

CLASS 560-C-3250 CONCRETE (NO FLY ASH)
 1.73 CU. FT./L.F.
 1 CU. YD. = 15.60 L.F.

MINIMUM PERMISSIBLE GRADE 0.50%
 MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAXIMUM
 MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%
 ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING
 SEE STANDARD 206 FOR JOINT DETAILS

* USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2,
 CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL
 BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

TYPE "B" (8") CURB & GUTTER

DETAIL

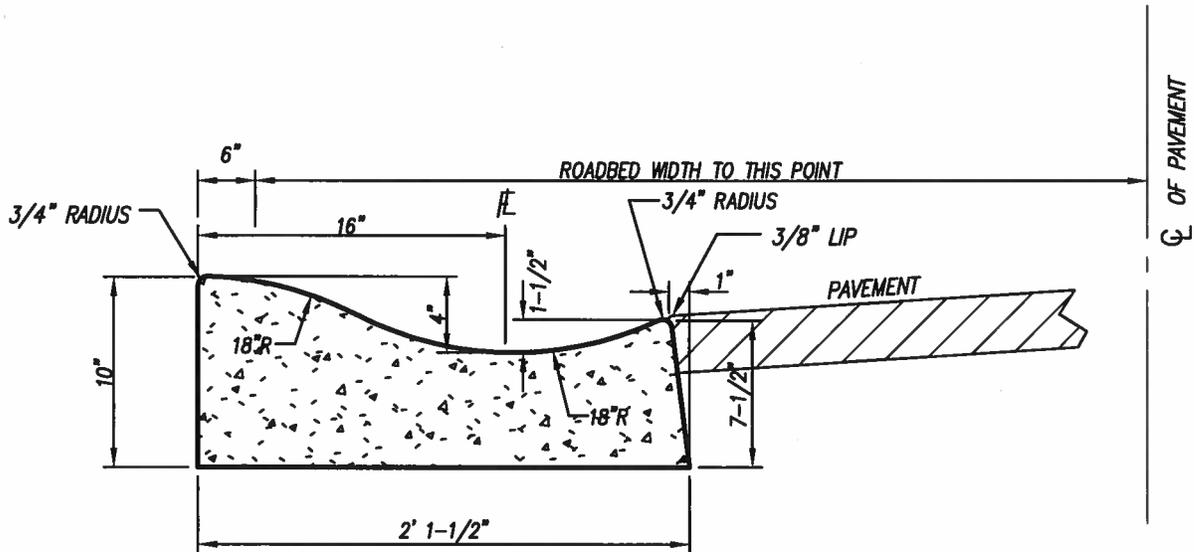
12/17/2001

* 7/5/2012

[Signature] 7/12/12

201

APPROVED BY: CITY ENGINEER DATE



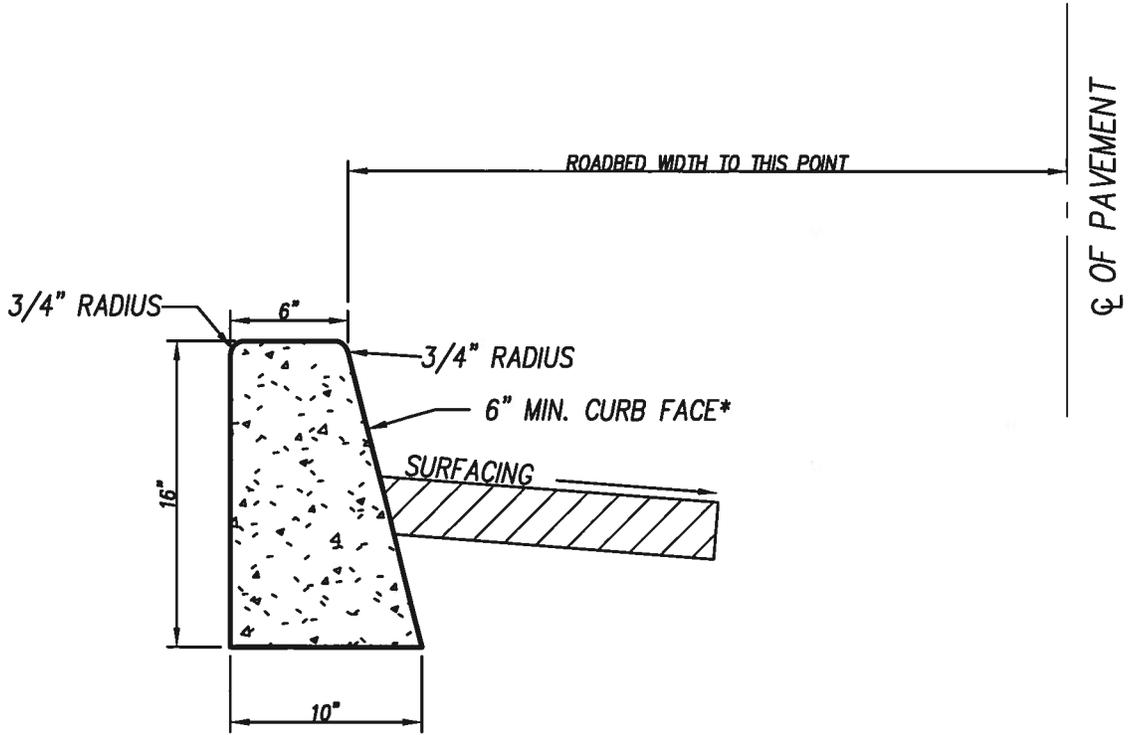
CLASS 560-C-3250 CONCRETE (NO FLY ASH)
 1.319 CU. FT./L.F.
 1 CU. YD.=20.47 L.F.

MINIMUM PERMISSIBLE GRADE 0.50%
 MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAXIMUM
 MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%
 ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING
 SEE STANDARD 206 FOR JOINT DETAILS

- * USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

NOTE: USE OF THIS STD. REQUIRES PRIOR APPROVAL FROM THE CITY ENGINEER. THIS SHOULD ONLY BE USED WHEN MATCHING EXISTING ROLL-CURB. SEE STANDARD DRAWING 222 FOR "WEDGE CURB" ALTERNATIVE.

CITY OF RANCHO MIRAGE		STANDARD DETAIL 202
REVISIONS	TYPE "C" CURB (ROLLED)	
12/17/2001		
* 7/5/2012		
APPROVED BY: CITY ENGINEER DATE		



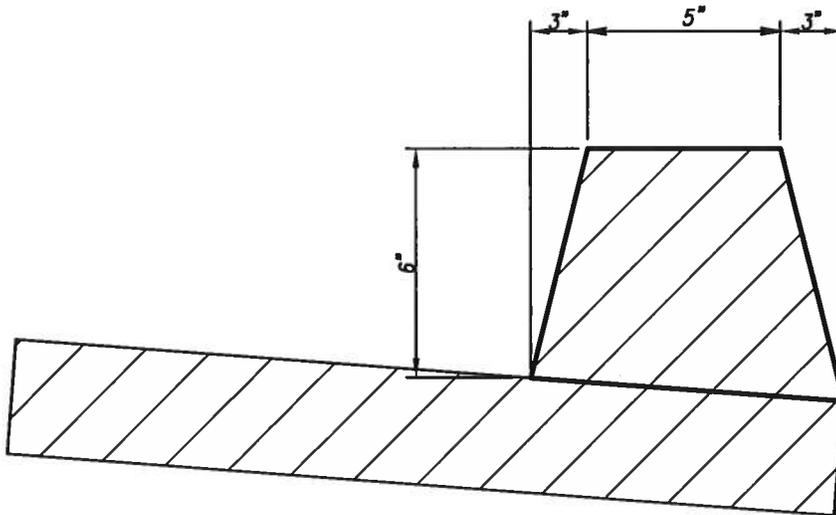
CLASS 560-C-3250 CONCRETE (NO FLY ASH)
 .888 CU. FT./L.F.
 1 CU. YD. = 30.41 L.F.

MINIMUM PERMISSIBLE GRADE 0.50%
 MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAXIMUM
 MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%
 SEE STANDARD 206 FOR JOINT DETAILS

* USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2,
 CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL
 BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

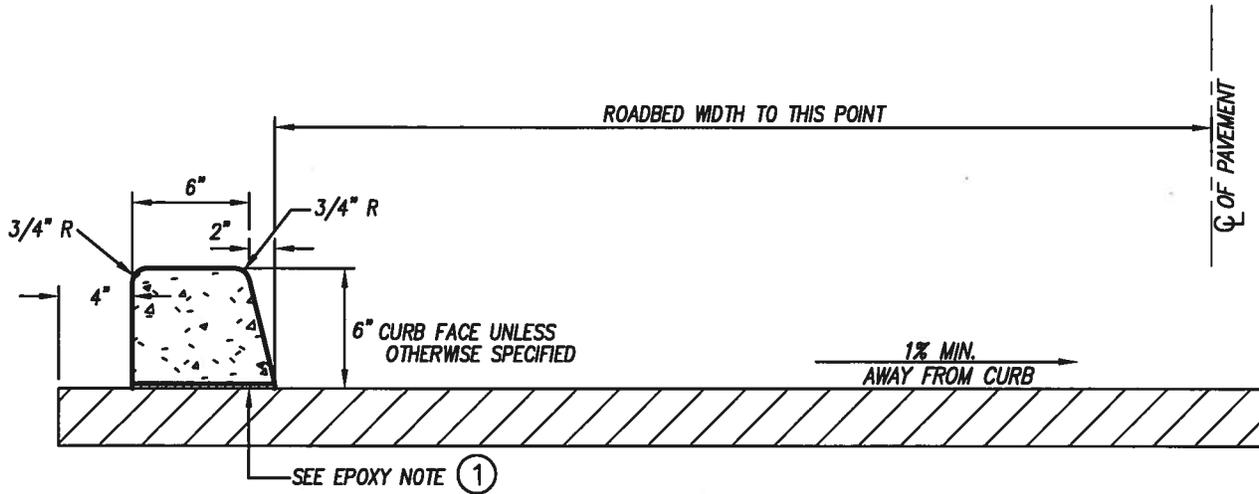
8" CURB FACE SHALL BE USED ON ALL ROADWAY MEDIAN CURBS

CITY OF RANCHO MIRAGE		STANDARD DETAIL 203
REVISIONS	TYPE "D" (6" OR 8") CURB	
12/17/2001		
* 7/5/2012		
	<i>[Signature]</i> 7/12/12	
	APPROVED BY: CITY ENGINEER DATE	



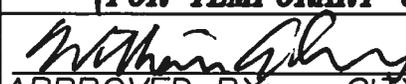
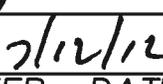
- * GRADE PG 70-10 ASPHALT CONCRETE
- MINIMUM PERMISSIBLE GRADE 0.50%
- BERM SHALL BE PAINTED TRAFFIC WHITE UNLESS OTHERWISE SPECIFIED.
- MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%

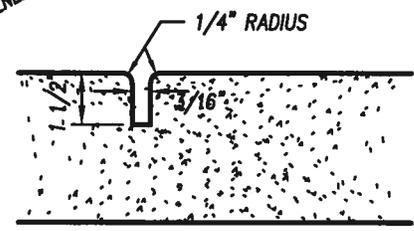
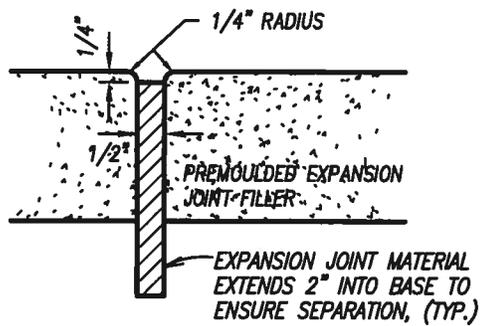
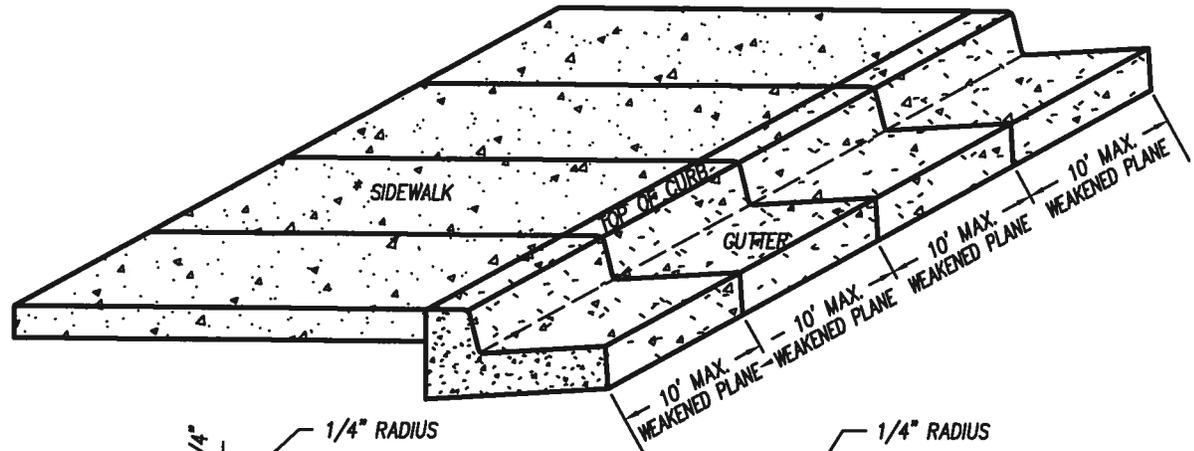
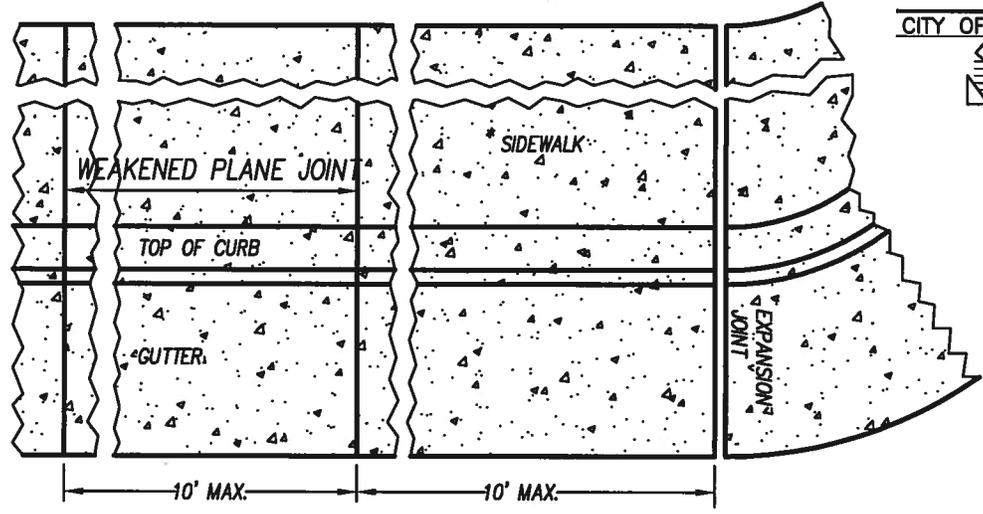
CITY OF RANCHO MIRAGE		STANDARD DETAIL 204
REVISIONS	TYPE "F" CURB-A.C. DIKE	
* 7/5/2012		
	<i>William Perry</i> 7/12/12	
	APPROVED BY: CITY ENGINEER DATE	



CLASS 560-C-3250 CONCRETE (NO FLY ASH)
 0.292 CU. FT./L.F.
 1 CU. YD. = 92.46 L.F.
 SEE STANDARD 206 FOR JOINT DETAILS

- ① EPOXY ADHESIVE SHALL BE IN ACCORDANCE WITH STATE OF CALIFORNIA (CALTRANS 2010) * SPECIFICATIONS SECTION 95. IN ADVANCE OF PLACING THE CURBS ON EXISTING PAVEMENT, THE PAVEMENT SHALL BE THOROUGHLY CLEANED AND THE ADHESIVE SHALL BE APPLIED. THE PAVEMENT SHALL BE CLEANED BY EITHER WIRE BRUSHING OR BY BLAST CLEANING. THE PAVEMENT SHALL BE FREE FROM DUST, LOOSE MATERIAL, OR OIL. MANUFACTURERS LITERATURE SHALL BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL PRIOR TO THE PLACEMENT OF THE EPOXY ADHESIVE.

CITY OF RANCHO MIRAGE		STANDARD DETAIL 205
REVISIONS	TYPE "D MODIFIED" GLUE DOWN CURB (FOR TEMPORARY USE ONLY)	
12/17/2001		
* 7/5/2012		
	 	
APPROVED BY: CITY ENGINEER DATE		



EXPANSION JOINT

WEAKENED PLANE JOINT

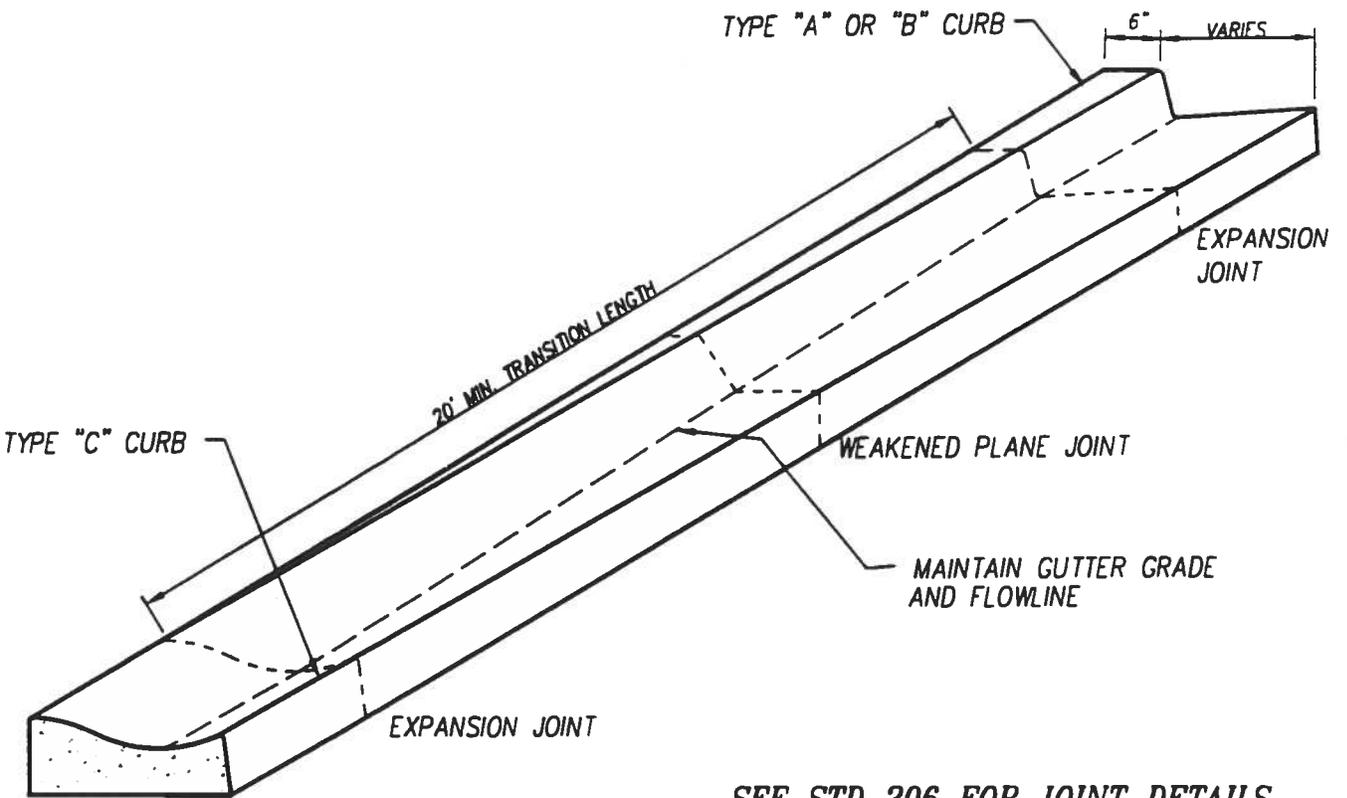
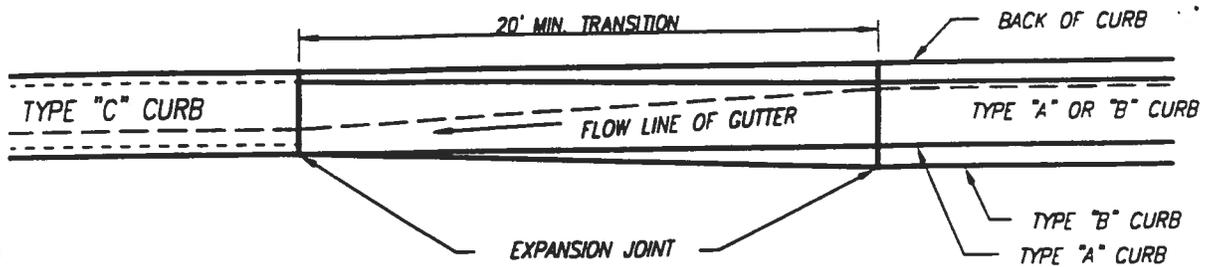
- EXPANSION JOINTS TO BE INSTALLED AT ALL B.C., E.C.,
- * CURB RETURNS, DRIVEWAYS, CATCH BASINS, OR
- * SIMILAR STRUCTURES, IN CURBS AND SIDEWALKS
- * SIDEWALKS SHALL ALSO GET EXPANSION JOINTS @ 40' O.C.

10' INTERVAL BETWEEN TRANSVERSE JOINTS MAY BE VARIED TO ALLOW MATCHING OF JOINTS IN ADJACENT EXISTING IMPROVEMENTS.

NOTE:

FOR MACHINE EXTRUDED CURBING IT IS REQUIRED TO "SAWCUT" JOINTS INTO THE CONCRETE WHEN IT HAS SET ENOUGH TO NOT FRACTURE. DO PARTIAL CUTS AT 10' INTERVALS, THROUGH THE CURB AND 1.5" INTO THE GUTTER FOR WEAKENED PLANE JOINTS. FOR EXPANSION JOINTS IN EXTRUDED CURBING, SAWCUT COMPLETELY AND INSERT JOINT MATERIAL AS DESCRIBED ABOVE.

CITY OF RANCHO MIRAGE		STANDARD DETAIL 206
REVISIONS	CURB, GUTTER & SIDEWALK JOINTS	
11/27/2002		
* 7/5/2012		
APPROVED BY:	7/12/12 CITY ENGINEER DATE	

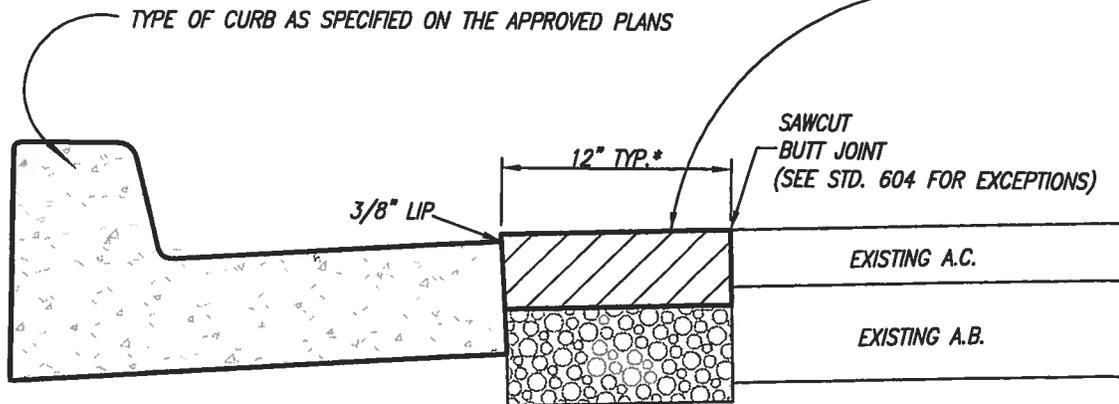


SEE STD 206 FOR JOINT DETAILS

CITY OF RANCHO MIRAGE		STANDARD DETAIL 207
REVISIONS	CURB TRANSITION	
APPROVED BY: <i>William G. ...</i> CITY ENGINEER 5/31/61 DATE		



RESIDENTIAL STREETS: 4" A.C. OVER 4" A.B.*
ALL OTHER STREETS: 5" A.C. OVER 10" A.B.*



MATCH EXISTING STREET CROSS SLOPE (MINIMUM 1%)

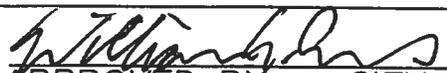
MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAX.

SLURRY OR FOG SEAL MAY BE REQUIRED AT THE DISCRETION OF THE CITY ENGINEER OR INSPECTOR.

ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING

SEE STANDARD 604 FOR "TRENCH" REPAIR PROCEDURES IF REMOVED ASPHALT AFFECTS THE ESSENTIAL 12 FOOT "DRIVING LANES" RATHER THAN JUST THE "PARKING LANE", OR IF THE SHOULDER AREA IS A "DESIGNATED BIKE ROUTE". AN ADDITIONAL "T CUT", GRINDING, AND OVERLAY WILL BE REQUIRED IN THOSE SITUATIONS.

* OR AS REQUIRED BY THE CITY ENGINEER

CITY OF RANCHO MIRAGE		STANDARD DETAIL 208
REVISIONS	CURB CONSTRUCTION IN EXISTING PAVEMENT	
11/27/2002		
 12/2/02 APPROVED BY: CITY ENGINEER DATE		



GIVEN:

B.C.R. ELEV.: _____

E.C.R. ELEV.: _____

P.I. ELEV. : _____

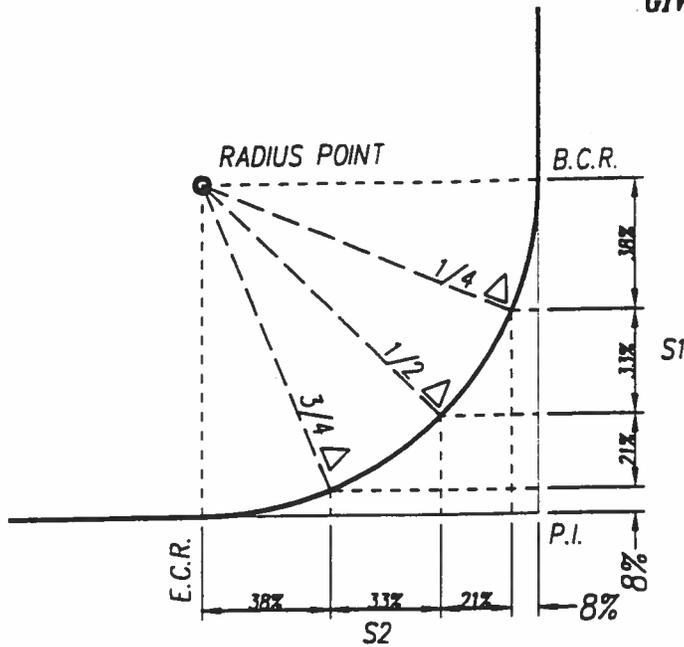
$S1 = (PI \text{ ELEV.}) - (BCR \text{ ELEV.}) =$ _____

$S2 = (ECR \text{ ELEV.}) - (PI \text{ ELEV.}) =$ _____

$1/4 \Delta = B.C.R. \text{ ELEV.} + 38\% S1 + 8\% S2$

$1/2 \Delta = B.C.R. \text{ ELEV.} + 38\% S1 + 33\% S1 + 8\% S2 + 21\% S2$

$3/4 \Delta = B.C.R. \text{ ELEV.} + 38\% S1 + 33\% S1 + 21\% S1 + 8\% S2 + 21\% S2 + 33\% S2$



NOTE: P.I. ELEV. IS POINT OF INTERSECTION OF TANGENT SLOPES FROM B.C.R. AND E.C.R. TYPICALLY THIS IS LABELED AS "P.I.G." IN SPANDRELS. GRADE BREAKS AT B.C.R. AND E.C.R. ARE NOT PERMITTED. NON-SPANDREL CURB RETURNS WITH NO POINT OF INTERSECTION MAY USE BACK-TO-BACK VERTICAL CURVES, PROPERLY LABELED.

B.C.R.= _____

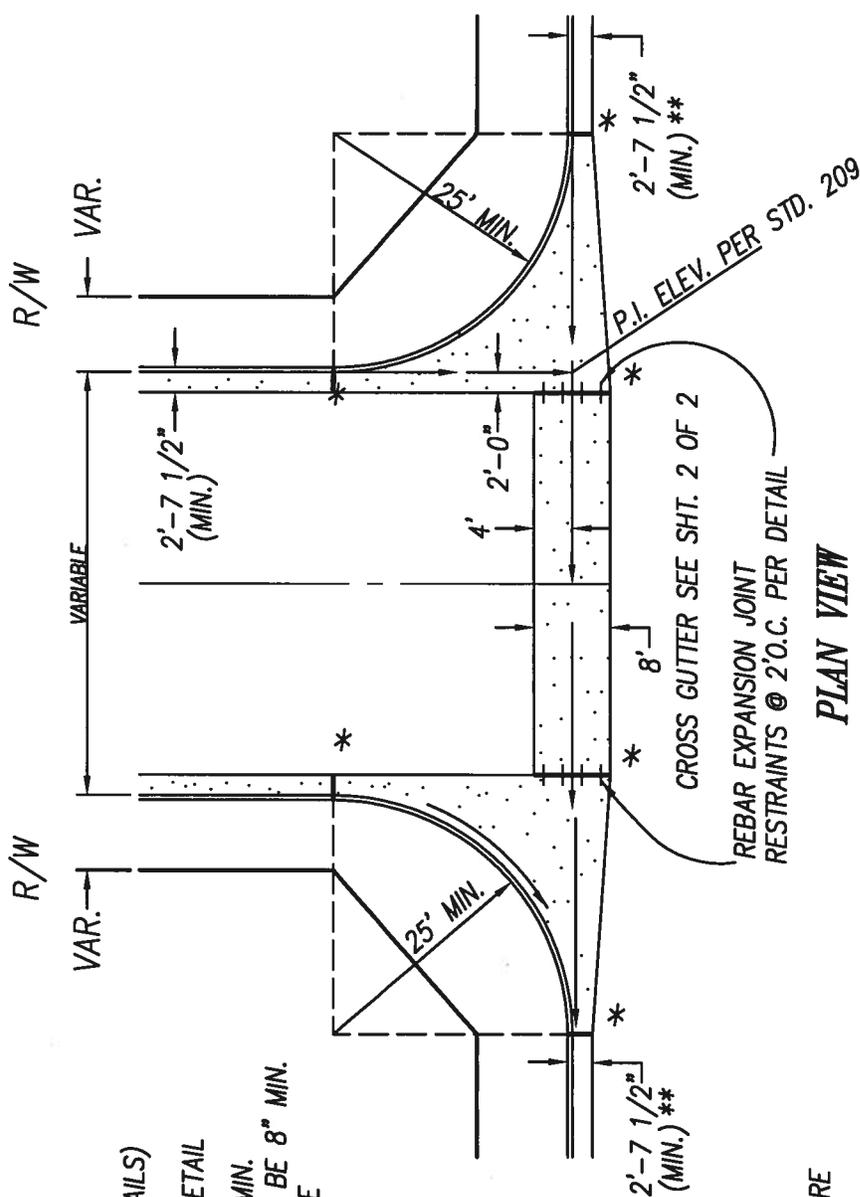
$1/4 \Delta$ B.C.R. ELEVATION + 38% S1 + 8% S2
 _____ + 0.38 () + 0.08 ()
 _____ + _____ = _____

$1/2 \Delta$ B.C.R. ELEVATION + 71% S1 + 29% S2
 _____ + 0.71 () + 0.29 ()
 _____ + _____ = _____

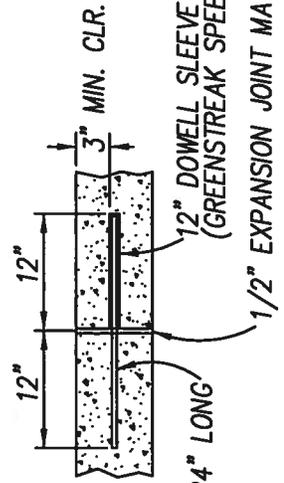
$3/4 \Delta$ B.C.R. ELEVATION + 92% S1 + 62% S2
 _____ + 0.92 () + 0.62 ()
 _____ + _____ = _____

E.C.R.= _____

CITY OF RANCHO MIRAGE		STANDARD DETAIL 209
REVISIONS	PLANAR CURB RETURN GRADE CALCULATIONS	
	<i>William [Signature]</i> 5/31/01	
	APPROVED BY: CITY ENGINEER DATE	



PLAN VIEW



#4 REBAR, 24" LONG
12" DOWELL SLEEVE FOR #4 REBAR (GREENSTREAK SPEED DOWELL, OR EQUIV.)
1/2" EXPANSION JOINT MATERIAL

REBAR EXPANSION JOINT RESTRAINT DETAIL

NOTES

- CROSS GUTTER FOR USE WITH TYPES "A" AND "B" CURB
- EXPANSION JOINTS AS SHOWN (SEE STD 206 FOR JOINT DETAILS) ADD REBAR EXPANSION JOINT RESTRAINTS PER PLAN AND DETAIL
- APRON THICKNESS TO BE 8" MIN.
- CROSS GUTTER THICKNESS TO BE 8" MIN.
- CLASS 560-C-3250 CONCRETE MINIMUM GRADE 0.50%
- PLACE 6" CL. 2 AGG. BASE UNDER ENTIRE SPANDREL & CROSS GUTTER AREA
- ** VARIES DEPENDING ON CURB AND GUTTER AS SPECIFIED
- ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING
- USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CONCRETE BEFORE ANY CONCRETE PAINTING OR THERMOPLASTIC APPLICATION

CITY OF RANCHO MIRAGE

STANDARD

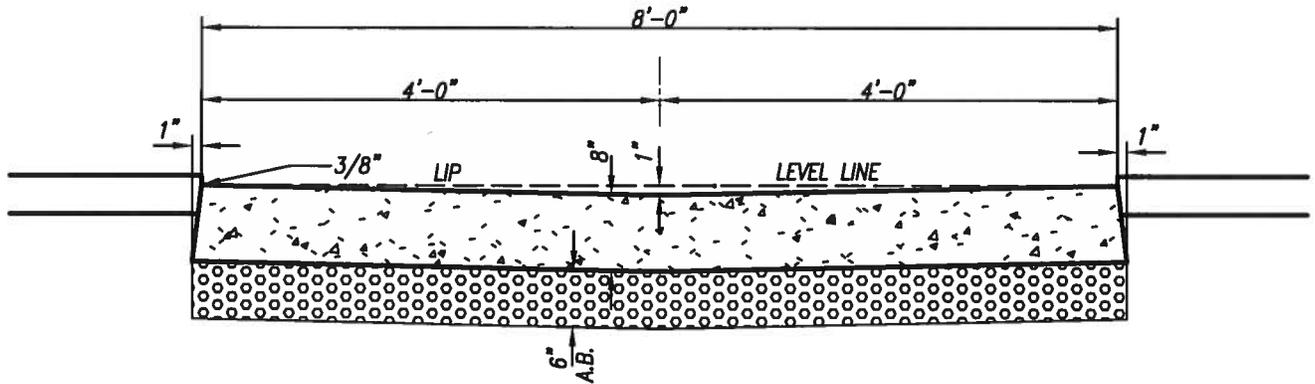
REVISIONS
* 7/5/2012

SPANDREL/CROSS GUTTER LAYOUT

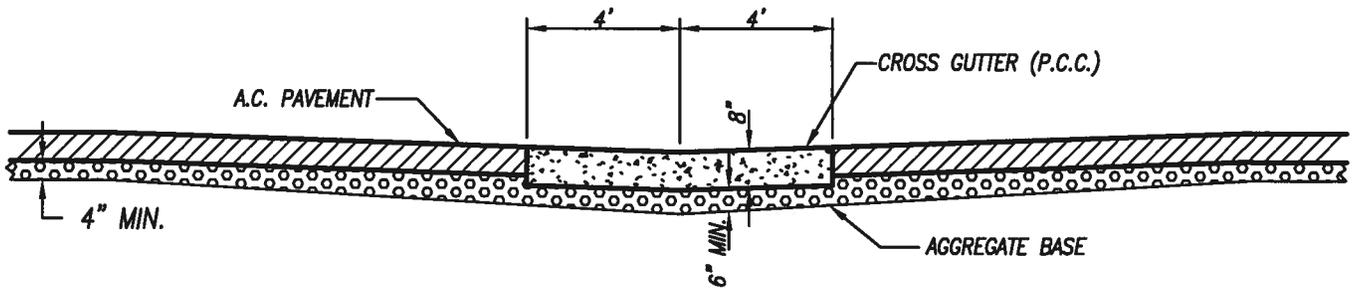
DETAIL

APPROVED BY: *[Signature]* 7/12/12
CITY ENGINEER DATE

210
SHT. 1 OF 2

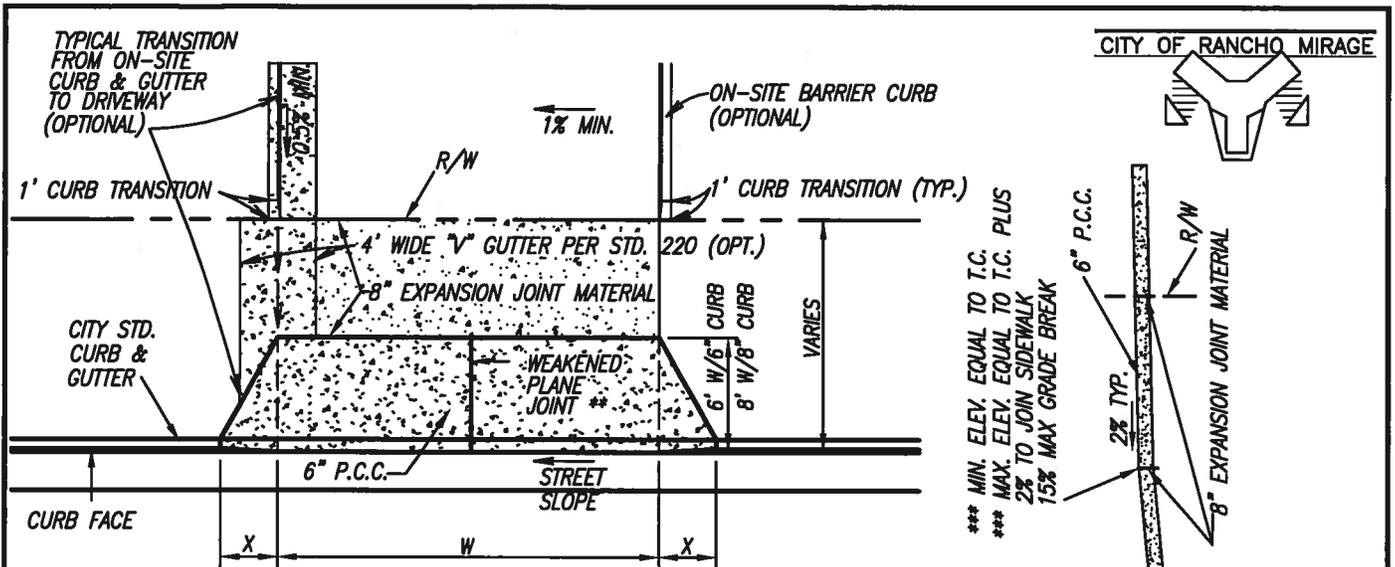


CROSS GUTTER



CLASS 560-C-3250 CONCRETE
ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING

CITY OF RANCHO MIRAGE		STANDARD DETAIL 210 SHT. 2 OF 2
REVISIONS	CROSS GUTTER DETAIL	
* 7/5/2012		
	<i>William G. G...</i> 7/12/12	
	APPROVED BY: CITY ENGINEER DATE	

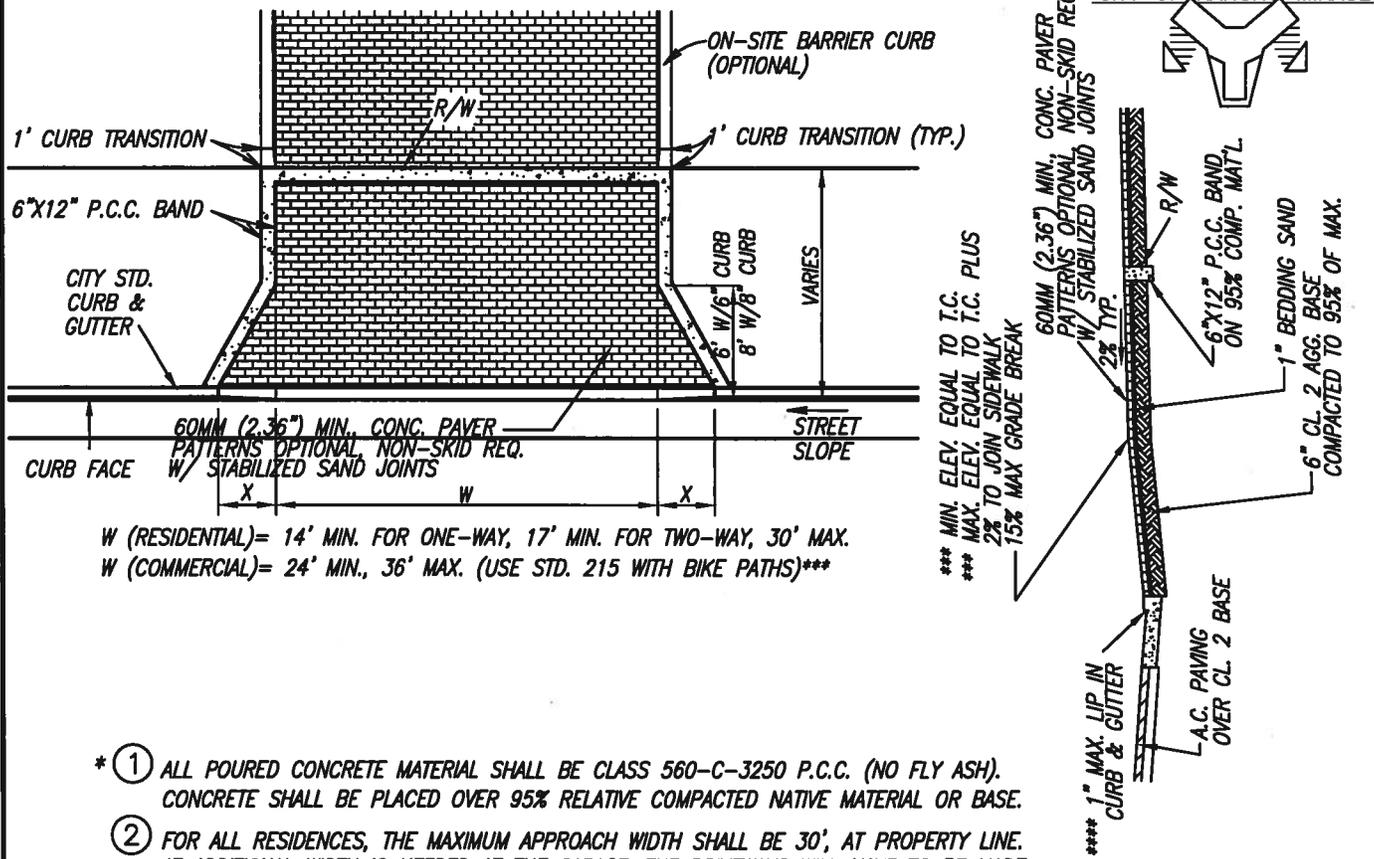


W (RESIDENTIAL)= 14' MIN. FOR ONE-WAY, 17' MIN. FOR TWO-WAY, 30' MAX.
 W (COMMERCIAL)= 24' MIN., 36' MAX. (USE STD. 215 WITH BIKE PATHS)***

- * ① ALL MATERIAL SHALL BE CLASS 560-C-3250 P.C.C. (NO FLY ASH) OF A SIX INCH THICKNESS. CONCRETE SHALL BE PLACED OVER 95% RELATIVE COMPACTED NATIVE MATERIAL OR BASE.
- ② FOR ALL RESIDENCES, THE MAXIMUM APPROACH WIDTH SHALL BE 30', AT PROPERTY LINE. IF ADDITIONAL WIDTH IS NEEDED AT THE GARAGE, THE DRIVEWAYS WILL HAVE TO BE MADE TO TRANSITION FROM THE 30' MAXIMUM AT PROPERTY LINE, WIDENING NEARER THE GARAGE.
- * ③ USE OF COLORING ADDITIVES IS ALLOWED PER MUNICIPAL CODE 15.68.050. COLOR APPROVAL BY PLANNING DEPARTMENT WILL BE REQUIRED. ALL DRIVEWAYS, WHETHER COLORED OR NOT, REQUIRE A COVENANT AND ENCROACHMENT PERMIT FOR PLACEMENT IN PUBLIC RIGHT-OF-WAY.
- ** ④ WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SEC. 303-5 OF THE STANDARD SPECIFICATIONS. FOR APPROACH WIDTHS OF 14'-20', ONE JOINT SHALL BE PLACED AT MID-APPROACH. FOR APPROACH WIDTHS OF 20'-30', TWO JOINTS SHALL BE PLACED WITH SYMMETRIC SPACING.
- ⑤ 20' OF FULL FACE CURB IS REQUIRED BETWEEN ANY TWO DRIVEWAY APPROACHES.
- *** ⑥ FOR SITES WITHOUT FULL-FACED CURB, THE MAXIMUM SLOPE ALLOWED ON THE APPROACH SHALL BE 10%. FOR SITES WITH FULL-FACED CURB, THE APPROACH SHALL AT A MINIMUM SLOPE UPWARDS TO THE TOP OF CURB LEVEL-LINE AND AT A MAXIMUM, THE APPROACH SHALL SLOPE UPWARDS TO THE TOP OF THE CURB LEVEL-LINE PLUS 2%, UNLESS OTHERWISE APPROVED.
- **** ⑦ NOTE THAT BECAUSE OF 1" LIP AND SLOPE OVER 8.33% THE STANDARD DRIVEWAY APPROACH IS NOT "ADA COMPATIBLE". IF IT IS TO BE UTILIZED AS A HANDICAPPED ACCESS ROUTE, THEN IT MUST BE BUILT IN ACCORDANCE WITH THE STATE TITLE 24 ACCESSIBILITY REGULATIONS. SEE STANDARDS 215 OR 223 FOR EXAMPLES.
- * ⑧ NO WATER METERS OR UTILITY BOXES ARE ALLOWED IN DRIVEWAY APPROACHES
- * ⑨ USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING.

"X" DIMENSIONS	
CURB HEIGHT	"X"
6"	2'-0"
8"	3'-0"

CITY OF RANCHO MIRAGE		STANDARD DETAIL 211 SHT. 1 OF 2
REVISIONS	P.C.C. DRIVEWAY APPROACH	
12/4/2002		
6/8/2004		
* 7/5/2012	<i>W. Whittaker</i> 7/12/12	
	APPROVED BY: CITY ENGINEER	DATE

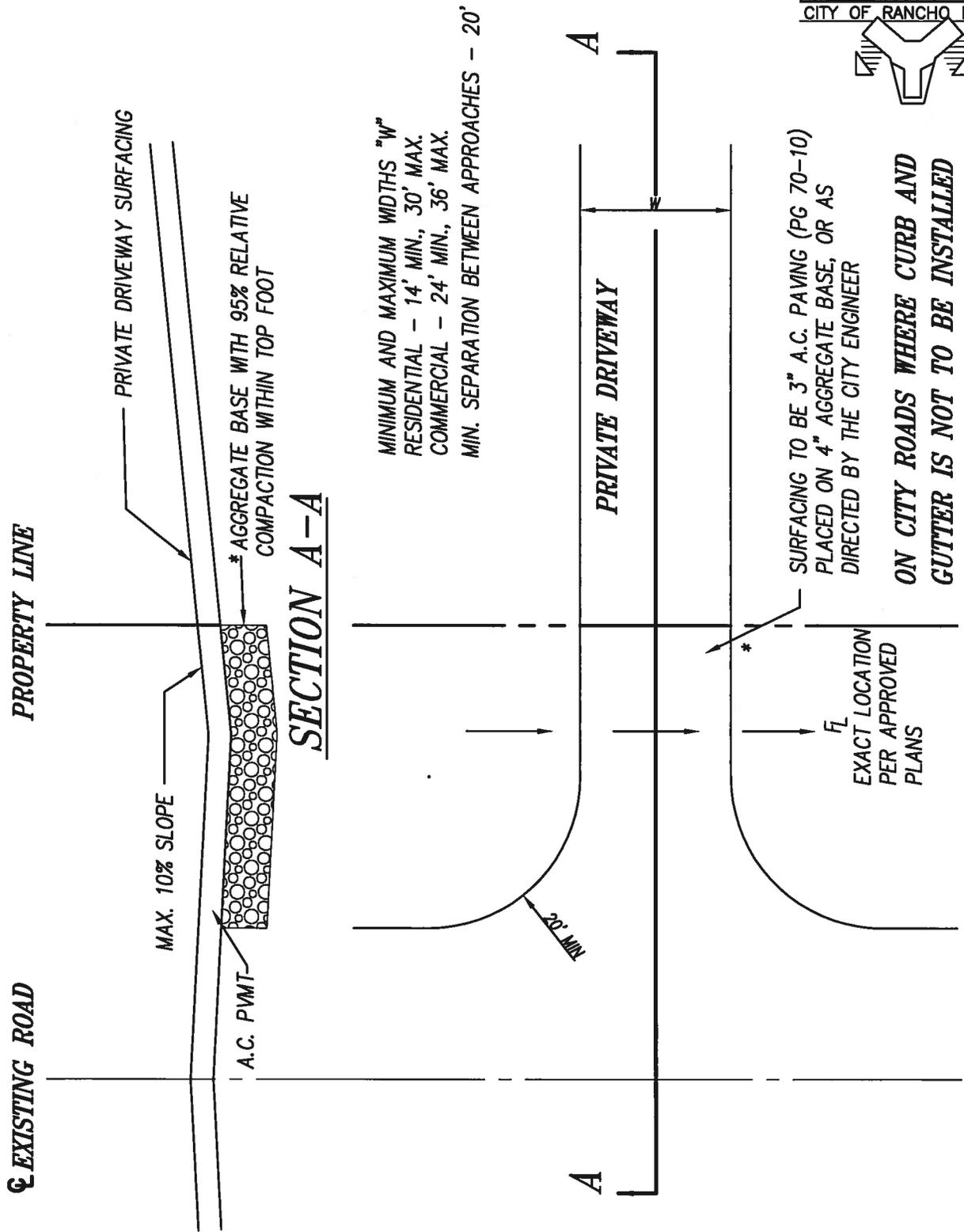


W (RESIDENTIAL)= 14' MIN. FOR ONE-WAY, 17' MIN. FOR TWO-WAY, 30' MAX.
 W (COMMERCIAL)= 24' MIN., 36' MAX. (USE STD. 215 WITH BIKE PATHS)***

- * ① ALL POURED CONCRETE MATERIAL SHALL BE CLASS 560-C-3250 P.C.C. (NO FLY ASH). CONCRETE SHALL BE PLACED OVER 95% RELATIVE COMPACTED NATIVE MATERIAL OR BASE.
- ② FOR ALL RESIDENCES, THE MAXIMUM APPROACH WIDTH SHALL BE 30', AT PROPERTY LINE. IF ADDITIONAL WIDTH IS NEEDED AT THE GARAGE, THE DRIVEWAYS WILL HAVE TO BE MADE TO TRANSITION FROM THE 30' MAXIMUM AT PROPERTY LINE, WIDENING NEARER THE GARAGE.
- * ③ USE OF COLORING ADDITIVES IS ALLOWED PER MUNICIPAL CODE 15.68.050. COLOR APPROVAL BY PLANNING DEPARTMENT WILL BE REQUIRED. ALL DRIVEWAYS, WHETHER COLORED OR NOT, REQUIRE A COVENANT AND ENCROACHMENT PERMIT FOR PLACEMENT IN PUBLIC RIGHT-OF-WAY.
- ** ④ WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SEC. 303-5 OF THE STANDARD SPECIFICATIONS. FOR APPROACH WIDTHS OF 14'-20', ONE JOINT SHALL BE PLACED AT MID-APPROACH. FOR APPROACH WIDTHS OF 20'-30', TWO JOINTS SHALL BE PLACED WITH SYMMETRIC SPACING.
- ⑤ 20' OF FULL FACE CURB IS REQUIRED BETWEEN ANY TWO DRIVEWAY APPROACHES.
- *** ⑥ FOR SITES WITHOUT FULL-FACED CURB, THE MAXIMUM SLOPE ALLOWED ON THE APPROACH SHALL BE 10%. FOR SITES WITH FULL-FACED CURB, THE APPROACH SHALL AT A MINIMUM SLOPE UPWARDS TO THE TOP OF CURB LEVEL-LINE AND AT A MAXIMUM, THE APPROACH SHALL SLOPE UPWARDS TO THE TOP OF THE CURB LEVEL-LINE PLUS 2%, UNLESS OTHERWISE APPROVED.
- **** ⑦ NOTE THAT BECAUSE OF 1" LIP AND SLOPE OVER 8.33% THE STANDARD DRIVEWAY APPROACH IS NOT "ADA COMPATIBLE". IF IT IS TO BE UTILIZED AS A HANDICAPPED ACCESS ROUTE, THEN IT MUST BE BUILT IN ACCORDANCE WITH THE STATE TITLE 24 ACCESSIBILITY REGULATIONS. SEE STANDARDS 215 OR 223 FOR EXAMPLES.
- * ⑧ NO WATER METERS OR UTILITY BOXES ARE ALLOWED IN DRIVEWAY APPROACHES
- * ⑨ ON POURED CONCRETE, USE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING.

"X" DIMENSIONS	
CURB HEIGHT	"X"
6"	2'-0"
8"	3'-0"

CITY OF RANCHO MIRAGE		STANDARD DETAIL 211 SHT. 2 OF 2
REVISIONS	PAVER DRIVEWAY APPROACH	
12/4/2002		
6/8/2004		
* 7/5/2012		
	APPROVED BY:	7/12/12 DATE



CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

A.C. DRIVEWAYS

DETAIL

4/09/2012

* 7/5/2012

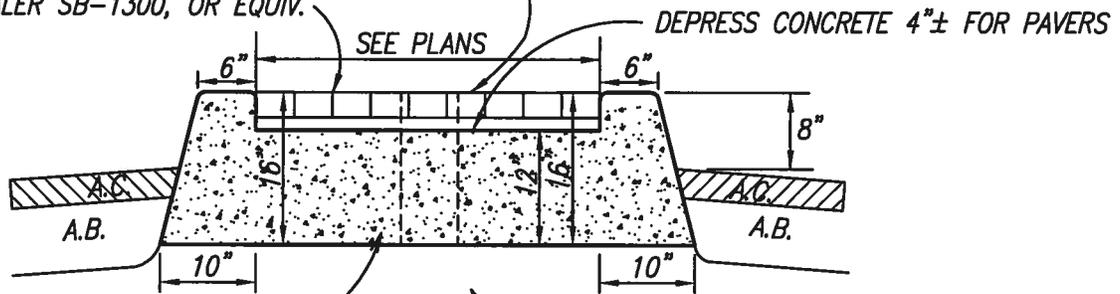
William Roberts 7/12/12
 APPROVED BY: CITY ENGINEER DATE

212

* SET 4" Lx4" Wx2.36" D PAVERS IN 1.64" ± SAND BED, "STACK BOND" PATTERN
 (60mm ACKERSTONE HOLLAND II, STANDARD FINISH, "THROUGH MIX, CREAM COLOR", OR EQUIV.)



STABILIZED SAND JOINTS WITH SUREBOND JOINT STABILIZING SEALER SB-1300, OR EQUIV.



CLASS 560-C-3250 CONCRETE

COMPACT SUBGRADE TO 90% RELATIVE TO MAX.

SECTIONAL VIEW

PAVERS OVER 1" BEDDING SAND OVER 4" CL. 2 AGG. BASE

STD. 203 BARRIER CURB

SEE PAVER SECTION DETAIL ON SHEET 2

EXPANSION JOINT

EXPANSION JOINTS

PAVERS OVER 1.64" BEDDING SAND OVER CONCRETE NOSE

MEDIAN NOSE DESIGNS PER STANDARD DETAIL 406

2 WAY REFLECTOR ON TOP OF CURB

6" DIAMETER PVC SLEEVE

1 WAY REFLECTORS ON TOP OF CURB

TOP VIEW

CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

MEDIAN NOSE DETAIL

DETAIL

07/01/2008

09/12/2011

* 7/5/2012

[Signature] 7/12/12
 APPROVED BY: CITY ENGINEER DATE

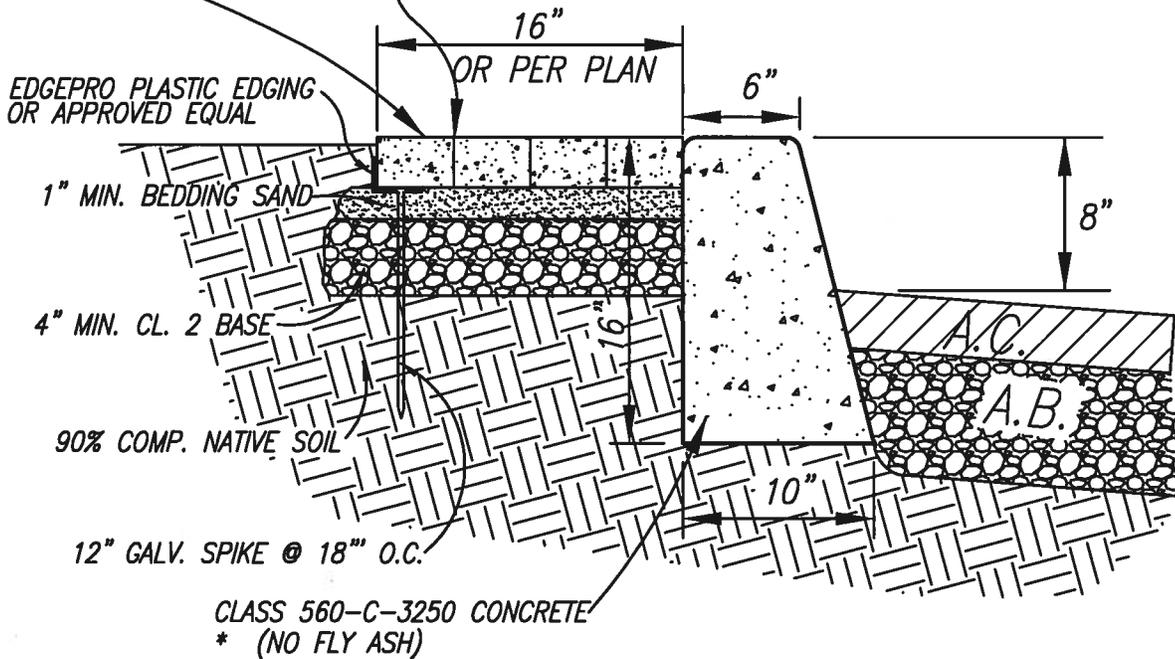
213

SHT. 1 OF 2



* SET 4" Lx4" Wx2.36" D PAVERS IN 1" MIN. SAND BED, "STACK BOND" PATTERN
 (60mm ACKERSTONE HOLLAND II, STANDARD FINISH,
 "THROUGH MIX, CREAM COLOR", OR EQUIV.)

STABILIZED SAND JOINTS WITH SUREBOND JOINT
 STABILIZING SEALER SB-1300, OR EQUIV.



CLASS 560-C-3250 CONCRETE
 * (NO FLY ASH)

PAVER SECTION DETAIL

CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

MEDIAN PAVER DETAIL

DETAIL

07/01/2008

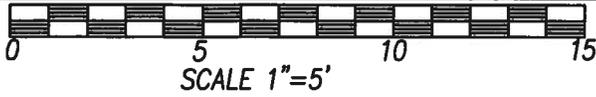
09/12/2011

* 7/5/2012

William [Signature] 7/12/12
 APPROVED BY: CITY ENGINEER DATE

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SHT. 2 OF 2



INSTALL ONE ISA R99 (AND R99A IF APPLICABLE) HANDICAPPED PARKING SIGN 3' OR MORE FROM CURB OR WHEEL STOP OR BEYOND SIDEWALK (PREFERRED)

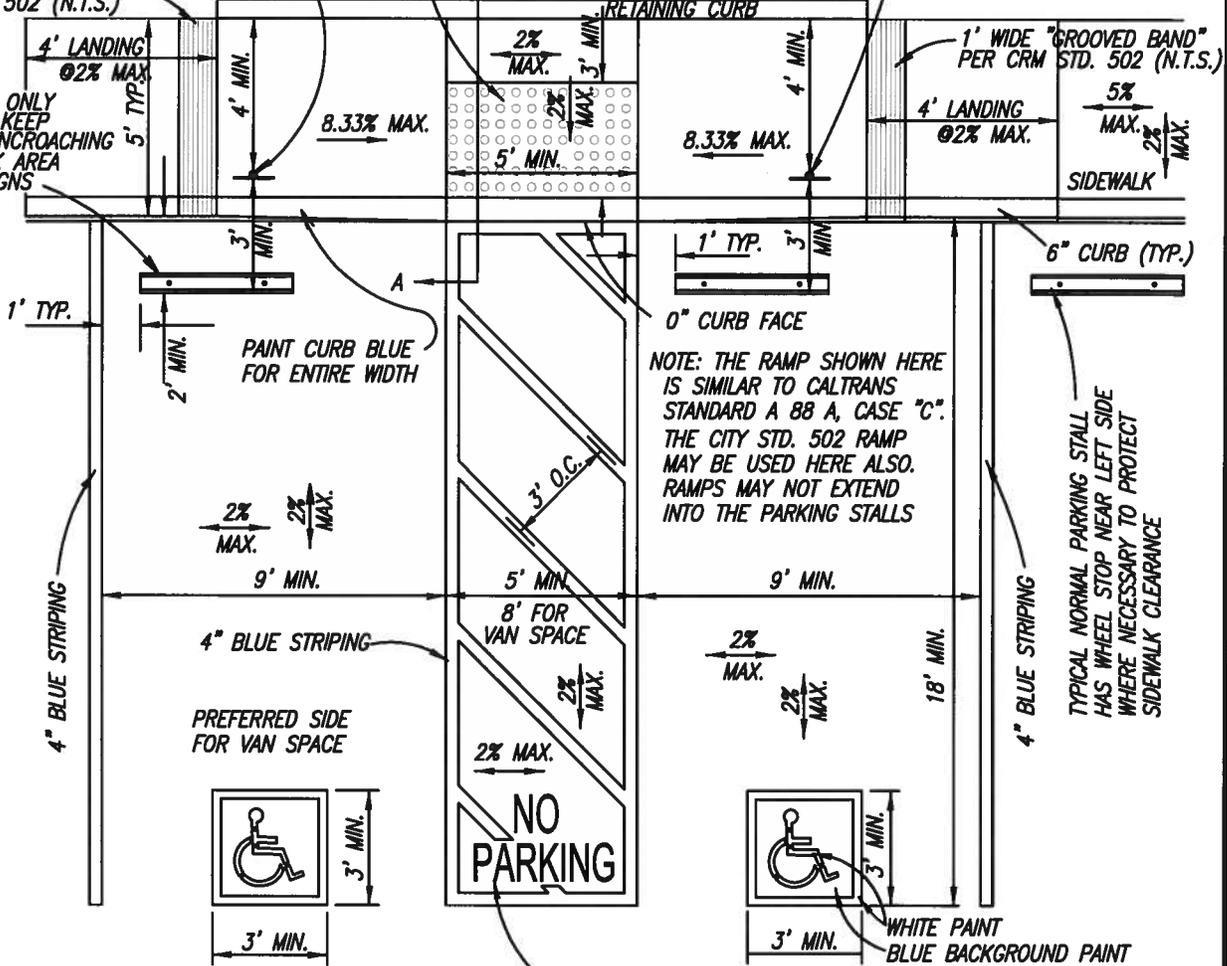
* NOTE: IF UNDER 5%, ANY CONNECTION TO PARKING AREA IS NOT A "RAMP" BUT STILL REQUIRES THE 3' WIDE BAND OF DOMES

TRUNCATED DOMES, ALWAYS REQUIRED * SEE STD. 502 FOR DETAILS (N.T.S.)

ISA R99 (INSTALL ONE PLACE) PREFERABLY NOT IN RAMP

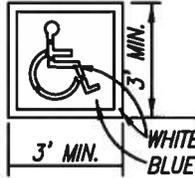
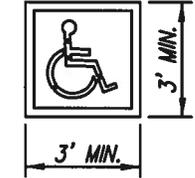
1' WIDE "GROOVED BAND" PER CRM STD. 502 (N.T.S.)

WHEEL STOPS ONLY REQUIRED TO KEEP CARS FROM ENCROACHING INTO SIDEWALK AREA OR HITTING SIGNS



NOTE: THE RAMP SHOWN HERE IS SIMILAR TO CALTRANS STANDARD A 88 A, CASE "C". THE CITY STD. 502 RAMP MAY BE USED HERE ALSO. RAMP MAY NOT EXTEND INTO THE PARKING STALLS

TYPICAL NORMAL PARKING STALL HAS WHEEL STOP NEAR LEFT SIDE WHERE NECESSARY TO PROTECT SIDEWALK CLEARANCE



ISA PARKING SIGN (R99) 12"x18" NOT TO SCALE

VAN ACCESSIBLE SIGN (R99A) 12"x8" NOT TO SCALE



NOTE: SITE DRAINAGE WATER SHOULD NOT BE DIRECTED TO RAMP OR ACROSS THE ENTRANCE TO IT, WHEREVER POSSIBLE.

SECTION A-A

CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

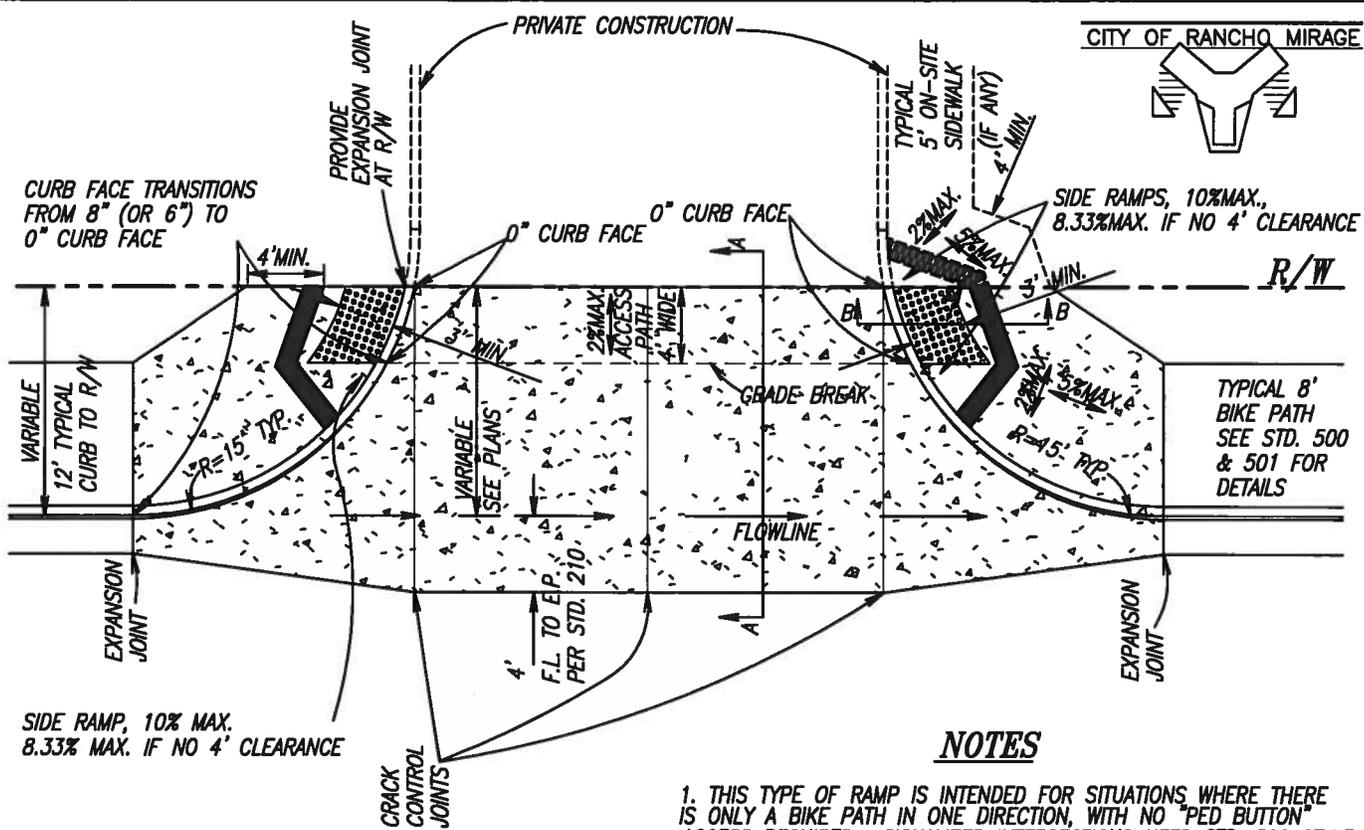
HANDICAP ACCESSIBLE PARKING STALLS (TYP.)

DETAIL

214

5/1/2003
5/19/2003
1/22/2009

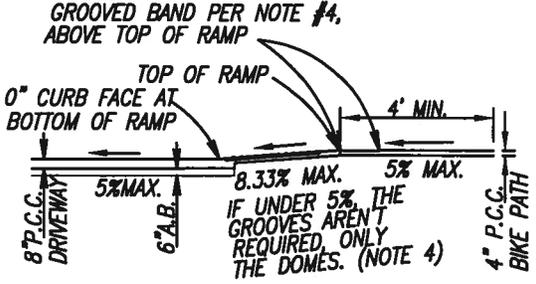
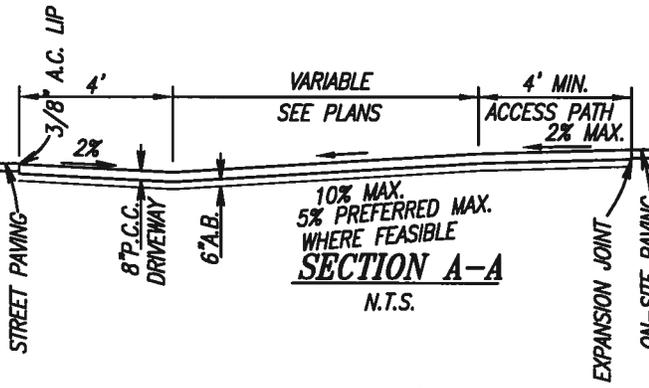
APPROVED BY: *[Signature]* 2/11/09
CITY ENGINEER DATE



SIDE RAMP, 10% MAX.
8.33% MAX. IF NO 4' CLEARANCE

NOTES

1. THIS TYPE OF RAMP IS INTENDED FOR SITUATIONS WHERE THERE IS ONLY A BIKE PATH IN ONE DIRECTION, WITH NO "PED BUTTON" ACCESS REQUIRED. SIGNALIZED INTERSECTIONS NEED STD. 502 STYLE.
2. THE BOTTOM OF THE RAMP SHALL HAVE A 0" CURB FACE.
3. SIDEWALK AND RAMP THICKNESS SHALL BE A MIN. 4".
4. IF OVER 5% SLOPE (DEFINITION OF "RAMP"), THE RAMP SHALL HAVE A 12" WIDE BORDER WITH 1/4" DEEP GROOVES APPROXIMATELY 3/4" O.C. SEE GROOVING DETAIL, STD. 502
5. NOTE THAT RAMP DIMENSIONS SHOULD BE DETERMINED BY SLOPE CRITERIA ONLY.
6. THE BIKE PATH AND ACCESSIBLE ROUTE THROUGH THE DRIVEWAY SHOULD NOT HAVE MORE THAN A 2% CROSS SLOPE.
7. ALL RAMP AND WALKWAY CONNECTIONS TO DRIVING AREAS NOW REQUIRE "TRUNCATED DOME" DETECTABLE WARNING SURFACE MARKERS PER STD. 502 DETAIL 3 FEET DEEP, "IN-LINE" PATTERN. THESE ARE AVAILABLE ON INSERTABLE PANELS, INDIVIDUAL UNITS, OR AS A "STAMP" FOR THE CONCRETE. THEY NEED TO BE COLORED * FEDERAL YELLOW IN PUBLIC RIGHT OF WAY.
- * 8. ALL MATERIAL SHALL BE CLASS 560-C-3250 P.C.C., (NO FLY ASH), THICKNESS AS INDICATED. CONCRETE SHALL BE PLACED OVER 95% RELATIVE COMPACTED AGGREGATE BASE IN THE DRIVEWAY. CONSTRUCT BIKEPATHS OVER 95% REL. COMP. NATIVE MATERIAL.
- * 9. PLACE 6" CL. 2 AGG. BASE UNDER ENTIRE CURB, SPANDREL AND DRIVEWAY, WITH 95% RELATIVE COMPACTION.
- * 10. USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CONCRETE BEFORE ANY CONCRETE PAINTING OR THERMOPLASTIC APPLICATION.



SECTION B-B
N.T.S.

CITY OF RANCHO MIRAGE		STANDARD DETAIL 215
REVISIONS	COMMERCIAL DRIVEWAY	
12/04/2002	APPROVED BY: CITY ENGINEER DATE	
1/22/2009		
* 7/5/2012		

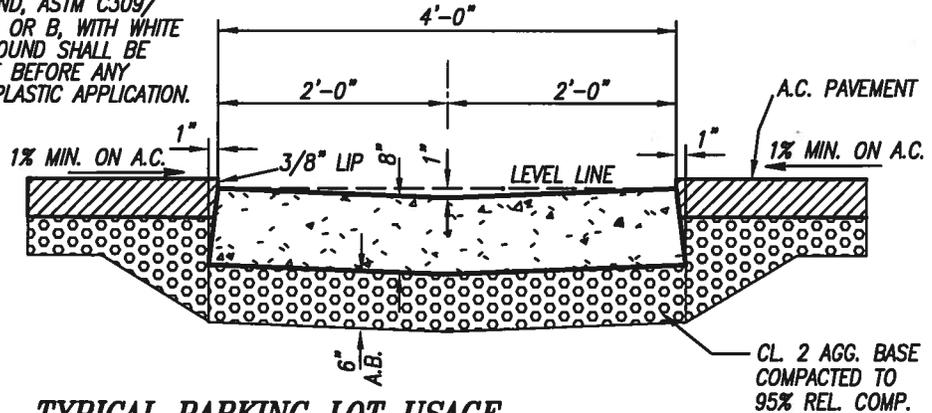
4' P.C.C. VALLEY GUTTER

CITY OF RANCHO MIRAGE

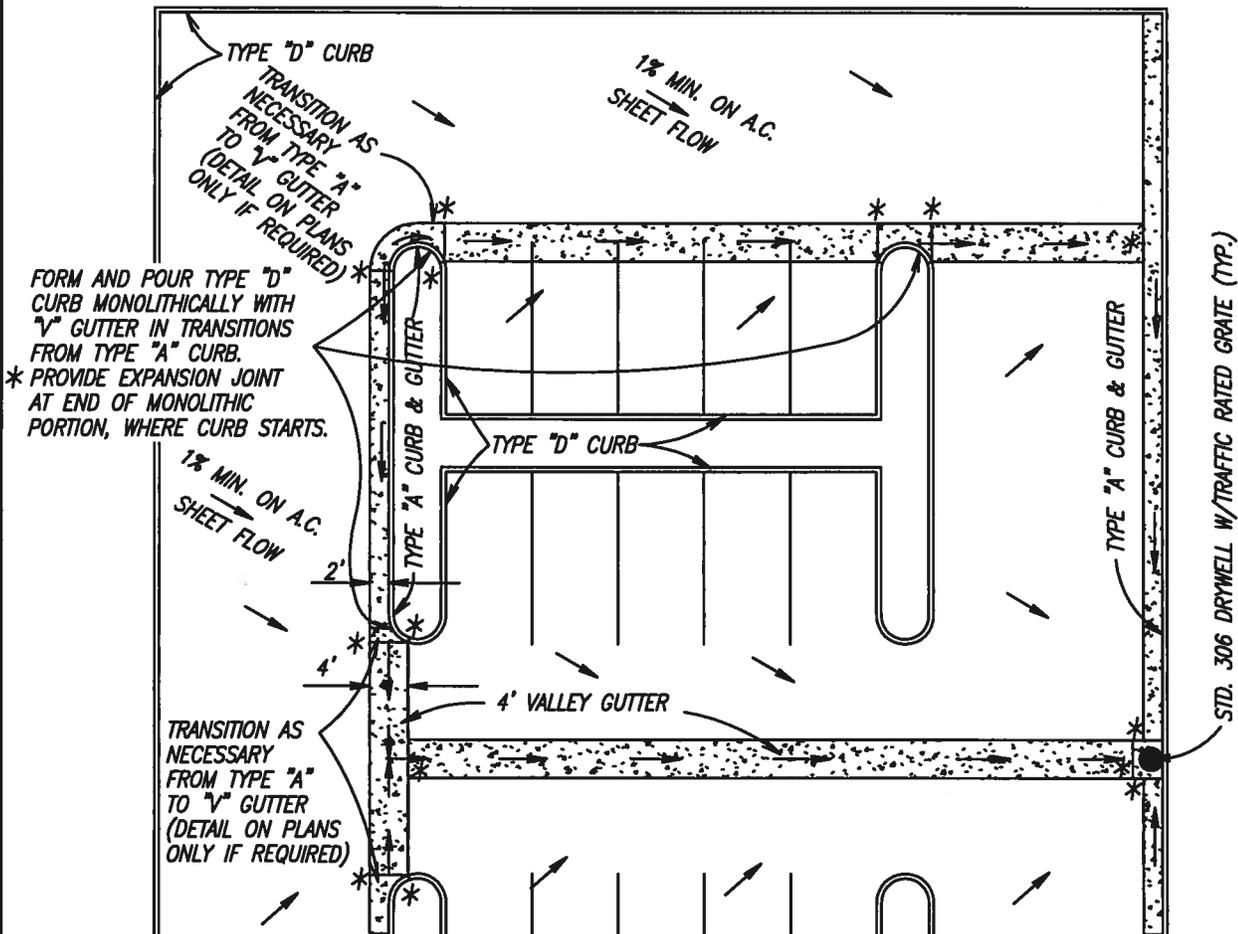


NOTES

- * 1. CLASS 560-C-3250 CONCRETE (NO FLY ASH)
- 2. 0.5% MIN. FLOWLINE SLOPE
- 3. ALL GUTTER SHALL BE "FLOW TESTED" PRIOR TO PAVING
- 4. SEE STANDARD 206 FOR JOINT DETAILS
- * 5. USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CONCRETE BEFORE ANY CONCRETE PAINTING OR THERMOPLASTIC APPLICATION.



TYPICAL PARKING LOT USAGE



CITY OF RANCHO MIRAGE

STANDARD

REVISIONS

4' VALLEY ("V") GUTTER

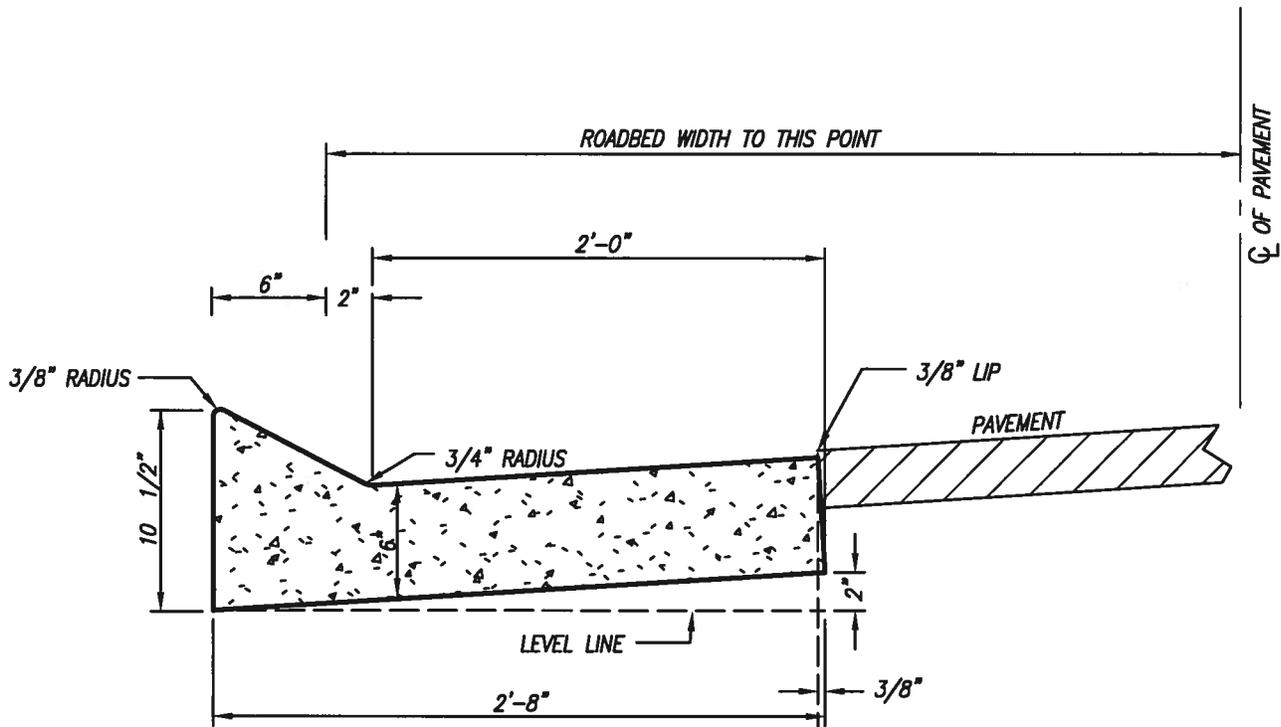
DETAIL

12/17/2001

* 7/5/2012

William A. ... 7/12/12
 APPROVED BY: CITY ENGINEER DATE

220



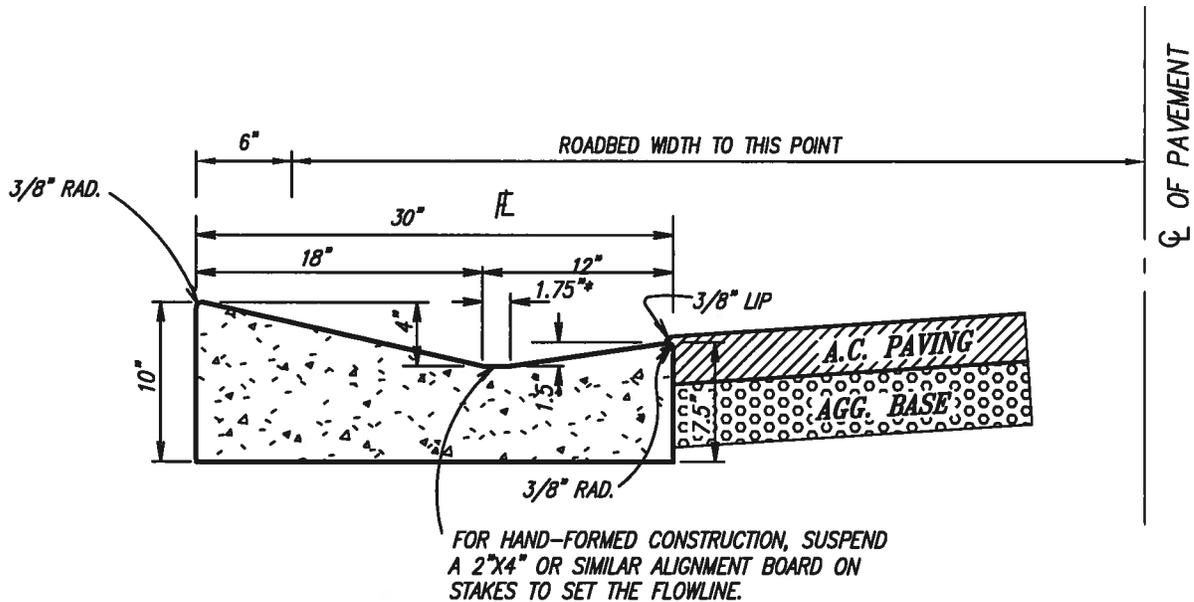
- * CLASS 560-C-3250 CONCRETE (NO FLY ASH)
 1.461 CU. FT./L.F.
 1 CU. YD. = 18.48 L.F.
 MINIMUM PERMISSIBLE GRADE 0.50%
 MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAXIMUM
 MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%
 ALL GUTTER SHOULD BE "FLOW TESTED" DURING FINISHING
 SEE STANDARD 206 FOR JOINT DETAILS
- * USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2,
 CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL
 BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

NOTE: THIS CURB IS FOR SECONDARY FIRE ACCESS ROADS OR SIMILAR LITTLE-USED DRIVEWAYS WHERE USE BY PASSENGER CARS IS DISCOURAGED. USE OF THIS CURB MUST BE PRE-APPROVED BY THE CITY ENGINEER AND FIRE MARSHALL.

CITY OF RANCHO MIRAGE		STANDARD DETAIL 221
REVISIONS	"MOUNTABLE" 4" CURB & GUTTER (EMERGENCY VEHICLE ACCESS)	
12/17/2001		
* 7/5/2012		
	<i>William [Signature]</i> 7/12/12	
APPROVED BY: CITY ENGINEER DATE		



SCALE 1"=1'



* CLASS 560-C-3250 CONCRETE (NO FLY ASH)

1.56 C.F./L.F.

1 C.Y. = 17.30 L.F.

MINIMUM PERMISSIBLE GRADE 0.50%

PROVIDE 3/8" LIP AT EDGE OF ASPHALT

MINIMUM SUBGRADE COMPACTION 95% RELATIVE TO MAXIMUM

MAX. GRADE BREAK ON ROADWAYS SHALL NOT EXCEED 0.50%

ALL GUTTER SHOULD BE "FLOW TESTED" DURING FINISHING

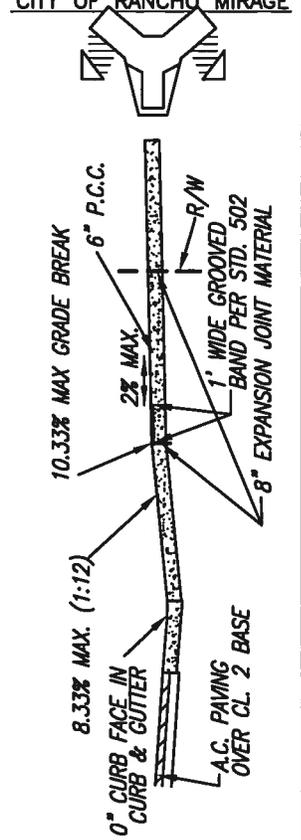
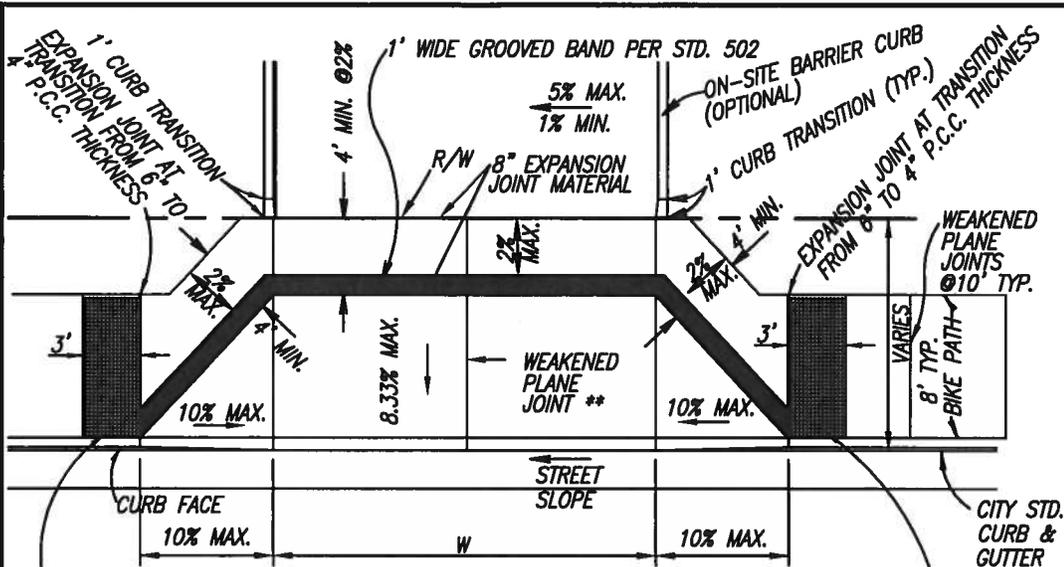
SEE STANDARD 206 FOR JOINT DETAILS

* USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

DRIVEWAY WIDTH ATTACHED TO WEDGE CURB SHALL BE IN ACCORDANCE WITH STANDARD 211

NOTE: THIS CURB IS INTENDED FOR PRIVATE RESIDENTIAL USE ONLY AND MUST BE PRE-APPROVED BY THE CITY ENGINEER PRIOR TO USE.

CITY OF RANCHO MIRAGE		STANDARD
RESIDENTIAL "WEDGE" CURB		
REVISIONS		DETAIL
12/17/2001		
06/08/2004		
* 7/5/2012		
APPROVED BY: 		222
CITY ENGINEER DATE 7/12/12		



W (RESIDENTIAL)= 14' MIN. FOR ONE-WAY, 17' MIN. FOR TWO-WAY, 30' MAX.
 W (COMMERCIAL)= 24' MIN., 36' MAX. (STD. 215 IS A RADIUSED ALTERNATIVE FOR BIKE PATHS)***
 3' WIDE TRUNCATED DOME PANELS (IN-LINE STYLE, FEDERAL YELLOW, PER STD. 502) (PREFERABLY SET IN WET CONCRETE) TO INDICATE EDGE OF PEDESTRIAN AREA

- * ① ALL MATERIAL SHALL BE CLASS 560-C-3250 P.C.C. (NO FLY ASH) OF A SIX INCH THICKNESS. CONCRETE SHALL BE PLACED OVER 95% RELATIVE COMPACTED NATIVE MATERIAL OR BASE.
- ② FOR ALL RESIDENCES, THE MAXIMUM APPROACH WIDTH SHALL BE 30', AT PROPERTY LINE. IF ADDITIONAL WIDTH IS NEEDED AT THE GARAGE, THE DRIVEWAYS WILL HAVE TO BE MADE TO TRANSITION FROM THE 30' MAXIMUM AT PROPERTY LINE, WIDENING NEARER THE GARAGE.
- ③ USE OF COLORING ADDITIVES IS ALLOWED PER MUNICIPAL CODE 15.68.050. COLOR APPROVAL BY PLANNING DEPARTMENT WILL BE REQUIRED. ALL DRIVEWAYS, WHETHER COLORED OR NOT, REQUIRE A COVENANT AND ENCROACHMENT PERMIT FOR PLACEMENT IN PUBLIC RIGHT-OF-WAY.
- ** ④ WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SEC. 303-5 OF THE STANDARD SPECIFICATIONS. FOR APPROACH WIDTHS OF 14'-20', ONE JOINT SHALL BE PLACED AT MID-APPROACH. FOR APPROACH WIDTHS OF 20'-30', TWO JOINTS SHALL BE PLACED WITH SYMMETRIC SPACING, WITH A MAXIMUM SPACING OF 10' O.C.
- ⑤ 20' OF FULL FACE CURB IS REQUIRED BETWEEN ANY TWO DRIVEWAY APPROACHES.
- * ⑥ FOR SITES WITHOUT FULL-FACED CURB, THE MAXIMUM SLOPE ALLOWED ON THE APPROACH SHALL BE 8.33%. FOR SITES WITH FULL-FACED CURB, THE APPROACH SHALL AT A MINIMUM SLOPE UPWARDS TO THE TOP OF CURB LEVEL-LINE AND AT A MAXIMUM, THE APPROACH SHALL SLOPE UPWARDS TO THE TOP OF THE CURB LEVEL-LINE PLUS 2%, UNLESS OTHERWISE APPROVED.
- * ⑦ USE CONCRETE CURING COMPOUND, ASTM C309/AASHTO M148, TYPE 2, CLASS A OR B, WITH WHITE PIGMENT. AFTER CURING, COMPOUND SHALL BE "POWER WASHED" OFF CURB TOP AND FACE BEFORE ANY CURB PAINTING

CITY OF RANCHO MIRAGE		STANDARD DETAIL 223
REVISIONS	A.D.A COMPATIBLE DRIVEWAY	
12/04/2002	APPROVED BY: CITY ENGINEER DATE	
06/08/2004		
02/11/2009		
* 7/5/2012		