHT ABOVE GROUND LEVEL



(F) VEHICLE DIRECTIONAL MONUMENTS ILLUSTRATE PROJECT LAYOUT AND LOCATE ESTABLISHMENTS.

(E) PEDESTRIAN DIRECTIONAL MONUMENTS LOCATE ESTABLISHMENTS WITHIN AN INDIVIDUAL COMPLEX.

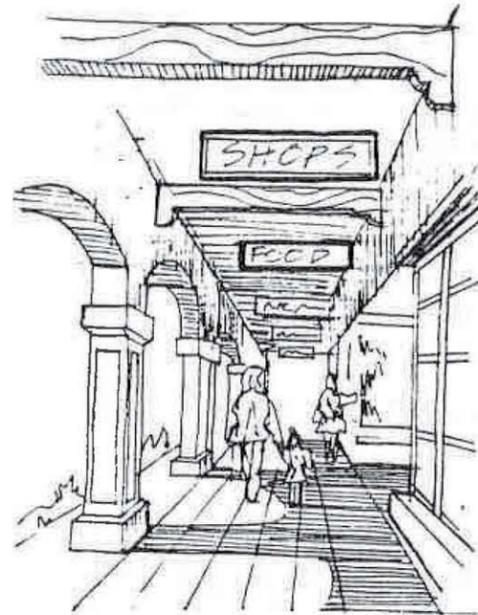




CITY OF RANCHO MIRAGE

### ILLUSTRATION 10

Identification Signs



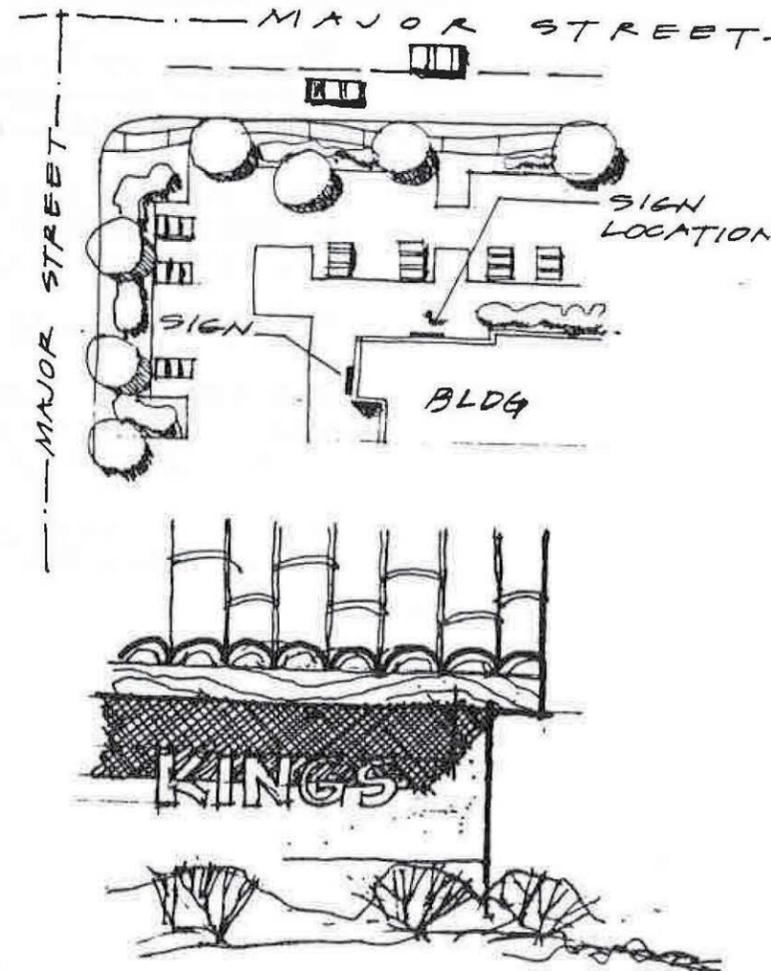
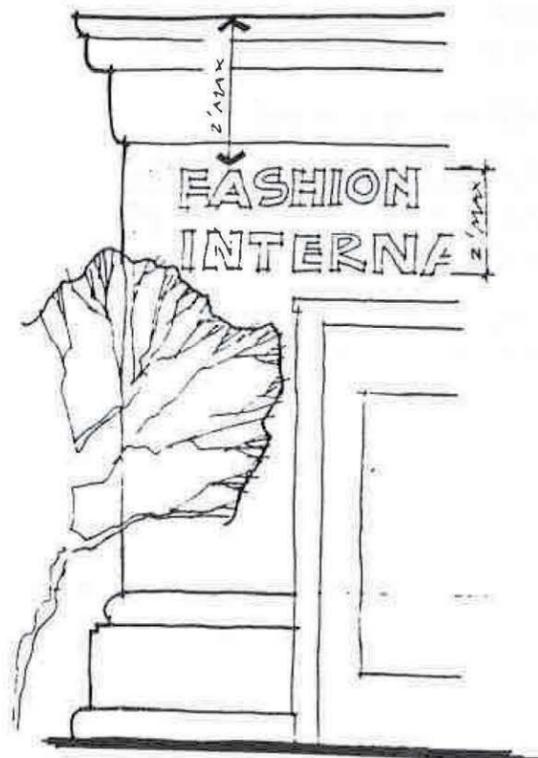
(G)

HANGING OR PROJECTING  
SIGNS BELOW PORTALES  
OR ZAGUANS.

ONLY ONE IDENTIFICATION  
SIGN PER BUILDING PER  
MAJOR STREET FRONTAGE

(I)

SIGNS TWO  
FEET BELOW THE  
TOP OF THE PARAPET



(H)

WALL SIGNS BELOW  
BUILDING EAVE





- Background colors shall be earthtones ranging from natural wood, tan and brown to rust or white.
- As a general rule, there shall be no more than 3 colors used on a sign. The sign message shall visually contrast and complement the background colors.

#### Projecting or Hanging Signs

- These signs can be used by individual tenants in multi-tenant commercial areas underneath pedestrian arcades or covered walkways.
- As an alternative to parapet or wall signs, signs may be hung underneath and parallel to pedestrian walkways, facing out towards parking areas.
- Quality wood or metal construction is required.
- Sign dimensions shall be limited to a maximum height of twelve (12) inches by a maximum length of seven (7) feet to twelve (12) feet.
- Any required sign lighting must be concealed in architecture away from pedestrian view.

#### TEMPORARY SIGNS

Signage identifying uses or activities temporary in nature, such as signage associated with real estate sales and leasing and the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area.

Temporary signage must be located on or in close proximity to the uses identified.

The multitude of signs associated with development, design, construction and leasing shall be combined into one sign for each project and the sign shall be located on site.

Applications and concise plans for temporary signage, setting forth dimensional requirements, materials of construction, mounting heights, color and graphic format shall be subject to review and approval by the Community Development Director.

#### Commercial Construction and Leasing Signs

These Signs are intended to inform the viewer of new buildings and leasing opportunities, opening dates, and builder names and telephone numbers.

- Wood construction with a painted sign surface is recommended in this application.
- One sign per individual parcel or project is allowed, and shall be located on-site ten (10) feet from the right-of-way line.
- The maximum sign dimensions shall be five (5) feet in height by four (4) feet in width.
- Sign face may include any sketch or architectural rendering of the proposed use.
- Sign removal must occur after 75% occupancy or lease out or within six months of the Certificate of Occupancy has been issued.

#### Residential Sales and Leasing Signs

- Temporary real estate sales and leasing, construction or model tour ground signs are permitted off-site provided two signs referring to the same place or product are not within one mile of each other on the same route.
- One (1) project identification monument or ground sign is permitted per major entrance to a development, provided there is a minimum of 25 dwelling units. Sign copy is limited to name and address of the development. The sign area will not exceed sixteen (16) square feet. A minimum distance of 50 feet must be maintained between signs.
- Temporary model complex signage may be erected by the project builder. All levels of signage shall conform to Specific Plan criteria.
- No advertising, real estate or subdivision signs are permitted within the Specific Plan area for projects or products outside Rancho Mirage.

#### PROHIBITED SIGNS

- **Obsolete or Abandoned Signs:** Any sign located on vacant or unoccupied property that was erected for a business which no longer exists, or any sign which pertains to a time, event or purpose which no longer exists, shall be removed within 30 days after the use has been abandoned.

- **Signs Constituting a Potential Traffic Hazard:** Or which simulates or imitates in size, color, lettering or design any traffic sign or signal.
- **Temporary Signs On Public Property:** Street, median island, parkway, sidewalks, traffic control sign posts, utility poles, park land, trees, etc.
- **Animated or Moving Signs:** Signs consisting of any moving, swinging, rotating, flashing, blinking or otherwise animated components.
- **Off-premise Signs:** Any sign, other than a simple directional sign, installed for the purpose of advertising a project, development, event, person or subject not related to the premises upon which sign is located.
- **Vehicle Signs:** Signs on or affixed to trucks, vans, automobiles, trailers or other vehicles which identify, or provide direction to a use or activity not related to its lawful making of deliveries or sales of merchandise or rendering of service from such vehicles.
- **Pole Signs, Pylon Signs:** A free-standing sign mounted on a post or pole which exceeds four (4) feet in height above grade.
- **Portable Signs:** A free-standing sign not permanently affixed, anchored, or secured to the ground or a structure on the lot it occupies, including tailored signs, except as approved by the Community Development Director.
- **Roof Signs:** Any sign erected, constructed and maintained upon or over the roof of any building, unless it is a projecting canopy sign or sign tied in architecturally to the framework of the roof.
- **Other prohibited signs:** advertising signs and billboards, inflatable signs or balloons, inflatable animals, magnetic signs, plastic flags unless otherwise specified in these guidelines or approved by the Community Development Director.



## 11. Lighting Guidelines

In the design of lighting for the planning area, careful consideration must be given to the community architectural theme as well as the safety of the site users. Traffic signal poles, pedestrian and building lighting also play a vital role in defining a quality appearance within developments. The lighting concept outlined in this chapter has been devised to provide a hierarchy of lighting effects which will contribute to the overall cohesiveness of the community image.

Public sidewalks, plazas and alleys, building facades, roofs, outer walls and fences of buildings and other construction and signs visible from any public street, place or position in the Specific Plan area shall not be illuminated by privately controlled lights or any other illumination except as described herein.

The lighting system shall be compatible with the surrounding buildings and express the unique character of the corridor. Lighting must be designed and installed so that all direct rays are confined on site and adjacent properties are protected from glare.

Uplighting of median landscaping and “bollard” lighting of pedestrian sidewalks will constitute the only street lighting for Highway 111. In this way, the landscaping character of 111 is maintained at night as well as pedestrian safety while preserving the desert night sky so valued by the Rancho Mirage Community.

On-site parking lot and building light fixtures may vary from one sub-area to the next, but illumination levels shall remain consistent and not compete with street lighting and signage. Appropriate special lighting effects that will be compatible with the theme proposed for each sub-area are encouraged.

Street lighting and parking lot lighting contribute to the safety and security of each development improving night visibility. Unique lighting fixtures may provide easy identification of the corridor for motorists. Lighting potentially visible from adjacent properties shall be subdued and incorporate cut-off shields or be oriented to Highway 111, however, lighting shall not interfere with vehicular traffic.

### GENERAL PROVISIONS

- Use of mercury vapor or exposed fluorescent lights is discouraged. Warm white lighting is encouraged.
- No lighting shall blink, flash, or be of unusually high intensity or brightness.

- Automatic timers on lighting shall be designed to maximize personal safety during nighttime use while saving energy.

### HIGHWAY 111 LIGHTING

- Recessed landscape uplighting and ground lighting bollards shall be of constant design with durable finishes and materials, to create unity along Highway 111.

### ON-SITE PARKING LOT AND PEDESTRIAN LIGHTING

- In pedestrian areas the use of low level walkway lighting is preferred over area lighting from above. Lights shall be positioned no higher than four (4') feet above grade.
- Concrete or metal light bollards not exceeding four (4') feet in height may be used to accentuate building entrance areas or to provide separation for pedestrians from vehicular traffic lanes.
- Pedestrian scale lighting shall illuminate entryways, courtyards, parking lots, and other such areas.
- For pedestrian walkways point-to-point lighting is acceptable with no specific illumination levels required. The main emphasis shall be to clearly identify the pedestrian walkway and direction of travel.
- Lighting shall be used to enhance landscaping and reinforce architecture, with dramatic uplighting or wall shadow effects with plant materials encouraged.
- Parking lot light fixtures and bollards shall be consistent in styling with the design theme proposed for that sub-area.
- Light standards shall not exceed fifteen (15) feet in height or the height of the building, whichever is less.

### ARCHITECTURAL LIGHTING

- Service area lighting shall be contained within service yard boundaries, with light sources concealed.

- Building illumination and architectural lighting shall be creative and must reinforce the design theme. Indirect wall lighting or “wall washing,” overhead down lighting, or interior illumination which spills outside is encouraged.
- Lighting shall not cast any glare onto adjacent lots and streets in such a manner as to decrease the safety of pedestrian and vehicular movement.
- Architectural lighting shall be used to articulate the particular building design. Rim lighting of eaves, bending, uplighting, wall washing, and other effects shall be used in compliance with the design theme for the sub-area.
- Building or wall lighting shall be indirect. A limited number of lights may be used to create shadow, relief and outline effects.
- Architectural lighting shall be integrated with building elements and concealed flush with grade wherever possible.
- Wall “washing” effects shall be sparingly used to accentuate architecture at key visual areas and not an attempt to advertise buildings to roadways.
- Architectural lighting shall not be visually apparent during the daytime.
- Interior building lighting may be left on at night (to the extent that energy use is justifiable) to enhance pedestrian activity on the street.

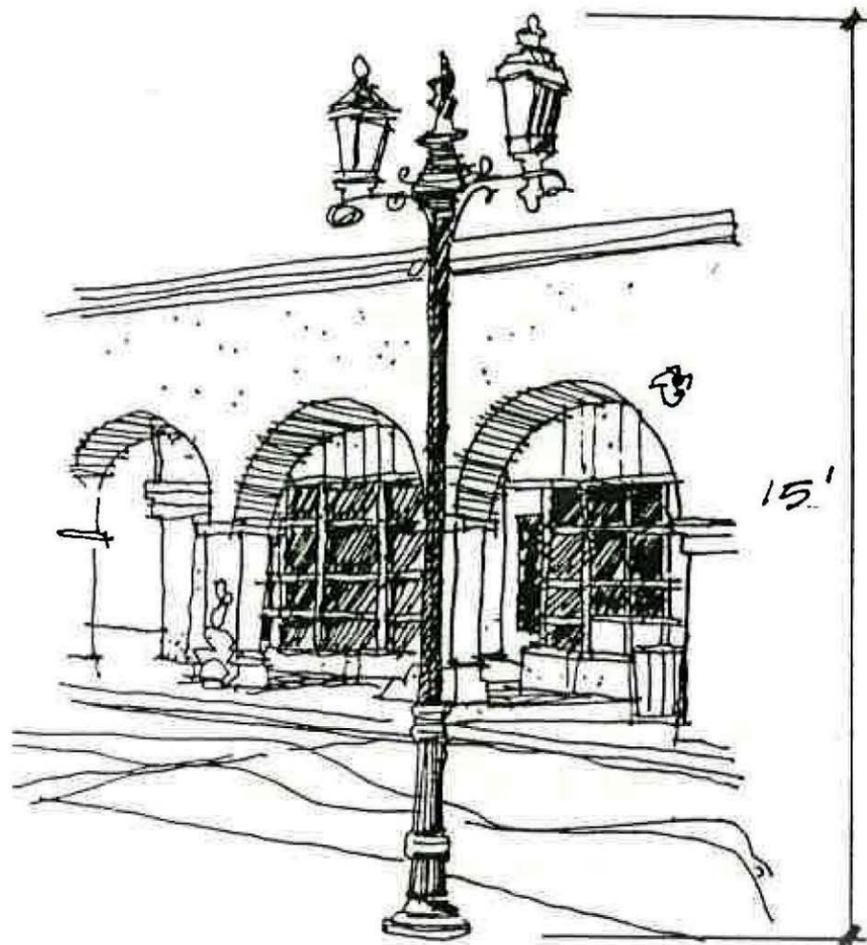
### LANDSCAPE LIGHTING

- Lighting shall be used to highlight landscape features such as specimen trees, slopes or pedestrian areas.
- As with architectural lighting, light sources shall be concealed flush with grade or otherwise screened during the daytime.
- String lights, with small nonflashing bulbs, may be used to highlight trees and similar features within public plazas and other similar outdoor areas at night to create excitement and a festive ambiance.

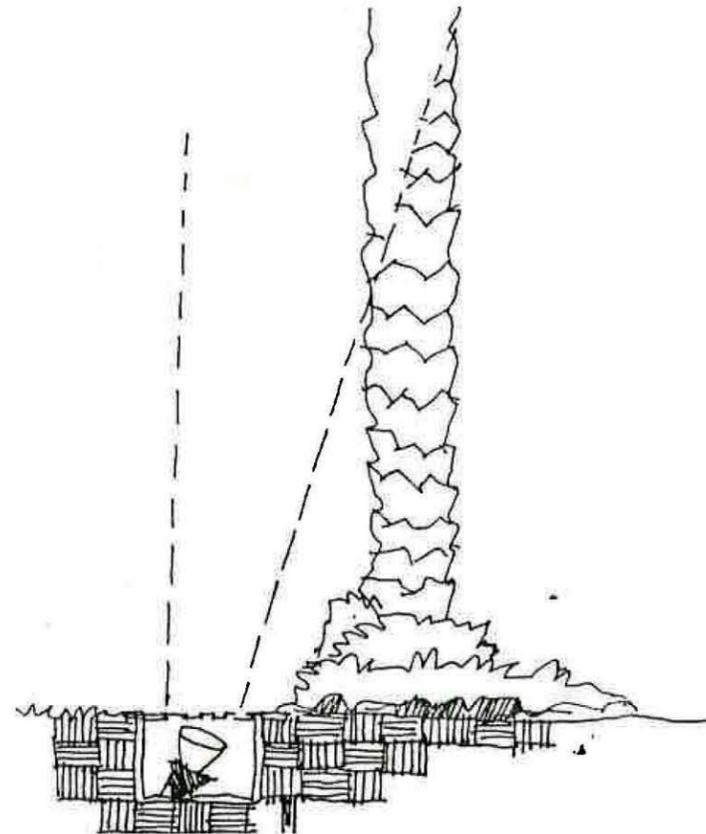


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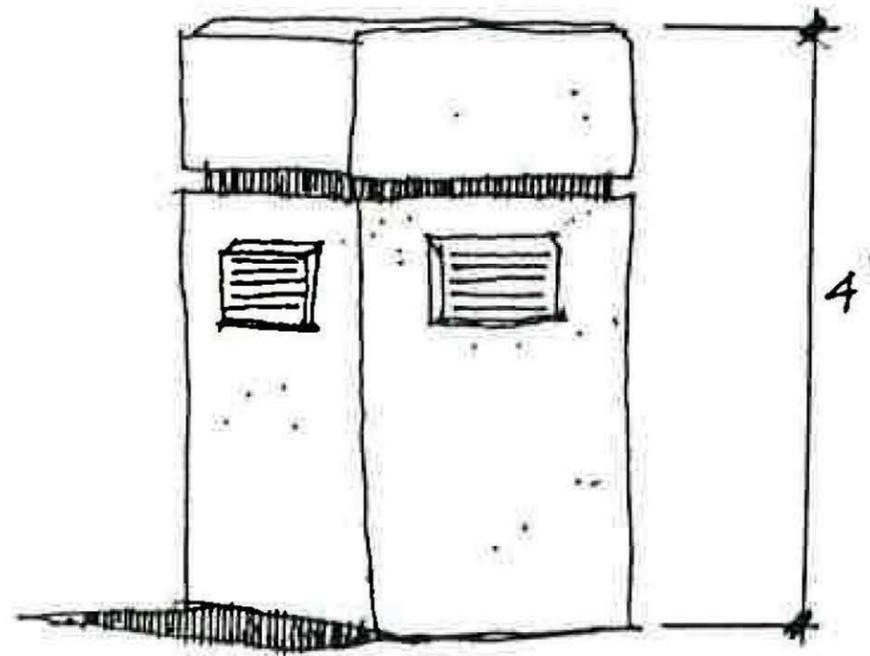
**ILLUSTRATION 11**  
Lighting Design Principles



Light standards shall not exceed 15 feet. Warm, white light is encouraged. Frequency and distribution shall be controlled. Light is to be directed down to preserve the desert night sky.



Lighting shall be used to highlight landscape features such as slopes and specimen trees.



Concrete or metal light bollards may be used to accentuate building entrance areas and provide ground lighting.



## 12. Parking and Parking “Plazas”

Parking is an important site planning consideration in as much as parking areas take over half of the ground space in most developments. In Rancho Mirage parking shall be actively incorporated into overall project design and elevated to multi-use potential as “parking plazas.”

On-site parking will be provided in the 111 West Specific Plan area in accordance with the standards found in Chapter 17.24 of the City of Rancho Mirage Zoning Ordinance. Reduced parking area and shared parking incentives are integral to this Specific Plan and applicability will be considered by the City on a case-by-case basis in the process of Development Plan review.

The following guidelines are in addition to the parking requirements set forth above:

- Parking in commercial areas shall be configured to provide convenient vehicular access to customers, and shall be arranged to allow easy pedestrian movement from the parking to the destination.
- In designing parking lots, thought shall be given to the “entry sequence” that channels a vehicle from the street to the building entrance, with a logical path to the parking space clearly apparent. Drivers shall be allowed to orient themselves and perform drop-off and pick-up functions with conveniently located right-side passenger drop-offs at main business entries, in addition to walking from cars to entrances.
- Parking shall be designed so that it may easily accommodate expansions to structures without ruining the integrity of the parking lot design and circulation systems. It shall also allow for integration with adjacent parking areas and through circulation. Adequate parking must be provided adjacent to uses that will require that parking.
- Employees are encouraged to park in designated parking areas that are least convenient to visitors.
- Handicap parking shall be provided per City code, with consideration for additional handicap spaces and the reduction of compact spaces for medical office and related uses because of the larger passenger loading space needed.

- Shared parking concepts which allow adjacent uses with different peak parking demand times to share spaces are encouraged whenever possible. Examples include offices/theaters, schools/churches, and high density apartments/professional office uses. The main point for consideration is parking demand differences based on time (i.e. day vs. evening, weekday vs. weekend).
- Parking lots shall be well landscaped and screened from view from public right-of-ways. Every parking lot shall be bordered by a three (3) foot high screen which will consist of a wall, hedge, landscaped berming or some combination thereof. (See Landscape Guidelines, Chapter 8.)
- If the parking lot layout is one or two rows deep, walking space with a minimum of four feet between stalls shall be provided every ten spaces and at key locations to create pedestrian corridors.
- Vehicular access to parking lots shall be clearly marked with clear distinctions between long-term and short-term parking.
- Large areas of asphalt or concrete paving for parking lots can be uninteresting and can deflect heat into nearby buildings. Lots shall be landscaped to reduce paved surface areas. (See Landscape Guidelines, Chapter 8.)
- Parking structures for large uses are permitted provided it blends with the district’s architectural theme, and is screened and landscaped on all sides. Terracing and facade detail and variation is encouraged and may be required to reduce the visual impact of structures from public areas.
- Covered parking is allowed provided the structure blends with the architectural theme.
- All employee and visitor parking shall be provided on-site or on a contiguous site. Parking shall be shown on Site Plans and shall be clearly identified by on-site signage. (See Chapter 10, Signage.)
- When parking is provided on a site of different ownership, a recorded document signed by the owners of the alternate site, stipulating to the permanent reservation of use of the site for said parking shall be approved by the Community Development Director.
- Circulation within the parking areas shall allow relatively free flow of vehicular traffic.

- Parking lot construction standards shall be in compliance with City Code.
- No on-street parking is permitted. City signage will designate no parking on streets.
- Bicycle racks shall be provided in convenient locations to provide access to buildings.
- All handicapped spaces shall be constructed to comply with California State Accessibility Standards, Title 24. They shall be as near a building’s primary entrance as possible.
- Changes in occupancy or type of use, and increases in the intensity of use on any site may require changes in parking. Sufficient parking to meet the requirements shall be provided.

### RESIDENTIAL GARAGES AND CARPORTS

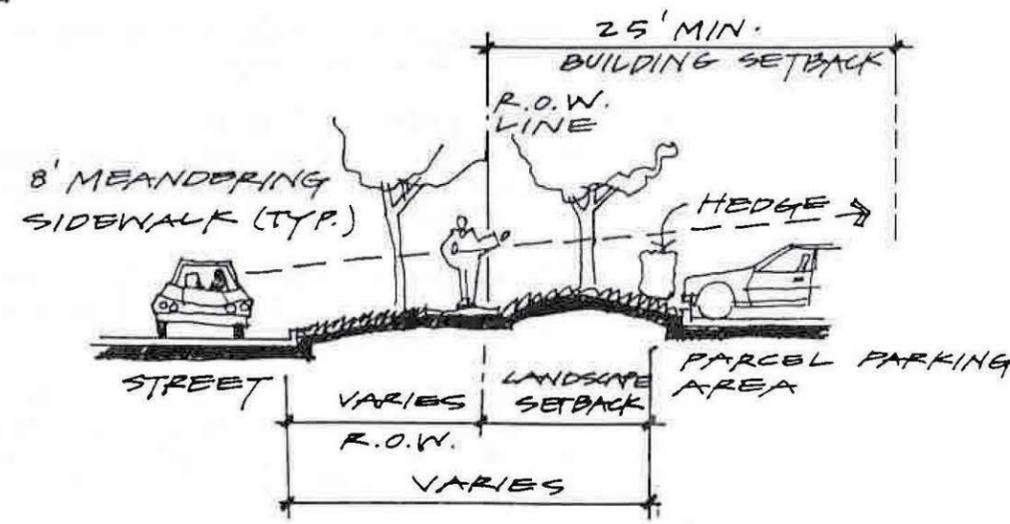
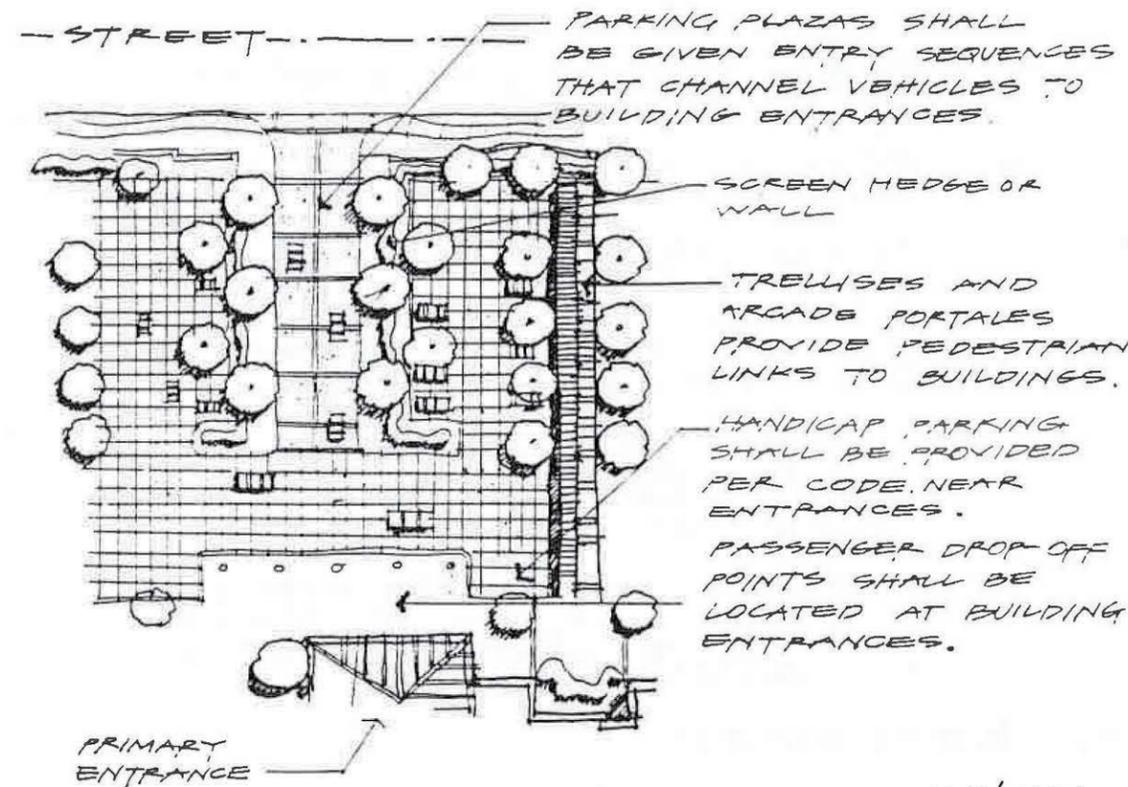
- The City may require that up to 50% of required parking spaces be covered by shade structures, in addition to shade tree requirements.
- Parking shelters shall be of wood or stucco and shall echo the predominant architectural theme. Integration with courtyard and screen walls and property line enclosures is encouraged.
- Parking shelters shall be visually screened on three sides by the use of earth forms, planting or architectural elements.
- Wood and metal doors and gates are allowed if consistent in material, style and color with the themes of surrounding architecture. Doors of other materials are prohibited.
- Roofs of structures shall echo predominant architectural themes in pitch and material.
- Parking shelters for multi-family residential units shall be no more than 8 spaces wide with a minimum parking island of 9 feet wide between shelters. Small multi-family parking shelters under four spaces are to be avoided.
- In residential areas garage entrances from alleys, corners, or side yards are encouraged wherever possible to prevent garage doors from dominating the streetscape.



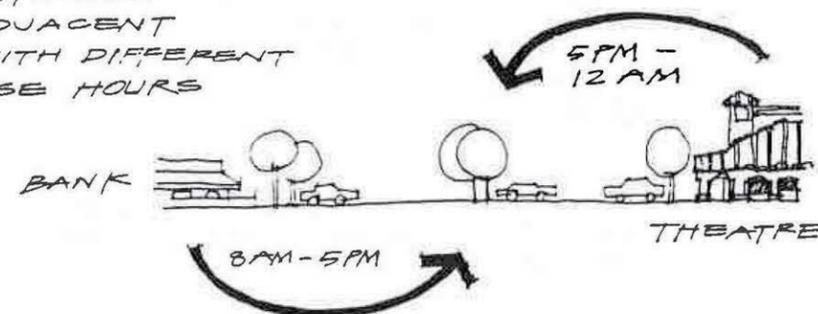
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### ILLUSTRATION 12

Parking Plaza Concept



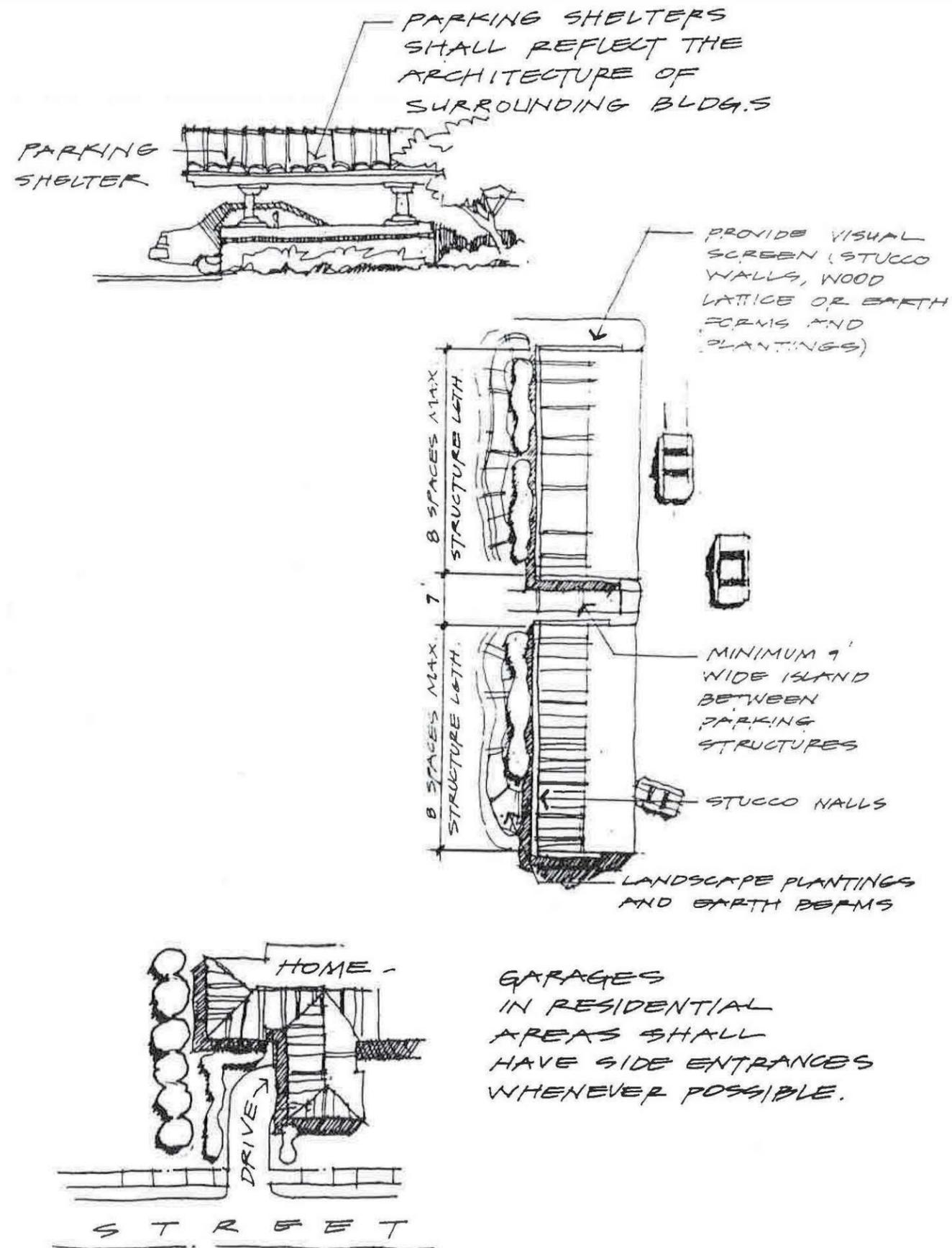
SHARED PARKING IS ENCOURAGED FOR ADJACENT USES WITH DIFFERENT PEAK USE HOURS





CITY OF RANCHO MIRAGE

**ILLUSTRATION 13**  
Parking Shelter Design Parameters





## 13. Architectural Guidelines

### INTRODUCTION

Over time, the influences of climate and history have combined to produce a unique architectural response in the desert areas of the Southwest. The architectural styles that best reflect this design character, loosely called “Spanish” or “Mediterranean”, which are recommended for Rancho Mirage are **California Mission Revival and Spanish Colonial Revival**.

The following text and graphics include a brief outline of the special architectural character and quality of these two major styles. Each reflects the region’s cultural heritage through the materials and the configuration of forms developed for environmental protection from sun, wind, moisture and temperature extremes. Each offers a small scale and intimate feeling to those that experience them and to the street and public spaces they adjoin, special qualities sought after in the architectural “signature” of the City of Rancho Mirage.

The accompanying images are tangible examples of these architectural forms and style. They are included not as restrictions, but as departure points for creative adaptations and innovation.

Designs in the Rancho Mirage Community may be contemporary or traditional interpretations, as long as they are comprised of a consistent and appropriate architectural vocabulary that is as true to the principles of the styles as possible. No single style is to be applied to the various uses within the Specific Plan. A framework based on common design elements has been created that will integrate building designs and project areas as a community while allowing each its own slightly different character.

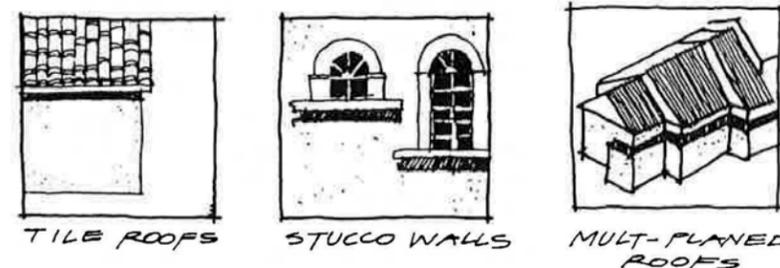
It is the intention of these guidelines to foster a community of unique architectural solutions that are appropriate for the project, rather than stage set, or “doll house” architecture.

Projects which follow these guidelines and the Development regulations in this report are actively encouraged. Creative and sensitive projects which do not conform to all of the guidelines will nevertheless be given careful consideration by the City.

### POSITIVE AND NEGATIVE DESIGN FEATURES

The recommended styles have many similarities of scale, proportion, massing, and color that are uniquely appropriate to the desert southwest. Furthermore, each of the styles has a common bond of detailing and plan form that is related to the environmental design requirements of the region:

- one or two story building heights
- red clay tile roofs
- shallow-pitched tile roofs (2:12 - 6:12 pitch)
- solid, massive walls
- smooth stucco wall surfaces
- arcades of round headed arches, set on columns
- enclosed courtyards
- recessed wall openings
- white or soft pastel earth tone colors
- ceramic tile accents such as planters, benches, fountains, etc.



It is the use of these design elements that is encouraged for both commercial and residential buildings in Rancho Mirage to accomplish the following:

- intimacy
- small scale sense of enclosure (courts or arcaded spaces)
- play of light (shade and shadow)
- “layering” of views and spaces

- vegetation close to, within or on buildings
- richness of surface

The elements to avoid or minimize are:

- high pitched or extensive flat roofs
- simple-box-like architecture
- large blank walls
- exaggerated elements (extremely tall columns, oversize moldings, unrelenting repetition)
- rustic (“woody”) architectural styles
- large roof/eave overhangs
- extensive use of wood, glass or metal
- slumpstone walls
- wood siding
- dark wall colors



## SITE PLANNING CONSIDERATIONS

### Building Orientation and Siting

- The orientation of the building and its elements shall be related to the natural design factors of sun, wind and rain. Fenestration shall be considered with exposure to natural elements in mind.
- Buildings shall be sited so windows and entries take advantage of views.
- Building complexes shall be laid out such that usable courts and gardens are created by building placement. Courts and gardens shall be protected as much as possible from spring winds while taking advantage of winter solar access.
- Convenient passenger drop-off courts shall be designed into the public areas of the project which work well with circulation and “right side” unloading.
- All designs shall appear as an integrated part of an overall site design concept.
- If restaurants, commercial retail uses or offices have public access on the backside of lots, the structures must demonstrate equal attention to architectural detail and quality on those backsides.

### Building Height

- Building or structure height shall not exceed that permitted by the Specific Plan.
- Regardless of building height, it is the intent of these guidelines to provide the greatest variation in building massing and setback possible

### Building Mass

- Building mass shall be simple in form and of strong geometry.
- The height and bulk of buildings shall not unduly block views and the solar access of adjacent and other nearby buildings. Full advantage of view corridors and solar access shall be taken in project design.

- Clusters of buildings shall be of similar scale and mass with one “key” building or element which stands out from each cluster. A “tower” is an example of one such architectural feature.

## PLAN COMPONENTS

### Courtyards

Courtyards in various forms are desirable plan characteristics for all types of structures due to the extremes of climate in the desert. They provide a sense of enclosure and privacy, particularly in higher density plan configurations where a sense of activity is to be fostered. Acceptable variations are atriums, patios and gardens.

### Portales

Portales are covered porches. They offer shade and wind protection and a special space for indoor/outdoor living. Each style has its own characteristics of detailing, but the dimension shall provide for at least one hundred (100) feet of roof cover.

### Zaguans

Zaguans are open passageways from the street to an internal patio. Introduced by the Spanish, they allow for a building to have a strong street presence while allowing the maximum number of rooms to have exposure to an internal courtyard or patio. The width shall be proportional to the length, but no less than (six) 6 feet.

### Towers

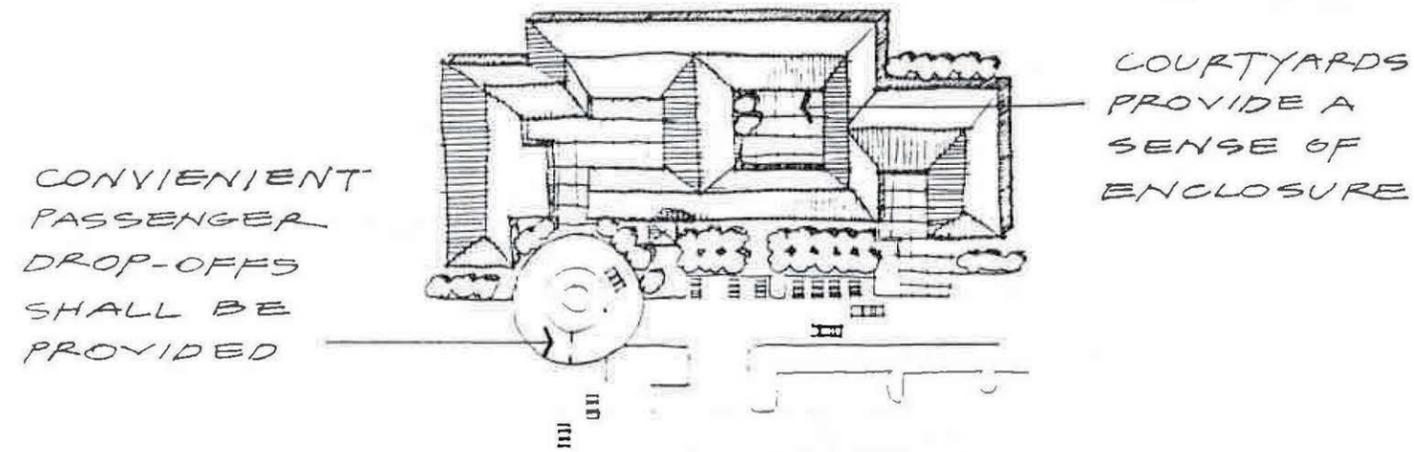
Towers are encouraged as part of the Rancho Mirage concept. However, they should be used minimally only as special focal points.



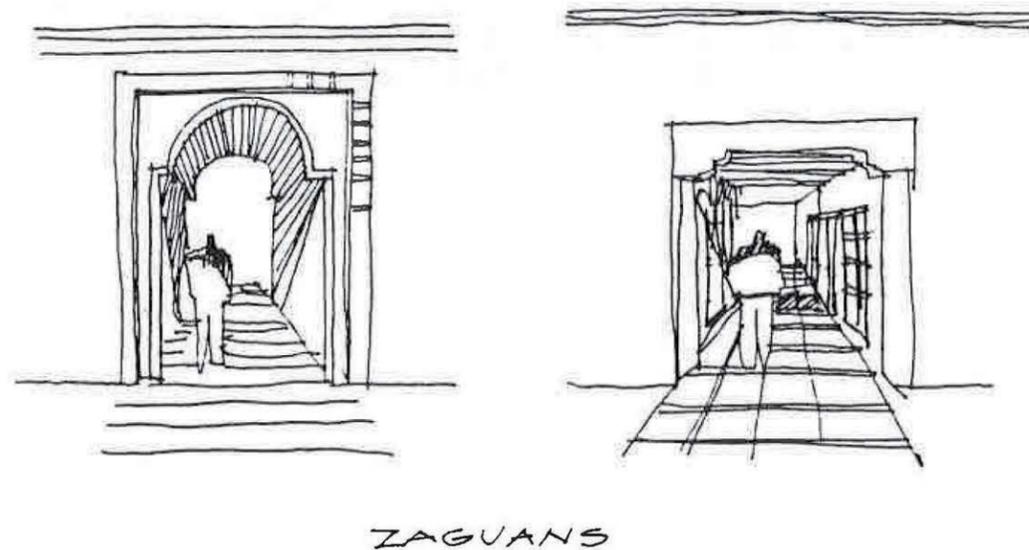
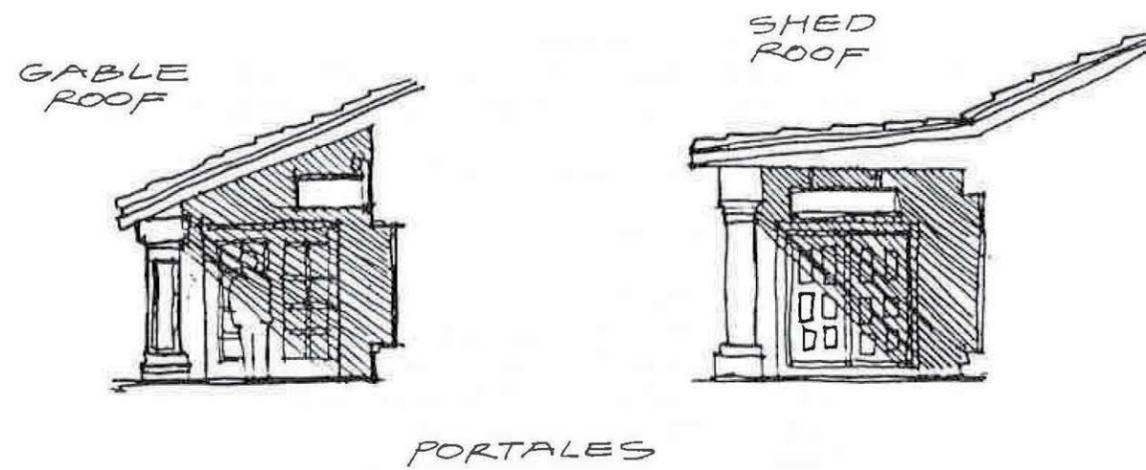
CITY OF RANCHO MIRAGE

## ILLUSTRATION 14

Plan Components



BUILDING COURTYARD AND PATIO CONFIGURATION



**COMMON DETAILS****Form and Mass**

- Simple, strong one and two story volumes
- Eaves and simple tiles
- Courtyards and patios used extensively
- Finished edge of platforms with molding continuing around the building as a belt course
- Typical chimney cap and molding

**Materials and Colors**

- Stucco (smooth texture) is typical
- Clay barrel tiles (red) are preferred
- White, tones of peach or sandy pink shall be the primary colors and accents of blue or green

**Openings**

- Limited use of arches
- Windows deeply set or defined with molded surrounds, window grills may be used with divided lights
- Doors shall appear to be of heavy carved wood

**Roof Shapes and Details**

- Sloped barrel tile roof
- Roofs of 2 in 12 to 6 in 12 pitch
- Double tile rake flush with the wall or corbeles
- Eaves flush with wall or with exposed beams
- Overhanging beams shaped at ends
- Gutter can be used as fascia trim
- Opening protected by overhangs

- Roof pitch can be 2 in 12 over balconies or accent features
- Finished edge of platforms with molding continuous around the building as a belt course
- Attention to chimney cap and molding



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## ILLUSTRATION 15

Common Details



TOWERS ACT AS  
KEY VISUAL ELEMENTS  
AND FOCAL POINTS FOR  
BUILDING CLUSTERS

BUILDINGS SHALL NOT  
EXCEED 35' IN  
HEIGHT

HEAVY WOODEN BEAMS -  
DARK STAIN OR PASTEL  
PAINT.

SMOOTH FINISH  
STUCCO - WHITE OR PASTEL.

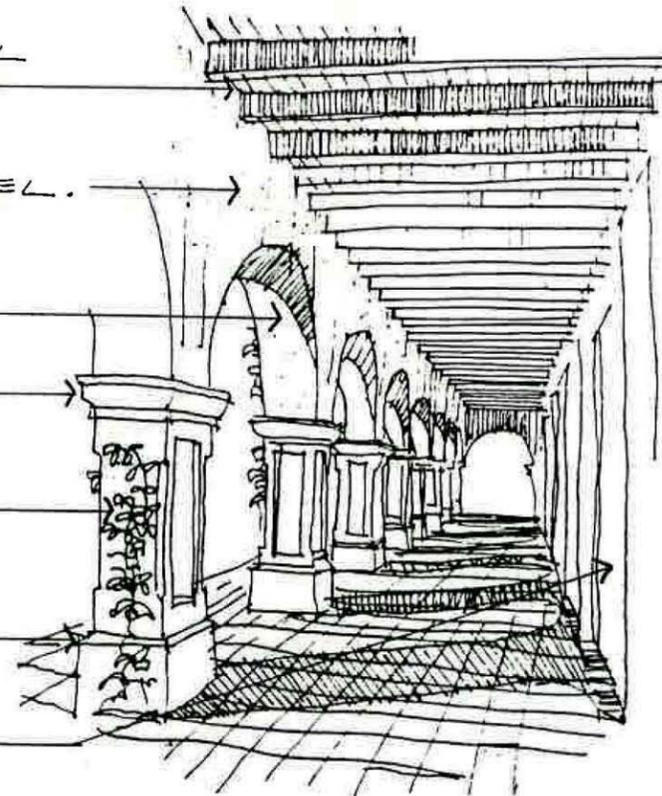
ROUND OR  
FLAT ARCHES.

CAPITALS.

VINES.

COLUMN BANDS.

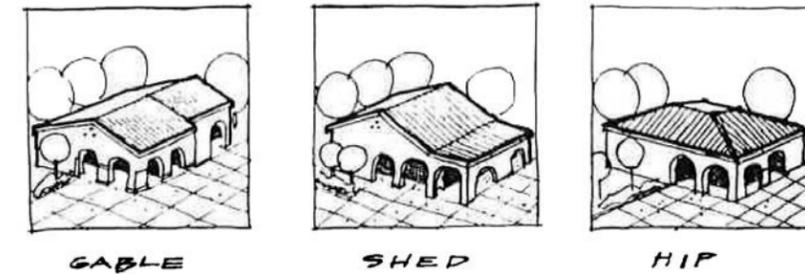
DEEPLY RECESSED  
WINDOWS AND DOORS.



**Roof Pitch and Materials**

Roof pitch for mission style architecture is typically low pitch. A typical 2:12 to 6:12 roof pitch (vertical rise to horizontal run) is required for all main roof surfaces, with the exception of arcades or colonnades.

Roof pitch for attached arcades or colonnades shall be a minimum of 2:12. Roof types shall be restricted to low-pitch hip roofs, with the occasional use of a gable roof or shed roof as an accent at the end of the building. The use of a mansard, A-frame, gambrel or flat roofs is specifically prohibited.



Roof material shall be comprised of standard mission barrel clay tiles. Each tile shall be a uniform reddish color and non-reflective (unglazed) for a soft finished look. Plastic tiles shall not be permitted.

On primary (street facing) roofs, the tiles shall be “U” tiles. Secondary (non-street facing) roofs may utilize “S” tiles of clay or concrete.

A random application of tiles is preferred to a symmetrical design. Tiles at the ends of the eaves shall be double or triple layered with exposed rusticated mortar to emphasize weight and thickness.

All flashing, vents, pipes and sheet metal will be colored to match or be subordinate to the adjoining roof or wall material. Solar panels are to be flush with the roof slope and screened from view.

**Building Exterior Walls**

Building exterior walls shall have the appearance of stucco and the surface finish shall be smooth. Heavily textured stucco walls are prohibited. Mortar-washed brick as a secondary material is acceptable.

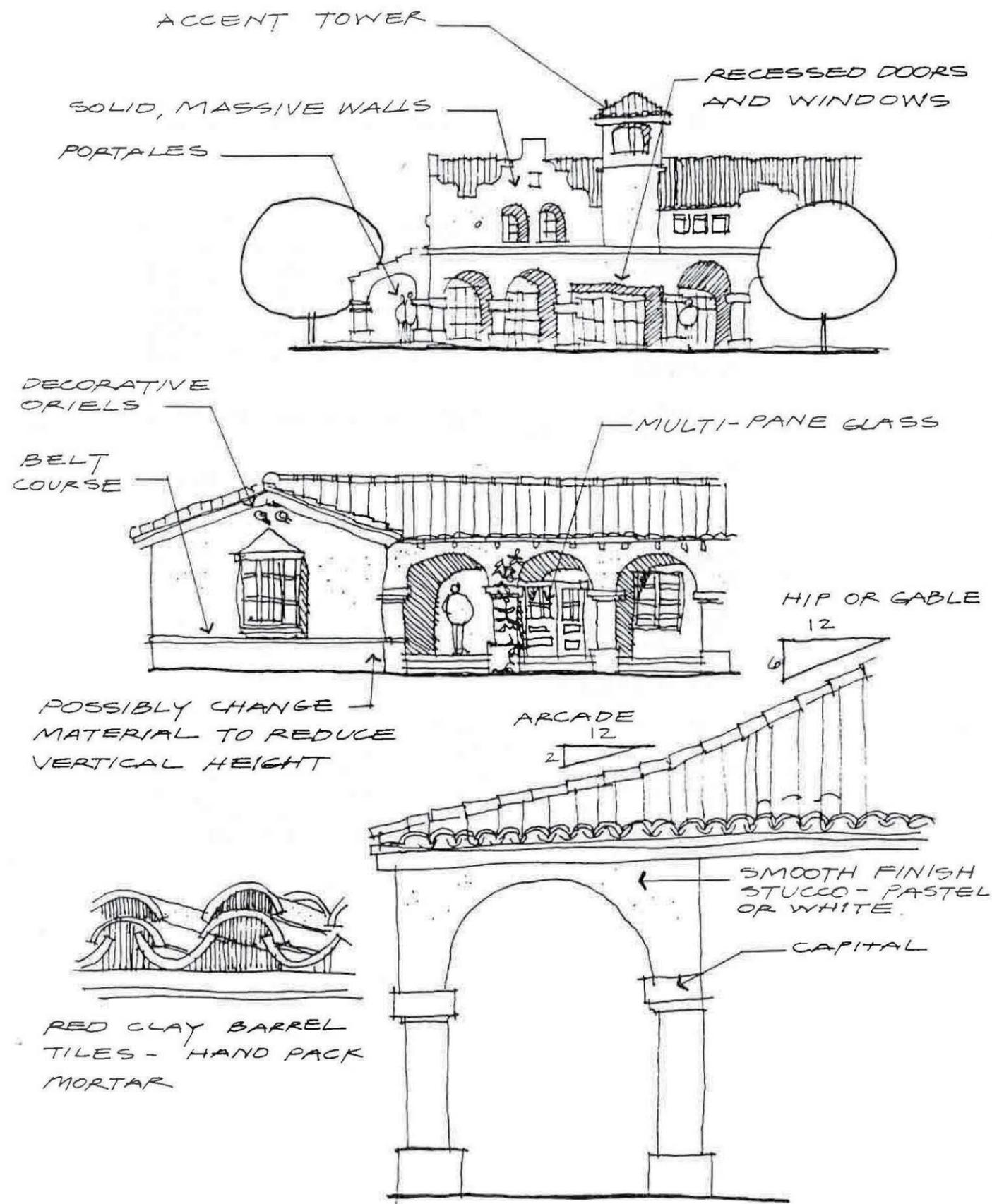
Wood, metal, glass, brick or slumpstone are specifically prohibited as predominant exterior wall materials.



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### ILLUSTRATION 16

Roofs and Walls



**Moldings**

A decoration of the wall or column surface itself creates lines and sculptural solidity.

**Columns and Arcades**

Stucco columns shall be square in cross-section and appear massive in thickness: 16 inches per side is desirable, the use of column bands and capitals is encouraged. A capital, approximately 2 to 3 inches thick by 3 to 6 inches high, shall be incorporated at the top of columns. The column's height shall be 4 to 5 times the width of the column (16 inches) a base band, height approximately one-half the column width, shall be provided.

Adequate thickness and height are important for pleasant and functional arcades.

Arcades shall have semi-circular arches in regular series with columns as supports. Variations such as flat arches are also acceptable.

Arcades, patios and colonnades are typically paved with tile, brick or stone.

The historic style of these elements does not require expensive materials and techniques.

**Shade and Shadow**

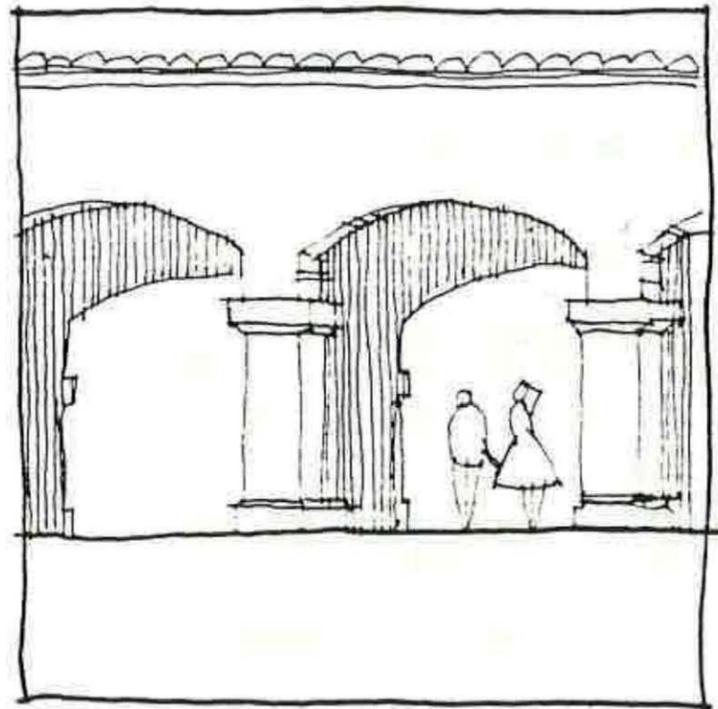
Because of the intensity and heat of the desert sun, each style seeks to create patterns of shade and shadow. These are provided by deep set windows and large overhangs where there are large areas of glass that are directly exposed to the sun. Particularly important are the shade and shadow patterns of the landscaping that soften the plain stucco or adobe surfaces.



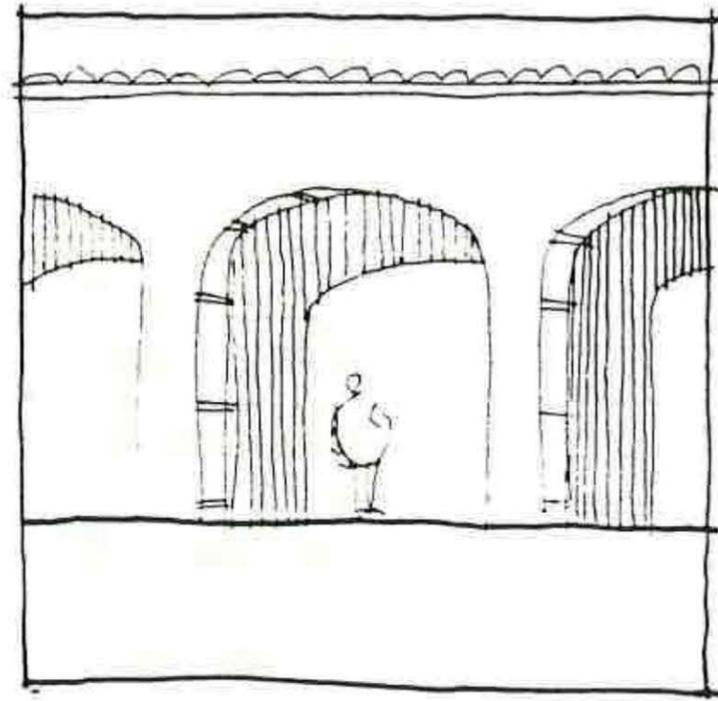
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## ILLUSTRATION 17

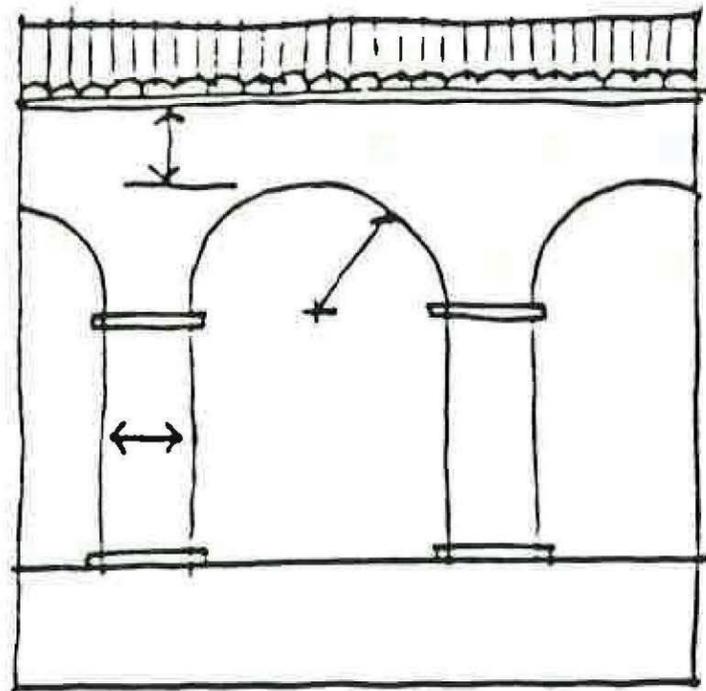
Columns and Arches



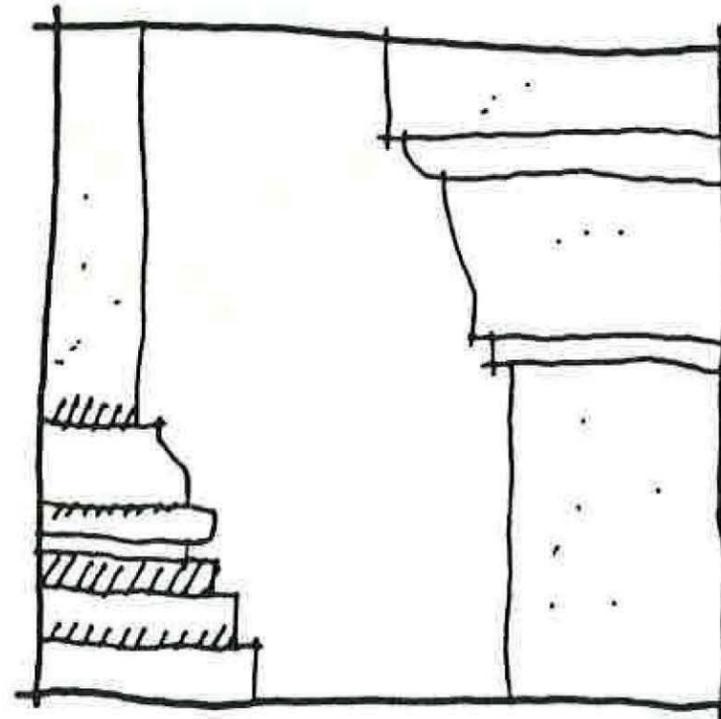
THIS...  
Columns are accented by decorative moldings.



NOT THIS...  
No articulation of columns.



Columns should appear massive and proportional.



Column bands and capitals.

**Windows**

Windows may be either simple or decorated but are best expressed with balanced proportions.

Mission-style windows range in shape from small rectangular, multi-paned casement frames to large round-headed windows. Deeply recessed windows (minimum of 12 inches) in thick walls is an important characteristic. Oriel windows, also deeply sent, can provide secondary architectural accent.

The use of flow pot shelves beneath windows and overhanging moldings and sills is also encouraged.

To add strength to a window, a border of paint or decorative tile works well.

Multi-paned windows recessed into wall surfaces are strongly encouraged as display windows for commercial areas.

Decorative iron grills on windows off-set massive walls and simple window openings.

Acceptable trim colors include pastel blues, greens, reds, browns and beiges.

The use of louvered glass, reflective mirror glass or non-anodized aluminum frame windows is inappropriate. Glass block is permitted if recessed and used selectively and in moderation.

Fabric awnings are permitted in moderation, while metal awnings are prohibited.



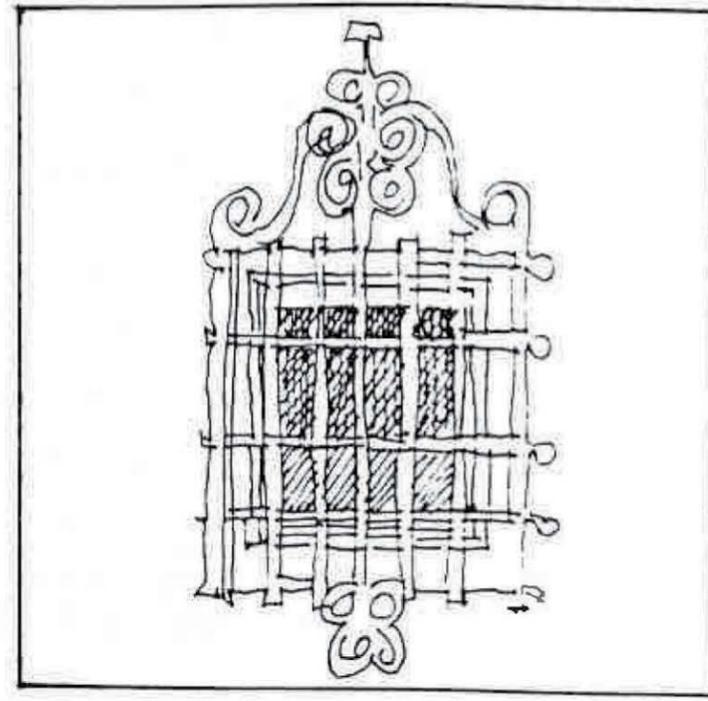
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## ILLUSTRATION 18

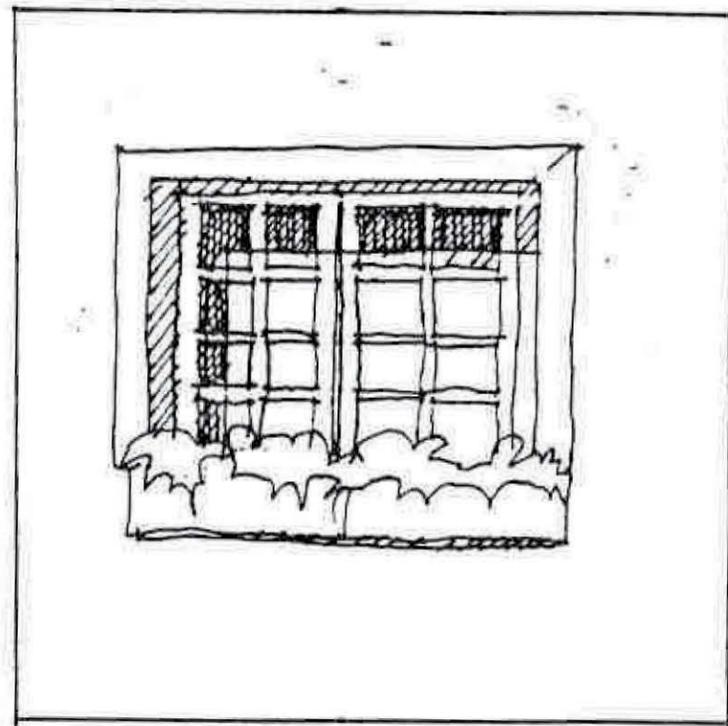
Windows



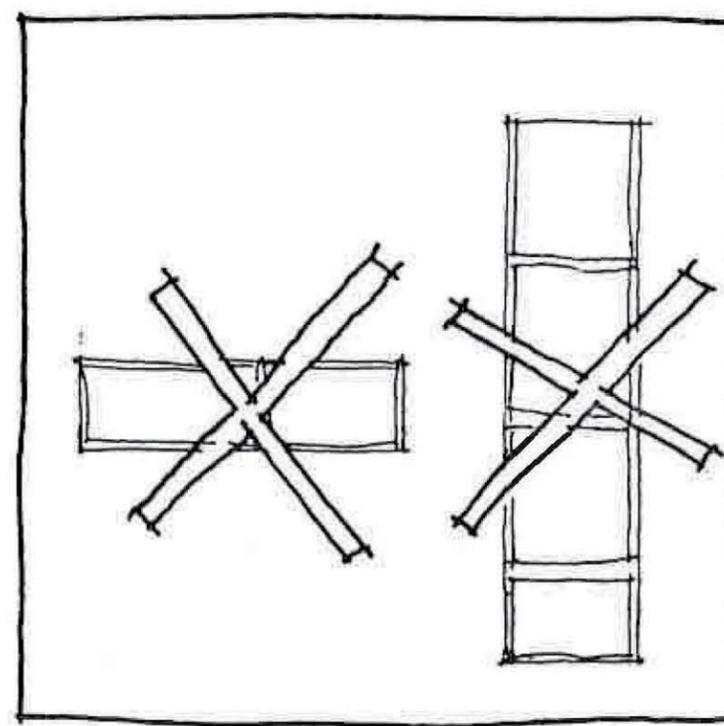
Multi-lited windows recessed into storefronts.



Iron grills create interesting shadows.



Window boxes below deeply recessed casement windows.



Windows may be simple or decorated but are best kept in balanced proportions.

**Exterior Doors**

Mission Revival and Spanish Colonial exterior doors are typically made of heavy wooden planks or panels. Doorways are often arched with a glass fan window above the door header. Arches over doors are strongly recommended. Doors shall have plain or decorated moldings and shall be recessed a minimum of 12" into primary building walls.

Shallow recess may be visually strengthened by a frame.

French doors are also acceptable.

Trim treatment of doors, as with windows, may be tile or painted. Doors themselves are usually painted or stained to contrast with the building color.

The use of non-anodized aluminum frame glass doors or sliding doors not in keeping with the architectural theme is prohibited.

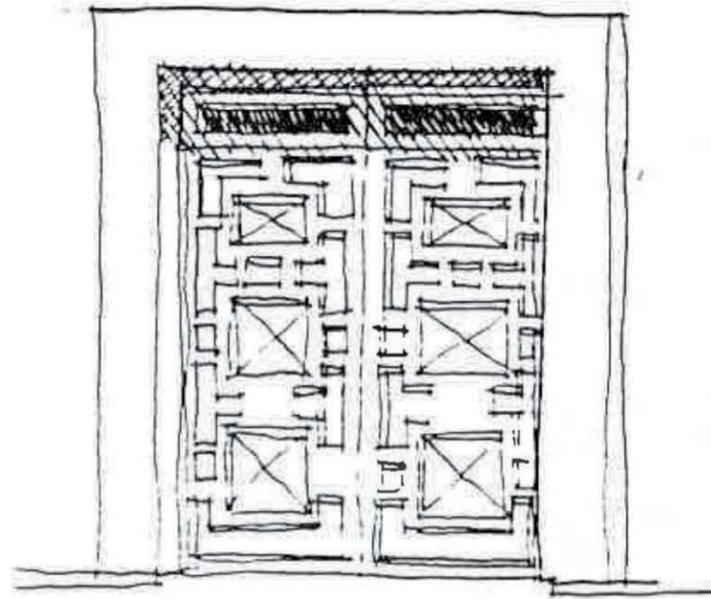
The design of gates, utility enclosure access, or garage doors shall be of wood or metal with a "heavy" appearance. Colors shall match the other building doors and contrast the building color.



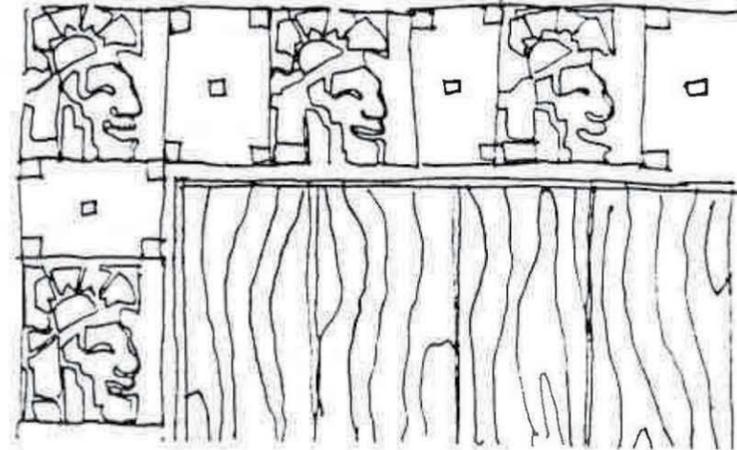
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## ILLUSTRATION 19

Doors



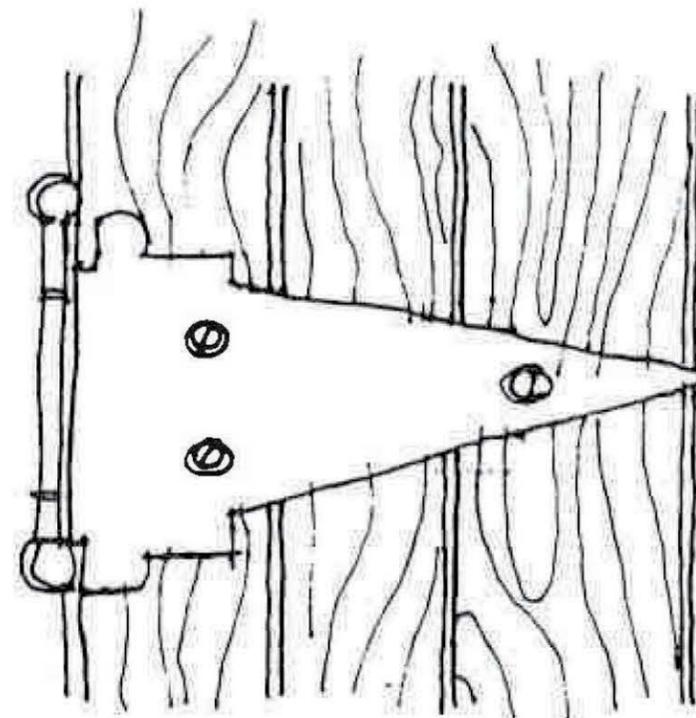
Recess doors 12" minimum into walls.



Tile or painted accents around doorways.



Half round fan windows above and painted or stained panel doors.



Forged iron exposed hinges on heavy wooden plank or panel doors.



**Paving**

Paving within arcades, plazas, crosswalks and walkways shall enhance the “pedestrian” nature of the space and shall consist of one of the following:

- Red clay tile (12" x 12")
- Textured, stamped concrete, deep red, to resemble tile
- Stamped concrete cobbles
- Brick or brick pavers, reds or browns
- Decomposed granite (for footpaths)

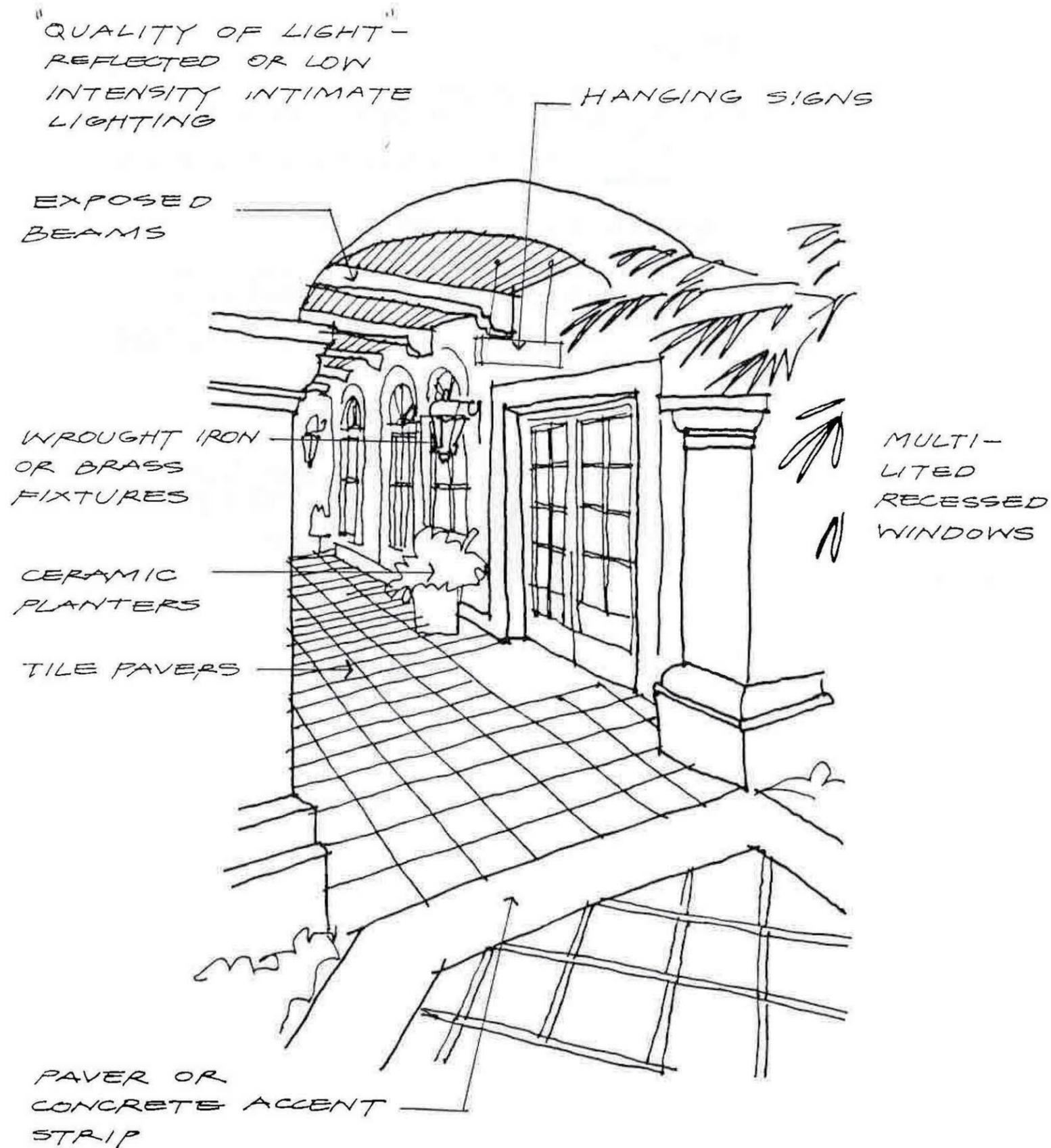
Avoid the use of unstamped concrete, dark brick, glazed brick pavers, rustic stone (flagstone) and river rock.



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## ILLUSTRATION 20

Paving





**Wells and Fences**

- Heavy thick walls with stucco finish
- Tile cap - either flat or sloped or optional brick cap wall
- Paneled wood gate or door with or without gate enclosure

**Pergolas at Balconies or at Grade**

- Heavy timber beams with decorative corbeled ends
- Stucco, precast concrete or carved stone columns
- Balcony walls with stucco, tile, or wood cap--decorative round tile, stucco, or prefabricated concrete panels.

**Shade Structures**

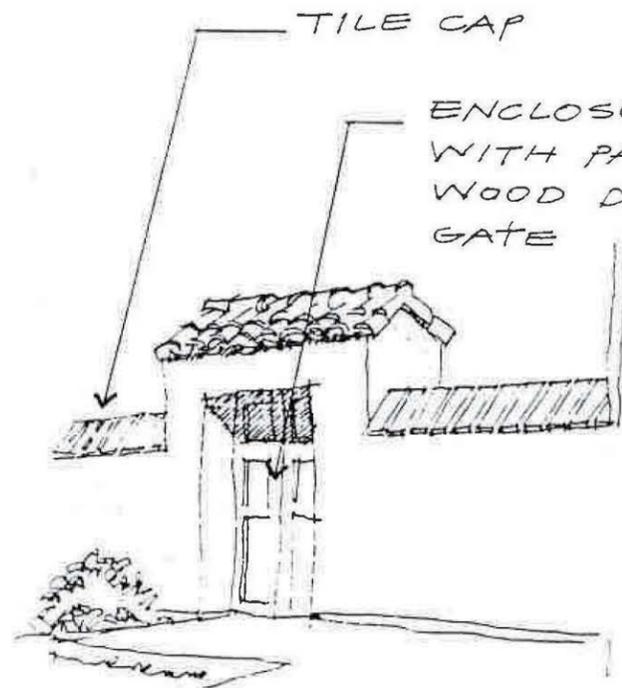
- Heavy timber beams and trellis
- Stucco, prefab concrete, or carved stone columns
- Stucco walls with articulated cap molding of wood, ceramic tile or stucco
- Occasional round tile or masonry panels



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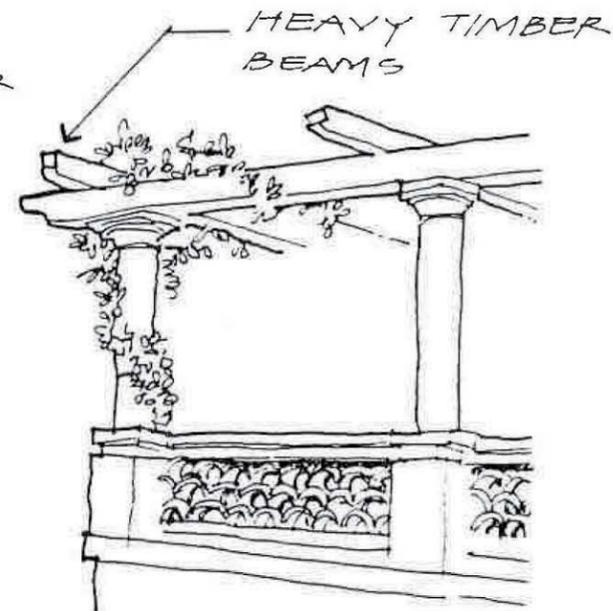
## ILLUSTRATION 21

Walls



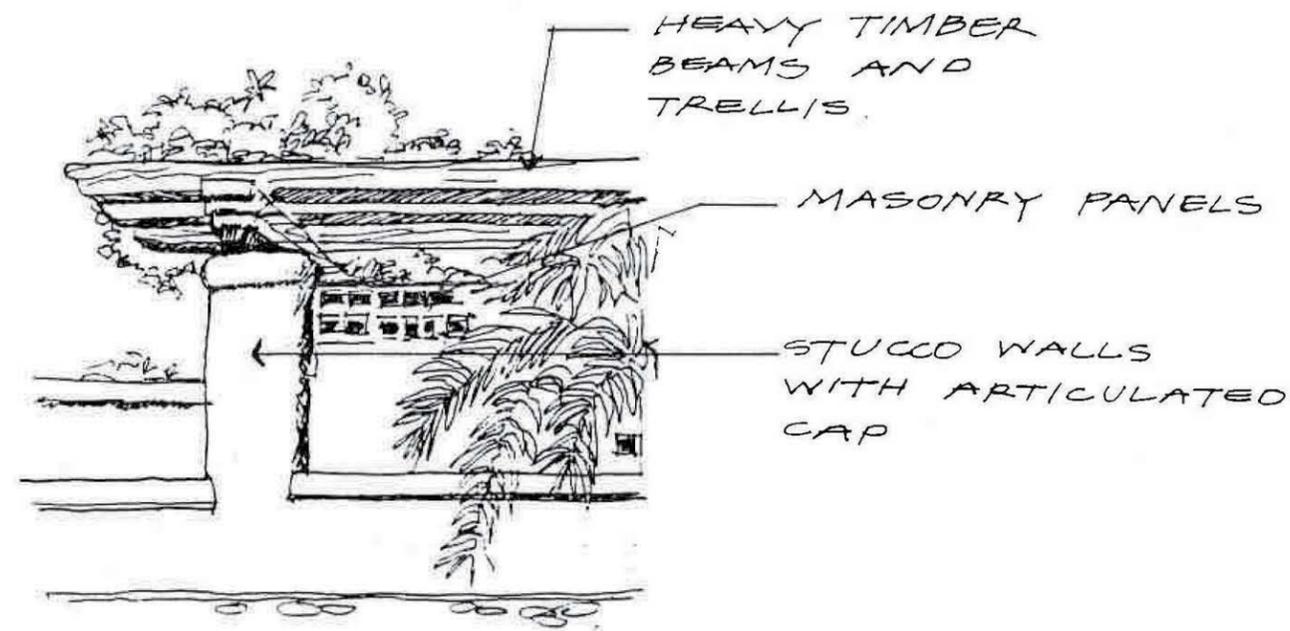
HEAVY THICK WALLS  
WITH SMOOTH STUCCO  
FINISH

walls



DECORATIVE TILE  
OR STUCCO WALLS

pergolas at balconies



shade structures



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# Section 5

## PROGRAMS AND INCENTIVES

### 14. Implementation

#### ASSISTANCE AND INCENTIVE PROGRAM CRITERIA

Two levels of City actions are envisioned by this plan. The first level consists of ways in which property owners and developers may be assisted in either legally consolidating lots to meet minimum area requirements or completing integrated plans for adjacent parcels which accomplish the same effect as if the land were under common ownership. The second level is aimed at providing incentives for parcels to be either combined or jointly planned in excess of minimum area requirements.

In either case, merely satisfying regulatory requirements in the Specific Plan will not justify either assistance or incentive commitments by the City or Agency. Rather, responsiveness to Design Guidelines will be used to determine the appropriate public role.

The most pivotal criterion is the extent to which the proposed project responds to the Specific Plan Design Guidelines. That responsiveness is the most direct indication of the quality of the project in terms of the plan's intent.

In order to qualify under this criterion, the plan must positively document how it responds to all of the following design categories and guidelines described herein, beyond the strict regulatory code requirements.

- Design objectives;
- Site planning;
- Parking - adequacy of spaces and quality of design;
- Landscape design;
- Lighting;
- Signs; and
- Combined overall implementation of the District's visual intent.

Degree of responsiveness is and will always be a matter of judgment. However, failure to address all the required subjects is not. They must all be addressed.

#### ASSISTANCE AND INCENTIVE PROGRAM ACTIONS

- Providing the criteria above are satisfied and, to the degree that they are satisfied, any or all of the following actions may be taken by the City or Agency as either assistance or incentive programs to achieve lot consolidation or integrated planning. The extent of each individual action or combination of actions taken may vary on a case-by-case basis and will be the result of negotiations between the project applicant and the City.
- Reduction or Waiver of Processing Fees -- May vary from a percentage to the total of such fees (e.g., Site Plan Review, etc.);
- Service Fee Advance -- Partial or total payment or required utility service fees, subject to recovery on a mutually agreeable pay-back schedule;
- Agency Loan for Infrastructure Improvements -- Partial or total funding of required infrastructure improvements, subject to recovery on a mutually agreeable pay-back schedule;
- Legal Assistance -- City legal or Agency staff assistance in lot consolidation transactions, title searches, reciprocal easement establishment or lease cancellation, buyout or amortization procedures and instruments;
- Loan Source Assistance -- Research and establishment of financial contacts to assist in project financing;
- Non-Residential Intensity Increase\* -- Added square footage up to a maximum of 20% above the prescribed square footage;
- Residential Density Increase\* -- Added dwelling units up to a maximum of 35% above the prescribed density;
- Site Planning Assistance -- Funding of site planning costs on a partial or total basis; and
- Agency Land Purchase, Lease or Write-Down -- Amount of lease rate adjustment or write-down if offered, to be determined on a case-by-case basis.

- \* Site development standards may be adjusted as necessary to achieve increased square footage or dwelling units.

#### LOT CONSOLIDATION

##### Purpose and Intent

The consolidation or integration of small lots is encouraged particularly for new development or large scale rehabilitation so as to benefit from greater land use efficiencies and reduced incremental development costs associated with larger development projects. Lot integration refers to planning and improving distinct single lots as a group with the use of cooperative agreements or easements to bring about development that is comparable to what could occur on a consolidated parcel.

##### Applicability

The lot minimums specified in the Development Regulations (Section III, Chapter 5) will apply to all new development, intensifications of use and redevelopment projects. Rehabilitation project will be subject to these minimums only where the structural area is increased 25% or more.

#### LANDSCAPE EASEMENT DEVELOPMENT

Landscape easements that occur along Specific Plan area roadways are intended to provide for landscape and hardscape as set forth in Chapter 4 of the Specific Plan. The development and maintenance of these easements will be governed by agreements between the City and individual property owners.



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